

Village of Wellington

*12300 Forest Hill Blvd
Wellington, FL 33414*



Meeting Agenda - Final

Tuesday, January 27, 2026

6:30 PM

Village Hall - Council Chambers

Village Council

*Michael J. Napoleone, Mayor
Tanya Siskind, Vice Mayor
John T. McGovern, Councilman
Maria Antuña, Councilwoman
Amanda Silvestri, Councilwoman*

Childcare Available - The Village of Wellington offers childcare for attendees of the Council Meetings. To sign up, please email us at VillageClerk@wellingtonfl.gov by Noon of the day prior to the meeting

1. CALL TO ORDER**2. INVOCATION**

Deacon Al Payne, St. Therese de Lisieux Catholic Church

3. PLEDGE OF ALLEGIANCE**4. APPROVAL OF AGENDA****5. CONSIDERATION OF EXTENDED TIME REQUESTS BY INTERESTED PARTIES FOR QUASI-JUDICIAL HEARINGS, IF ANY****6. PRESENTATIONS AND PROCLAMATIONS****A. [25-7510](#) PBSO INVITATION TO A DAY FOR AUTISM EVENT**

Deputy Scott Poritz with the Palm Beach County Sheriff's Office will extend an invitation to the Day for Autism Event to the Village Council.

7. CONSENT AGENDA**8. PUBLIC HEARINGS****A. [25-7506](#) ORDINANCE NO. 2026-06 (WELLINGTON VILLAGE (FKA K PARK) REZONING)**

AN ORDINANCE OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A REZONING [PETITION NUMBER 2025-0003-REZ] BY AMENDING THE ZONING DESIGNATION FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY FACILITIES (CF) TO MULTIPLE USE PLANNED DEVELOPMENT (MUPD), TOTALING APPROXIMATELY 65.98 ACRES, MORE OR LESS; FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY COMMERCIAL (CC) TO MUPD, TOTALING APPROXIMATELY 5.3 ACRES, MORE OR LESS; FOR A GRAND TOTAL 71.27-ACRES; PROVIDING A CONFLICTS CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING AN EFFECTIVE DATE.

Approval of Ordinance No. 2026-06 to amend the Zoning Designation of the property known as Wellington Village (fka K Park) from Community Facilities (CF) and Community Commercial (CC) to Multiple Use Planned Development (MUPD).

9. REGULAR AGENDA**10. PUBLIC COMMENT**

- 11. ATTORNEY'S REPORT
- 12. VILLAGE MANAGER'S REPORT
- 13. COUNCIL REPORTS
- 14. ADJOURNMENT

NOTICE

If a person decides to appeal any decision made by the Village Council with respect to any matter considered at this meeting, you will need a record of the proceedings, and you may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based. (The above notice is required by State Law. Anyone desiring a verbatim transcript shall have the responsibility, at his own cost, to arrange for the transcript).

Pursuant to the provision of the Americans With Disabilities Act: any person requiring special accommodations to participate in these meetings, because of a disability or physical impairment, should contact the Village Manager's Office (561) 791-4000 at least five calendar days prior to the Hearing.



Legislation Text

File #: 25-7510, **Version:** 1

ITEM: PBSO INVITATION TO A DAY FOR AUTISM EVENT

REQUEST: Deputy Scott Poritz with the Palm Beach County Sheriff's Office will extend an invitation to the Day for Autism Event to the Village Council.

EXPLANATION: Deputy Scott Poritz will speak briefly about the Day for Autism event scheduled for Sunday, February 22nd at Village Park and extend an invitation to the event to the Village Council.

BUDGET AMENDMENT REQUIRED: NO

PUBLIC HEARING: NO

QUASI-JUDICIAL:

FIRST READING:

SECOND READING:

LEGAL SUFFICIENCY: N/A

FISCAL IMPACT: NONE

WELLINGTON FUNDAMENTAL: Responsive Government

RECOMMENDATION: Deputy Scott Poritz with the Palm Beach County Sheriff's Office will extend an invitation to the Day for Autism Event to the Village Council.



Legislation Text

File #: 25-7506, **Version:** 2

ITEM: ORDINANCE NO. 2026-06 (WELLINGTON VILLAGE (FKA K PARK) REZONING)

AN ORDINANCE OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A REZONING [PETITION NUMBER 2025-0003-REZ] BY AMENDING THE ZONING DESIGNATION FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY FACILITIES (CF) TO MULTIPLE USE PLANNED DEVELOPMENT (MUPD), TOTALING APPROXIMATELY 65.98 ACRES, MORE OR LESS; FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY COMMERCIAL (CC) TO MUPD, TOTALING APPROXIMATELY 5.3 ACRES, MORE OR LESS; FOR A GRAND TOTAL 71.27-ACRES; PROVIDING A CONFLICTS CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING AN EFFECTIVE DATE.

REQUEST: Approval of Ordinance No. 2026-06 to amend the Zoning Designation of the property known as Wellington Village (fka K Park) from Community Facilities (CF) and Community Commercial (CC) to Multiple Use Planned Development (MUPD).

EXPLANATION: The Applicants are requesting a Rezoning, Master Plan, and two Conditional Use approvals for the 71.27-acre property for the development of a mixed-use project with multifamily residential, retail, hotel, restaurant, office, and a K-12 private school. The specific requests are presented below.

Ordinance No. 2026-06 - Rezoning (Petition 2025-0003-REZ): To amend the Zoning Designation of the property, known as Wellington Village (fka K Park), from Community Facilities (CF) and Community Commercial (CC) to Multiple Use Planned Development (MUPD).

Resolution No. 2026-03 - Master Plan (Petition 2025-0001-MP): To adopt the Wellington Village MUPD Master Plan, which includes the following:

- Creates a 71.27-acre MUPD with the following pods and respective development program:
 - Pod A (Institutional)
 - 43.76 acres
 - School - 1,750 students/405,000 SF
 - Pod B (Commercial)
 - 24.05 acres
 - Retail - maximum 210,000 SF
 - Restaurant - maximum 105,000 SF plus 20,000 SF of outdoor dining
 - Hotel - 180 rooms/175,000 SF
 - Office - maximum 75,000 SF
 - Multi-Family - 215 units/330,000 SF
 - Phase 1 - 15 units/30,000 SF
 - Phase 2 - 200 units/300,000 SF
 - Pod O (Open Space)

- 3.23 acres
- Lake and public use lake trail
- Municipal Pod (Utility)
 - 0.23 acres
 - Lift Station
- Adds three (3) access points on Stribling Way, one (1) access point along State Road 7, three (3) cross access points to Life Church property, and one (1) cross access point to the Farmington Estates and Family Vision for future cross access;
- Adopts a Project Standards Manual (PSM) specifically for the Wellington Village MUPD; and
- Establishes Conditions of Approval for the development.

Resolution No. 2026-04 - Conditional Use (Petition 2025-0001-CU): Approves a Conditional Use for a private school (primary and secondary) with a maximum of 1,750 students and an outdoor sports and aquatic center within Pod A of Wellington Village MUPD.

Resolution No. 2026-05 - Conditional Use (Petition 2025-0002-CU): Approves a Conditional Use of a 180-room hotel within Pod B of Wellington Village MUPD.

The Master Plan and Conditional Uses will be heard at the 2nd reading of the Rezoning application and have been provided for reference only.

At the January 21, 2026, Planning, Zoning and Adjustment Board Meeting, the Board unanimously recommended approval (7-0) of Ordinance No. 2026-06, Resolution No. R2026-03, Resolution No. R2026-04, and Resolution No. R2026-05.

The Board provided the following recommendations/suggestions to Council for Resolution No. R2026-02 (Wellington Village MUPD Master Plan):

- Streetscape materials should be clarified and added to the PSM;
- Discussion with Castellina POA regarding the proposed roundabout at the Castellina and Pod A intersection needs to be finalized; and
- Provide enhanced standards and illustrations within the PSM for the parking garage within Pod A

The Board recommended/suggested to Council for Resolution No. R2026-04 (Hotel CU) to advocate for a soft-branded hotel.

BUDGET AMENDMENT REQUIRED: NO

PUBLIC HEARING: YES **QUASI-JUDICIAL:** YES

FIRST READING: YES **SECOND READING:**

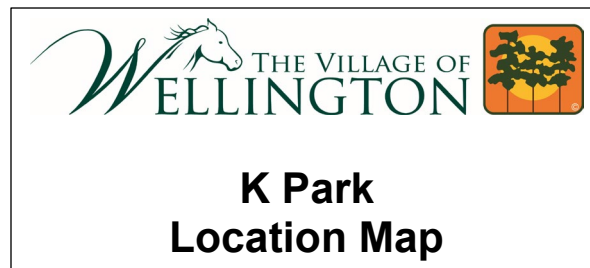
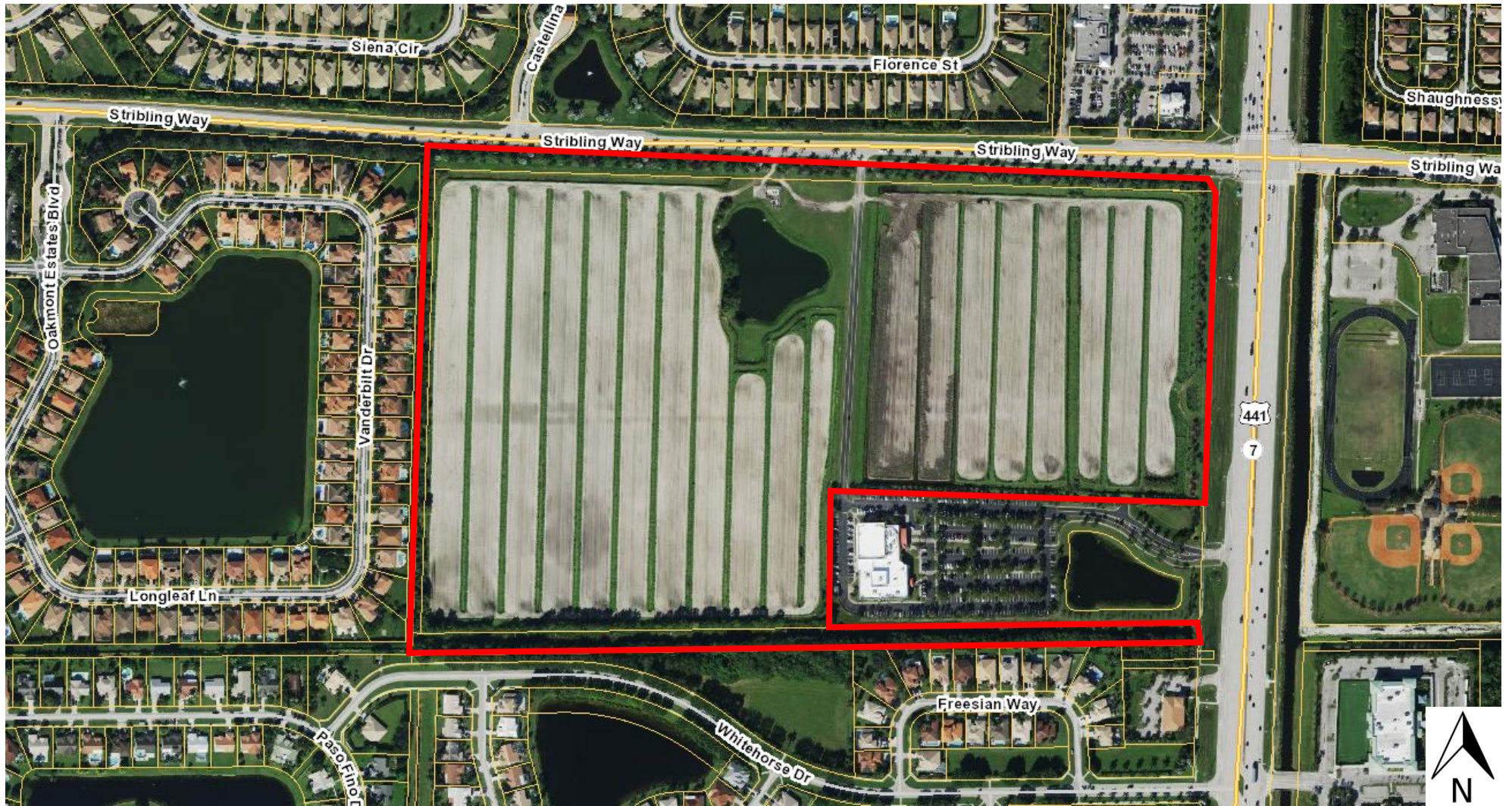
LEGAL SUFFICIENCY: YES

FISCAL IMPACT: N/A

WELLINGTON FUNDAMENTAL: Responsive Government

RECOMMENDATION: All required application documentation has been reviewed by the Development Review Manager and Wellington Departments to determine compliance with the Comprehensive Plan and the LDR, subject to the conditions provided in the staff report.

Findings of Fact are provided in the Staff Report that support the approval of the Rezoning request. These findings are subject to other competent substantial evidence presented at the quasi-judicial public hearing.



ORDINANCE NO. 2026-06

AN ORDINANCE OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A REZONING [PETITION NUMBER 2025-0003-REZ] BY AMENDING THE ZONING DESIGNATION FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY FACILITIES (CF) TO MULTIPLE USE PLANNED DEVELOPMENT (MUPD), TOTALING APPROXIMATELY 65.98 ACRES, MORE OR LESS; FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY COMMERCIAL (CC) TO MUPD, TOTALING APPROXIMATELY 5.3 ACRES, MORE OR LESS; FOR A GRAND TOTAL 71.27-ACRES; PROVIDING A CONFLICTS CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING AN EFFECTIVE DATE.

WHEREAS, the Wellington Council, as the governing body, pursuant to the authority vested in Chapter 163, Chapter 166, and Chapter 177, Florida Statutes, is authorized and empowered to consider changes to its Land Development Regulations (LDR), as adopted by Wellington, is authorized and empowered to consider petitions related to zoning and land development orders; and

WHEREAS, the Future Land Use Map designation for the subject property was recently changed to Mixed-Use by Ordinance No. 2025-04; and

WHEREAS, pursuant to Section 6.5.6.B of Wellington's LDR, the purpose and intent of a MUPD is to promote the use of a Planned Development District that has a unified design of mixed-uses within a single development; and

WHEREAS, the Zoning designation for the 65.98-acre property of CF is being changed to MUPD and the Zoning designation for the 5.3-acre property of CC is being changed to MUPD; and

WHEREAS, the proposed MUPD meets the purpose and intent of the Planned Development District Regulations, and is consistent with the property's Mixed Use Future Land Use Map (FLUM) designation; and

WHEREAS, the Planning, Zoning and Adjustment Board (PZAB) , with a __ to __ vote, recommended Council _____ of the Rezoning petition; and

WHEREAS, Wellington's Council has taken the recommendations from the Local Planning Agency and the comments from the public, along with the findings in the Staff Report, into consideration for the Zoning Map amendment that is the subject of this Ordinance; and

WHEREAS, the Council has determined that the proposed rezoning is consistent with Wellington's Comprehensive Plan Future Land Use Map and the Land Development Regulations.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF WELLINGTON, FLORIDA, THAT:

50
51 **SECTION 1:** Wellington's Official Zoning Map is hereby amended to change the zoning
52 designation for the 71.27-acre properties, known as Wellington Village (fka K Park), as legally
53 described in Exhibit A, from CF to MUPD.
54

55 **SECTION 2:** The Manager is hereby directed to update the Official Zoning Map of
56 Wellington, Florida, (Exhibit B) to designate the real property as described in Exhibit A as
57 MUPD and to effectuate the purpose of this Ordinance.
58

59 **SECTION 4:** Should any section paragraph, sentence, clause, or phrase of this
60 Ordinance be declared by a court of competent jurisdiction to be invalid, such decision shall
61 not affect the validity of this ordinance as a whole or any portion or part thereof, other than
62 the part to be declared invalid.
63

64 **SECTION 5:** Should any section, paragraph, sentence, clause, or phrase of this
65 Ordinance conflict with any section, paragraph, clause or phrase of any prior Wellington
66 Ordinance, Resolution, or Municipal Code provision; then in that event the provisions of this
67 Ordinance shall prevail to the extent of such conflict.
68

69 **SECTION 6:** This ordinance shall become effective immediately upon adoption by the
70 Wellington Council following second reading.
71
72

73 (The remainder of this page was intentionally left blank)

PASSED this ___ day of _____, 2026, upon first reading.

PASSED AND ADOPTED this _____ day of _____ 2026, on second and final reading.

WELLINGTON

FOR AGAINST

| | | |
|--------------------------------|-------|-------|
| BY: _____ | _____ | _____ |
| Michael J. Napoleone, Mayor | | |
| _____ | _____ | _____ |
| Tanya Siskind, Vice Mayor | | |
| _____ | _____ | _____ |
| John T. McGovern, Councilman | | |
| _____ | _____ | _____ |
| Maria Antuña, Councilwoman | | |
| _____ | _____ | _____ |
| Amanda Silvestri, Councilwoman | | |

ATTEST:

BY: _____
Chevelle D. Hall, MMC, Village Clerk

**APPROVED AS TO FORM AND
LEGAL SUFFICIENCY**

BY: _____
Laurie Cohen, Village Attorney

110 **Exhibit A**

111 **LEGAL DESCRIPTION**
112 **K PARK**
113

114 PARCEL 1 (FEE SIMPLE)

115 PORTIONS OF TRACTS 1 THROUGH 8, IN BLOCK 26, OF PALM BEACH FARMS CO.
116 PLAT NO. 3, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM
117 BEACH COUNTY, FLORIDA, AND THAT PART OF THE PALM BEACH FARMS CO. PLAT
118 NO. 3. ROADWAY NORTH OF TRACTS 1 TO 4 AND WEST OF TRACTS 4 AND 5, SAID
119 BLOCK 26, AS MORE FULLY DESCRIBED BELOW:

120
121 TRACTS 1 THROUGH 8. BLOCK 26, OF PALM BEACH FARMS CO. PLAT NO 3,
122 RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY,
123 FLORIDA,

124
125 LESS AND EXCEPT,

- 126
127 1. THE SOUTH 86 FEET OF SAID TRACTS 5 THROUGH 8 AND
128 2. THE RIGHT OF WAY FOR US 441. (STATE ROAD 7)
129 3. TEN ACRES, MORE OR LESS, DEEDED TO THE NEW COMMUNITY CHURCH OF THE
130 PALM BEACHES, AS RECORDED IN OFFICIAL RECORDS BOOK 10931, PAGE 489 AND
131 492.

132
133 ADD 4 ACRES OF PBF CO. ROADWAY NORTH OF TRACTS 1 TO 4 AND WEST OF
134 TRACTS 4 AND 5 ALL IN BLOCK 26, ORB 13325/1198, LESS THE SOUTH 86 FEET. ALSO
135 BEING DESCRIBED AS FOLLOWS: (THIS DESCRIPTION AUTHORIZED IN THE OFFICE
136 OF DENNIS J. LEAVY & ASSOCIATES, INC.)

137
138 A PARCEL OF LAND LYING IN SECTION 24, TOWNSHIP 44 SOUTH, RANGE 41 EAST,
139 BEING A PORTION OF TRACTS 1 THROUGH 8 INCLUSIVE, BLOCK 26, PALM BEACH
140 FARMS CO. PLAT NO. 3, AS RECORDED IN PLAT BOOK 2, PAGE 45, OF THE PUBLIC
141 RECORDS OF PALM BEACH COUNTY, FLORIDA, BEING MORE PARTICULARLY
142 DESCRIBED AS FOLLOWS:

143
144 COMMENCING AT THE NORTH ONE-QUARTER (1/4) CORNER OF SAID SECTION 24;
145 THENCE SOUTH 01°24'43" WEST, AS A BASIS OF BEARINGS, ALONG THE WEST LINE
146 OF THE NORTHEAST ONE-QUARTER OF SAID SECTION 24, SAID LINE ALSO BEING
147 THE WEST LINE OF BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, A DISTANCE OF
148 19.57 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 88°10'28" EAST, A
149 DISTANCE OF 1716.01 FEET; THENCE SOUTH 86°54'06" EAST, A DISTANCE OF 315.08
150 FEET; THENCE SOUTH 88°10'28" EAST A DISTANCE OF 330.50 FEET; THENCE SOUTH
151 43°08'16" EAST A DISTANCE OF 57.12 FEET TO A POINT ON THE WESTERLY RIGHT
152 OF-WAY LINE OF STATE ROAD #7 AND U.S. HIGHWAY #441, AS SHOWN ON THE
153 FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP, SECTION 93210-
154 2519, SHEET 9 OF 27, REVISION DATE 02-01-96, THE PRECEDING FOUR (4) COURSES
155 ALSO BEING COINCIDENT WITH THE NORTH LINE OF THOSE LANDS DESCRIBED IN
156 OFFICIAL RECORD BOOK 13325, PAGE 1198 OF THE PUBLIC RECORDS OF PALM
157 BEACH COUNTY, FLORIDA; THENCE SOUTH 01°53'57" WEST, ALONG SAID RIGHT-OF-

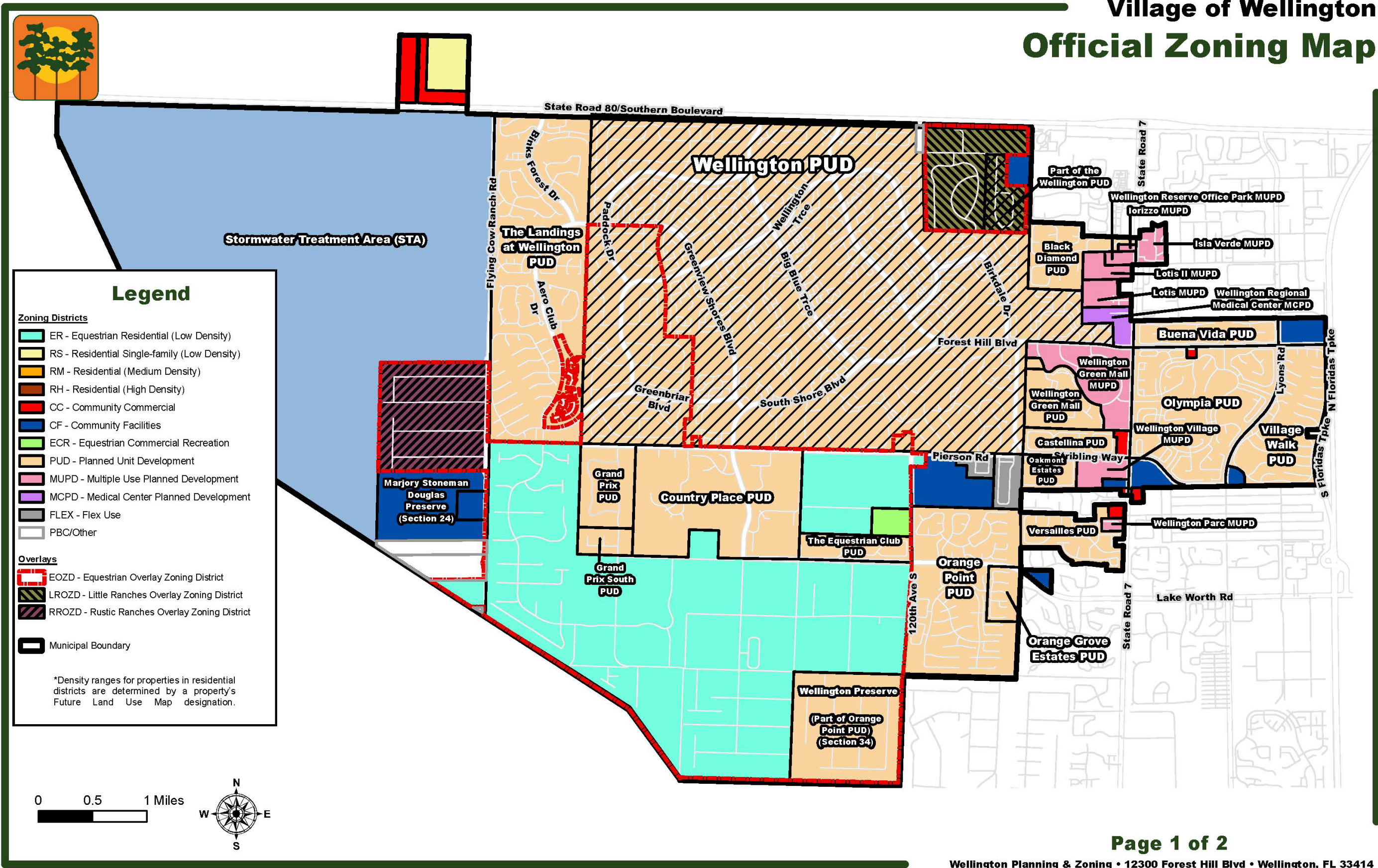
WAY LINE A DISTANCE OF 949.58 FEET; THENCE NORTH 88°05'50" WEST, DEPARTING SAID WEST RIGHT-OF-WAY LINE, A DISTANCE OF 1142.41 FEET; THENCE SOUTH 01°07'58" EAST, A DISTANCE OF 415.00 FEET TO A POINT ON A LINE 86.00 FEET NORTH OF AND PARALLEL WITH (AS MEASURED AT RIGHT ANGLES) THE SOUTH LINE OF SAID TRACTS 5 AND 6, THE PRECEDING TWO (2) COURSES ALSO BEING COINCIDENT WITH THE NORTH AND WEST LINES OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 10931, PAGE 489 AND 492 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; THENCE SOUTH 88°52'02" WEST, ALONG SAID PARALLEL LINE, A DISTANCE OF 1270.71 FEET TO A POINT ON THE AFOREMENTIONED WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF SECTION 24 AND THE WEST LINE OF SAID BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3; SAID LINE ALSO BEING COINCIDENT WITH THE WEST LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; THENCE NORTH 01°24'43" EAST, ALONG SAID WEST LINE, A DISTANCE OF 1475.51 FEET TO THE POINT OF BEGINNING. SAID LANDS SITUATE, LYING AND BEING IN PALM BEACH COUNTY, FLORIDA.

PARCEL 2 (FEE SIMPLE):

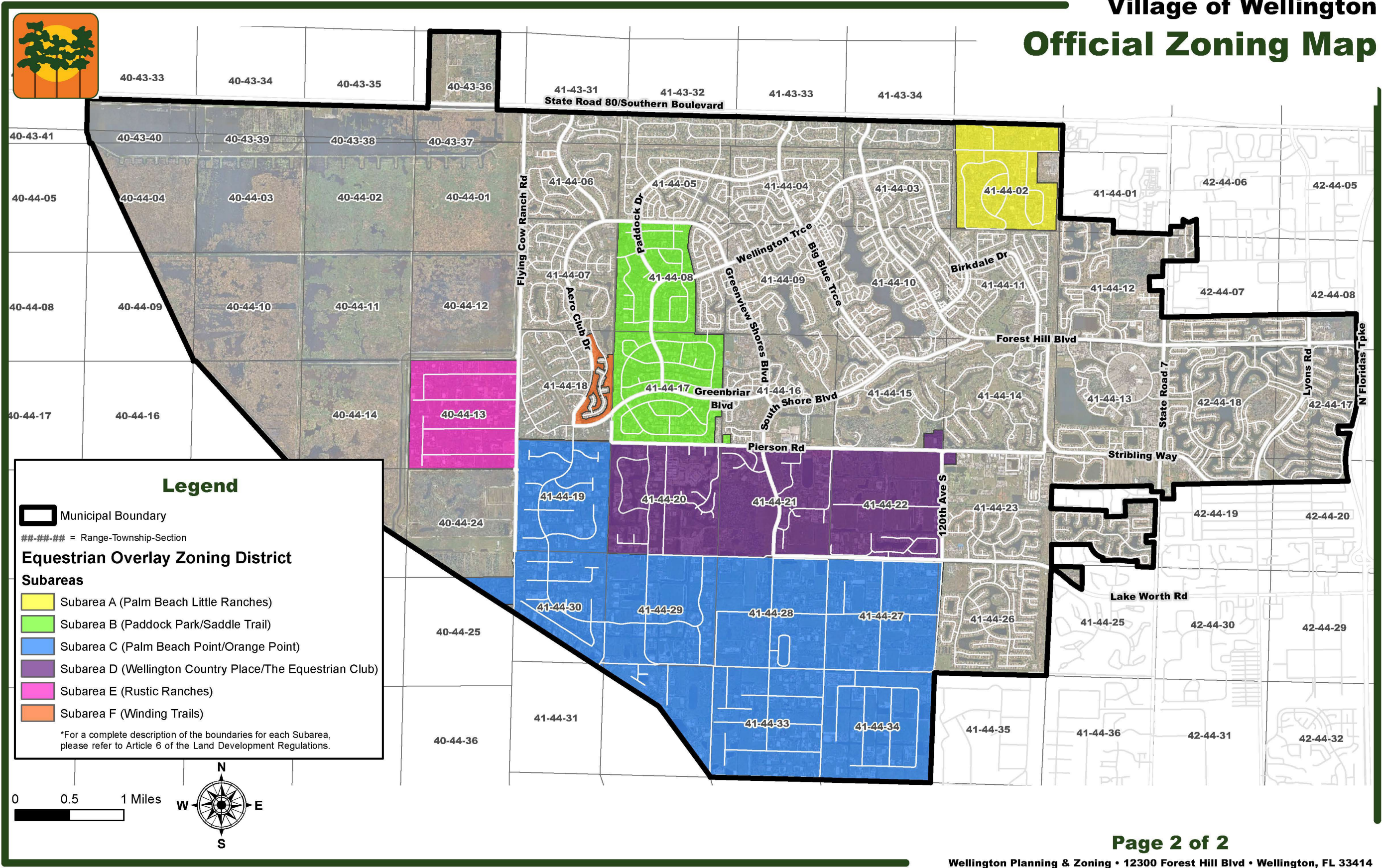
TRACT C, OAKMONT ESTATES P.U.D., ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 106, PAGE 2, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA.

PARCEL 3 (EASEMENT):

TOGETHER WITH: NON-EXCLUSIVE EASEMENT(S) FOR THE BENEFIT OF PARCEL 1 ABOVE AS SET FORTH AND CREATED BY THAT CERTAIN ROADWAY AND MAINTENANCE EASEMENT BY AND BETWEEN NEW COMMUNITY CHURCH OF THE PALM BEACHES, INC. AND HERBERT F. KAHLERT, PERSONAL REPRESENTATIVE OF THE ESTATE OF HEINZ KAHLERT, AND KARL A. KAHLERT AND HERBERT F. KAHLERT, AS CO-TRUSTEES UNDER TRUST A OF THE WILL OF FRITZ M. KAHLERT RECORDED FEBRUARY 16, 1999 IN OFFICIAL RECORDS BOOK 10933, PAGE 1503, WHICH EASEMENT WAS ASSIGNED TO ACME IMPROVEMENT DISTRICT, A DEPENDENT SPECIAL DISTRICT EXISTING UNDER THE LAWS OF THE STATE OF FLORIDA BY ASSIGNMENT OF EASEMENTS RECORDED IN OFFICIAL RECORDS BOOK 16026, PAGE 462, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, OVER, UNDER AND ACROSS THE LANDS DESCRIBED THEREIN.



Village of Wellington
Official Zoning Map





Wellington Village (FKA K Park)

STAFF REPORT

Petition Numbers: Rezoning
2025-0003-REZ
Ordinance No. 2026-06

Master Plan
2025-0001-MP
Resolution No. R2026-03

Conditional Use – School
2025-0001-CU
Resolution No. R2026-04

Conditional Use – Hotel
2025-0002-CU
Resolution No. R2026-05

Owner: Acme Improvement District
12300 Forest Hill Boulevard
Wellington, FL 33414

Applicants: Wellington Property Owner LLC
360 S Rosemary Avenue
Suite 800
West Palm Beach, FL 33401

Founders Acreage Wellington LLC
2054 Vista Parkway
West Palm Beach, FL 33411

Agent: Urban Design Studios
610 Clematis Street
Suite CU02
West Palm Beach, FL 33401

Site Address: 10400 Stribling Way

PCNs: 73-41-43-27-05-026-0011 and
73-41-44-24-06-003-0000

Future Land Use Designation (FLUM): Mixed Use

Current Zoning Designation: Community Facilities

Proposed Zoning Designation: Mixed Use Planned
Development (MUPD)

Acreage: 71.27 +/- acres

Project Manager:
Kelly Ferraiolo, Senior Planner
KFerraiolo@wellingtonfl.gov
(561) 753-5268

Location/Map: Southwest corner of Stribling Way and US
441/SR7.



Surrounding Uses:

| | Existing Use | Land Use | Zoning |
|-------|---|-------------------------------|-----------------------|
| North | Residential – Castellina | Residential C/Commercial | Residential PUD |
| South | Residential Unincorporated - Farmington | Residential Low Density (PBC) | Residential PUD (PBC) |
| East | Olympia Park | Community Facilities | Community Facilities |
| West | Residential – Oakmont Estates | Residential C | Residential PUD |

Boards, Council:

| Meeting | Notice Date | Meeting Date | Vote |
|-----------------|-------------|--------------|---------|
| PZAB | 1/6/2026 | 1/21/2026 | Pending |
| Village Council | 1/12/2026 | 1/27/2026 | Pending |
| Village Council | 1/26/2026 | 2/10/2026 | Pending |

REQUESTS:

The applicants are requesting a Rezoning, Master Plan, and two (2) Conditional Use approvals for the 71.27-acre property for the development of the property as a mixed-use project with multifamily residential, retail, hotel, restaurant, and a private school. Throughout this document, K Park MUPD and Wellington Village MUPD are used synonymously. The specific requests are presented below.

Ordinance No. 2026-06 - Rezoning (Petition 2025-0003-REZ): To amend the Zoning Designation of the property formerly known as K Park from Community Facilities (CF) and Community Commercial (CC) to Multiple Use Planned Development (MUPD).

(The Master Plan and Conditional Uses will be heard at the 2nd reading of the Rezoning application and have been provided for reference only):

Resolution No. 2026-03 - Master Plan (Petition 2025-0001-MP): To adopt the K Park MUPD Master Plan, which includes the following:

- Creates a 71.27-acre MUPD with the following pods and respective development program:
 - Pod A (Institutional)
 - 43.76 acres
 - School - 1,750 students/405,000 SF
 - Pod B (Commercial)
 - 24.05 acres
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 - 0.23 acres
 - Lift Station
- Adds three (3) access points on Stribling Way, one (1) access point along State Road 7, three (3) cross access points to Life Church property, and one (1) cross access point to the Farmington Estates and Family Vision for future cross access;
- Adopts a Project Standards Manual (PSM) specifically for the K Park MUPD; and
- Establishes Conditions of Approval for the development.

Resolution No. 2026-04 - Conditional Use (Petition 2025-0001-CU): Approves a Conditional Use for a private school (primary and secondary) with a maximum of 1,750 students and an outdoor sports and aquatic center within Pod A of K Park MUPD.

Resolution No. 2026-05 - Conditional Use (Petition 2025-0002-CU): Approves a Conditional Use of a 180-room hotel within Pod B of K Park MUPD.

SITE HISTORY:

On September 9, 2003, the Acme Improvement District (Acme) acquired 66.27 acres for \$8,457,000 (\$127,622/acre +/-). The additional acres for the 71.28-acre property were formerly road right-of-way south of Stribling Way and a strip of land providing access to State Road 7 on the south side of the property.

In February 2008, Wellington entered into a contract with Sunrise Development, Inc. to sell 5.3 acres of the site along State Road 7 for \$5,000,000 to develop an assisted living facility for seniors. As a part of that project, Wellington approved a Comprehensive Plan amendment (Ord. No. 2006-17) for the intended site in October 2006, changing the designations from Community Facilities to Community Commercial. However, the buyer elected not to proceed with the project and withdrew their offer to purchase on November 17, 2008.

In December 2010, Wellington began leasing the property to J. Alderman Farms, Inc. on an annual basis. Acme terminated the lease on January 31, 2026 due to the pending sale of the property.

In January 2025, Wellington executed a Purchase and Sale Agreement with Related Ross (aka Wellington Property Owner LLC) and Education in Motion (aka Founders Acreage Wellington LLC) to purchase the property upon receiving entitlements for a mixed-use development. In April 2025, Council approved a Village-initiated Comprehensive Plan Map Amendment for the properties, changing the land use designation of the entire 71.27-acre project to Mixed Use (MU).

ANALYSIS – REZONING AND MASTER PLAN:

As indicated above, the applicants are requesting: 1) a Rezoning to change Wellington's Official Zoning Map for the 71.27-acre property from CF to MUPD; and 2) approval of a Master Plan (MP) to create the K Park MUPD Master Plan for the development of a mixed-use project with multifamily residential, retail, hotel, restaurant, and a school. The Rezoning and MP applications were reviewed for consistency with both Wellington's Comprehensive Plan and Land Development Regulations (LDR) and are found consistent with the following:

A. That the proposed request is consistent with all elements of the Comprehensive Plan.

Land Use & Community Design Element

Objective LU&CD 1.1 Land Uses Establish compatible land use arrangements using land use categories appropriate for the neighborhoods, districts, commercial corridors, and equestrian areas.

The State Road 7 (SR 7) corridor is developed with a mix of uses, including residential, medical, office, retail, restaurant, and community facilities. The property fronts the SR 7 corridor and has been farmed, through a lease with Wellington, for several years. The request is reasonable and consistent with the land use characteristics along SR 7.

Policy LU&CD 2.1.4 Use Transitions Require appropriate transitioning, such as appropriate setbacks, buffers, and height limitations where commercial, industrial, or other high-intensity land uses are in proximity to single-family residential land use.

Appropriate setbacks, landscape buffers, and height limitations will be reviewed and applied during the development review process to mitigate any potential impact from the proposed development. The proposed uses will be situated in such a manner that the more intense uses are closer to SR 7.

Policy LU&CD 1.4.2 Mixed-Use Land Use Apply the Mixed-Use (MU) Land Use designation to new development or redevelopment of existing sites to accommodate a wide range of commercial and residential uses that are interconnected with a complementary mix of uses that are sensitive to the surrounding uses and to the natural environment, create neighborhoods with character, promote a walkable built environment, foster improved social and economic sustainability, and reduce infrastructure costs. Maximum FAR for all uses shall not exceed 0.50 and residential density shall not exceed 22 units per acre. The maximum FAR and density are measured for the gross MU acreage.

In January 2025, Wellington executed a Purchase and Sale Agreement with Related Ross and Education in Motion to purchase the property upon receiving entitlements for a mixed-use development. In April 2025, the Council approved a Comprehensive Land Use Map Amendment, which changed the Future Land Use Map Designation of the property from Community Facilities to Mixed Use.

The proposed development program offers a variety of uses (multifamily residential, retail, hotel, restaurant, and a school) that are interconnected, supporting a walkable built environment. The proposed FAR does not exceed the maximum 0.50, and the residential density of 3.02 du/ac is well below the maximum 22.0 du/ac.

Policy LU&CD 2.5.4 Entertainment and Gathering Places Encourage the expansion of family gathering places and entertainment venues along State Road 7.

The school, hotel, retail, and dining components encourage job creation and community interaction with open courtyards and plazas throughout Pod B, allowing for community programming and outdoor events.

Policy LU&CD 2.5.5 Housing Support mixed-use developments that include housing for all, but particularly young professionals, families, and seniors. This includes vertically integrated and non-vertically integrated mixed-uses.

The proposed residential units will be developed in two (2) phases. Phase 1 consists of 15 units that will be vertically integrated above the ground floor commercial uses. Phase 2 consists of 200 units that will be non-vertically integrated self-contained structures along Stribling Way and south of the hotel.

Policy LU&CD 3.1.1 Fiscal Impact Analysis (Commercial, Mixed-Use, Multi-Family) Requests for Land Use Map amendments or rezoning to a commercial, mixed-use, or multifamily designation shall be supported by fiscal impact analysis, which shows the balance of project costs to Wellington for providing municipal services, against income that the project will generate to Wellington.

Policy LU&CD 3.1.2 Demonstrated Need (Commercial, Mixed-Use, Multi-Family) Requests for Land Use Map amendments or Rezoning to a commercial, mixed-use, or multifamily designation shall be based on demonstrated needs and be located in such a manner to discourage strip development.

Policy LU&CD 3.1.3 Market Analysis (Commercial, Mixed-Use, Multi-Family) Requests for Land Use Map amendments or rezoning to a commercial, mixed-use, or multifamily designations shall be supported by a professionally prepared market analysis that demonstrates the need for the requested development at the proposed location, the types of marketable commercial activities or multi-family residential dwelling units there, and the projected market area of the proposed development.

A market study was conducted on behalf of Wellington by WTL+a Real Estate & Economic Advisors in 2021 and updated in 2022. The study included the Wellington Green Mall property and the SR 7 corridor, including K Park. The market study concluded that moderate-density, multifamily residential and a sustainable amount of additional retail uses offer the best opportunities for redevelopment. Palm Beach County has experienced sustained population growth of 1.18% per year for the last 21 years. Palm Beach County's population is expected to increase by more than 166,100 new residents over the next 14 years. Wellington has a historic annual growth rate of 0.81% per year. This would suggest a 2035 population of about 69,200 residents, reflecting the addition of 7,400 new residents and approximately 2,600 new households. Retail spending among Wellington households exceeds \$676.9 million per year. The study also concluded that the final approved redevelopment plan should provide flexibility in the overall mix of uses; the number and types of residential units to be provided; and appropriate other land uses that complement the site's residential potentials—selective retail uses that serve both on-site residents, workers, and visitors as well as area-wide consumers. This flexibility over time will allow for changes based on market conditions and development partner priorities, each of which will affect overall economic impacts and benefits to Wellington.

Parks and Recreation Element

Policy PR 1.1.7 Open Space & Recreation Requirements *Enforce open space and recreation requirements for development, such as:*

- 1. Amount of open space and recreation land and facilities required on a per unit basis.*
- 2. Types of land eligible to be counted toward open space and recreation calculations.*
- 3. Types of facilities eligible to be counted to offset open space and recreation requirements.*
- 4. Options for cash in lieu of land dedication.*
- 5. Design alternatives that will allow the clustering of development to avoid negative impacts on environmentally sensitive lands and encourage the preservation of open space.*

Per Section 6.5.2.E of Wellington's LDR, all Planned Developments that have a residential component shall be subject to land dedications or payment in lieu of fees, in addition to any impact fees required at building permit for parks and recreation facilities, as well as civic facilities. The developer shall provide five (5) acres of property for park and recreational facilities and one (1) acre of civic facilities, each 1,000 of the population proposed for the project. Based on the proposed 215 units, the project is required to dedicate 3.02 acres of park and recreational land and 0.6 acres of civic land. The 3.23-acre Pod O is designated as an Open Space pod consisting of a lake and public use trail. This area can be dedicated as a portion of the required Parks and Recreation facilities. The applicant can either dedicate additional land or pay in lieu, which will be included in the conditions of approval of the Master Plan Resolution No. R2026-03.

Education Element

Objective ED 3.2 School Facility Coordination *Continue to coordinate the development approvals and planning for school facilities.*

Community Partnership

Objective CP1.3 Land Development Coordination *Continue to implement the Village's development review processes for communication and coordination with local jurisdictions and other public agencies to address potential impacts of development.*

The applicant is required to notify the School Board of Palm Beach County of the proposed development and request a School Capacity Availability Determination (SCAD) letter. The School District reviews the potential impact the proposed development has on public school facilities and the available capacity and utilization percentage based on the evaluation of the most current School District's Five-Year Enrollment Projections. The residential portion will be developed in two (2) phases. A SCAD Letter and potential impact

were provided for each phase. Phase 1 proposes 15 units and Phase 2 proposes 200 units. Panther Run Elementary School, Polo Park Middle School, and Wellington High School will serve the development. The SCAD letters determined that there would be a negative impact on the public schools at the high school level. The table below identifies the number of students projected to be generated per school, the available capacity at the school, and the overall utilization percentage:

| | Panther Run Elementary School | Polo Park Middle School | Wellington High School |
|-------------------------------|-------------------------------|-------------------------|------------------------|
| Phase 1 – 15 units | | | |
| New Students Generated | 3 | 2 | 5 |
| Available Capacity | 187 | 274 | -118 |
| Utilization Percentage | 81% | 81% | 104% |
| Phase 2 – 200 units | | | |
| New Students Generated | 6 | 3 | 4 |
| Available Capacity | 181 | 270 | -122 |
| Utilization Percentage | 81% | 81% | 105% |

To address the school capacity impact of this proposed development at the district high school level, the School District has recommended that the development contribute to the District \$31,346.00 for Phase 1 and \$62,692.00 for Phase 2. The local government may incorporate this condition in the development approvals; therefore, this condition is included as part of the Master Plan Amendment, Resolution No. R2026-03 and shall be paid to the District prior to the issuance of the first building permit for each phase. The school capacity contribution is intended to supplement the required school impact fees imposed by Palm Beach County. Although the development program of the project includes a 1,750-student private school, this does not offset the required contribution to the SBPBC.

Mobility Element

Policy MB 1.1.2 Development Impact on Roadway LOS Development orders shall only be issued if the proposed development will not cause roadway levels of service to fall below the adopted LOS targets or ROW modifications are proposed to mitigate impacts and maintain the target LOS.

A Transportation Impact Statement was reviewed by Wellington's Traffic Engineer, Pinder Troutman Consulting, Inc, and provided as Exhibit F to the staff report. Palm Beach County Traffic Performance Standard approval letter will be required. Wellington's Traffic Engineer has determined that the proposed development meets the Traffic Performance Standards of Wellington. The following proportionate share payments will be required to be paid by the property owner prior to the issuance of the first building permit and have been included as conditions of approval of Resolution No. R2026-03:

- 0.56% of the total cost of the four-lane widening of Stribling Way from Forest Hill Boulevard to Fairlane Farms Road;
- 2.80% of the total cost of the four-lane widening of Stribling Way from Fairlane Farms Road to Castellina Way;
- 0.15% of the total cost of the four-lane widening of Big Blue Trace from Wellington Trace to South Shore Boulevard;
- 0.19% of the total cost of the ten-lane widening of Southern Boulevard from SR 7 to Lyons Road (payable to Palm Beach County);

- 47.6% of the total cost to construct an additional westbound left turn lane at the Forest Hill Boulevard and SR 7 intersection;
- 40.8% of the total cost to construct an additional northbound lane (one left turn lane and one thru lane) at the Stribling Way and Fairlane Farms Road roundabout; and
- 0.6% of the total cost to construct an additional eastbound thru lane at the South Shore Boulevard and Forest Hill Boulevard intersection.

In addition to the proportionate share payments, the property owner will be required to construct various improvements at their site driveways along Stribling Way and State Road 7. Construction shall begin prior to the first building permit and all improvements shall be completed prior to the issuance of the first certificate of occupancy. Detailed requirements per improvement have been included as conditions of approval of Resolution No. R2026-03, but are summarized below:

- Signalized intersection or roundabout at full access driveway on Stribling Way with appropriate turn lanes;
- An eastbound right turn lane and a westbound left turn lane shall be constructed at full access driveway on Stribling Way;
- An eastbound right turn lane shall be constructed at the right in/right out driveway on Stribling Way;
- A southbound right turn lane at the right in/right out driveway on SR 7, approximately 514 feet south of Stribling Way.
- Signalized intersection at full access driveway on SR 7, approximately 1,029 feet south of Stribling Way and appropriate turn lanes
- Close the existing right-in/right-out/left-in driveway on the west side of SR 7 and provide cross-access with existing church.
- Remove the existing directional median opening on SR 7

The property owner will also be required to construct improvements at the SR 7 and Stribling Way intersection that include the addition and extension of turn lanes for better functionality within that intersection. Construction of the widening of Stribling Way from Castalina Way to SR7 will be completed by the property owner.

Policy MB 2.1.1 Access Management Enforce standards and a review process to control roadway access points, on-site traffic flow, and on-site parking for access management, including the requirement of joint access drives for adjacent uses, the spacing and design of driveway curb cuts, the spacing and design of median openings, the provision of service roads.

Policy MB 2.1.3 Connectivity Require proposed new developments to include internal connections with existing and proposed collectors and adjacent properties to increase connectivity and reduce traffic impacts on Wellington's major thoroughfares.

As mentioned previously, various improvements are required at the project site driveways that include median closures and turn lane expansions prior to issuance of the first building permit. The development will also have cross-access connection to the Life Church property to the south, pending agreement, and

realignment of the shared driveway to allow for a signalized access point. This driveway will be used as the main access point for the secondary school, Life Church, and a majority of patrons coming from the south. Staff is requesting that future cross-access points be added to the master plan connecting the Family Vision and Farmington Estates properties to the south, which will encourage the use of the signalized intersection at the Life Church entrance.

Policy MB 2.1.2 On-site Circulation & Parking *On-site circulation and parking shall be designed to ensure adequate circulation aisles, turning radii and parking spaces.*

A shared parking study was provided and analyzed by Wellington's Traffic Engineer and staff. The LDR requires 1,204 parking spaces for the entire Pod B. However, the parking study determined that based on time of day, rate for non-captive and other adjustments, Pod B will have a maximum parking demand of 954 spaces. The developer plans to utilize valet parking for double stacking and a shared parking agreement with the neighboring Life Church. Two (2) contingency parking plans were provided, which include a shared parking agreement with Wingrove Academy (Pod A) within the adjacent future site for ball fields and the construction of a parking garage within Pod B. A monitoring condition was included in Resolution No. R2026-03, which requires the submission of an annual parking utilization monitoring study for a period of 10 years after the issuance of a certificate of occupancy for more than 150,000 square feet. If a parking deficiency is identified, the property owner will be required to construct the contingency parking within 18 months.

A circulation plan has been provided and included as Exhibit D, which analyzes both Pod A and Pod B's unique circulation. Pedestrian and vehicular connectivity has been provided between each pod and neighboring properties, pending cross-access agreements.

Capital Improvements Element

Policy CI 1.4.1 Development Impacts *Any new construction, redevelopment, or renovation projects that create the need for new or expanded public facilities shall be required to pay their proportional share of the costs of the required new or expanded facilities based upon the impacts of the project. Any infrastructure required solely for the proposed development shall be funded entirely by the proposed development, such as the extension of water or sanitary sewer lines.*

The developer is responsible for the funding and construction of all improvements/upgrades that the Wellington Utility Department determines are necessary for existing lift stations, water distribution systems, sanitary systems, and force main systems because of impacts to existing systems by the proposed project development plan.

Also, as previously mentioned, conditions of approval are provided in Resolution No. R2026-03 requires the developer to make proportionate share payments for future improvements to various impacted roadway segments.

B. Whether the proposed request is not in conflict with any portion of Wellington's LDR and is consistent with the purpose and intent of the LDR.

The proposed K Park MUPD zoning meets the minimum lot size and building coverage outlined in Table 6.3-1. Section 6.5.6 of Wellington LDR provides requirements for an MUPD. Based on the gross acreage, the MUPD is required to have a minimum of five (5) different use types. K Park is proposing a mix of School, Residential, Commercial, Open Space, and Office, which will meet this requirement. The proposed Master Plan and Site Plan incorporate an entry boulevard, aka main street, with building placement along the boulevard. Vertical integration of uses is provided in the site design, and inter-connectivity of all uses is provided throughout the site. No uses will exceed 60% of the total MUPD acreage. Community benefits

include public plazas and gathering places activated for various uses such as community events, entertainment programming, and leisure suitable for all ages. The MUPD is proposed to be designed and built to meet a minimum of LEED Silver or comparable building standards across the entire project.

A Project Standards Manual was provided and included as Exhibit C of Resolution No. R2026-03. A PSM is the governing document, in addition to the master plan and site plan, for a planned development that provides all regulations for the proposed development as required by Section 6.5.2.C and 6.5.4.E of the LDR. The PSM is adopted by resolution and provides design standards, landscape requirements, circulation, setbacks, etc. for a planned development. The purpose and intent of the MUPD is to provide flexibility to encourage distinctive design that results in the efficient use of land, parking, landscaping, open space, access, architectural compatibility, and adaptable space for future uses and sustainability. The developer has requested the following flexible regulations and deviations within the PSM. Staff has provided an analysis and recommendation for each request:

Pod A Deviations:

1. **Required Parking:** A deviation is requested from the required parking as outlined in the School Operational Plan prepared by Simmons and White and included in the Conditional Use request for the school. The applicant proposes a reduced parking rate based on the ITE Parking Generation and the Urban Land Institute's Shared Parking. The parking rates are based on faculty/staff members, visitors, and students in grades 10 and up. The proposed rates take into account that the athletic fields and indoor activity areas will not be used when school is in session. There are no objections to this request.



Image A: Parking Stall Width at 9 Feet

2. **Required Bicycle Parking:** Section 7.5.1.D requires one (1) bicycle parking space per 2,000 square feet of gross floor area, which would include all classrooms, theater, indoor athletic facilities, etc. Based on the school's proposed square feet, a total of 203 bicycle parking spaces would be required. The applicant is proposing to reduce the amount of required parking to 10% of the anticipated middle and high school students which would result in 108 bicycle parking spaces. There are no objections to this request.

3. **Parking Stall Width:** Required parking stall width is 9.5 feet. The proposed Pod A PSM proposes a width of 9' within the temporary east and west parking lots (Phase 1 only), and the parking garage (Phase 2) as shown in Image A. There are no objections to this request in areas for employees or students. Areas of high turnover shall remain 9.5 feet in width. The north parking lot is identified as a high turnover area, as it is intended for visitor parking.



Image B: Parking Drive Aisle Width

4. **90-Degree Parking Drive Aisle Width:** A one (1) foot reduction in drive-aisle width is requested to allow "for more efficient site planning and enabling the preservation of pedestrian oriented open-spaces and landscaping without compromising the circulation and functionality of parking areas"

within the area shown in Image B. The reduced width also serves as a traffic calming technique for the MUPD. There is no objection to this request.

| SCHOOL - 405,000 GSF | | | |
|------------------------------------|----------------|---------------|----------------|
| ZONING | REQUIRED COUNT | PROPOSED SIZE | PROPOSED COUNT |
| 15' X 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' X 55' @ 1/50,000 SF OVER 100K | 6.1 | | |
| TOTAL | 13 | 15'X50' | 3 |

Image C: Loading Spaces

5. **Loading Space Quantity and Loading Space Dimensions:** Loading space requirements are based on the gross square footage of a project versus the number/types of uses. Based on the size of the school, the code requires 13 loading spaces, a minimum of 15' x 55' in size. The applicant has relied on established use patterns and knowledge of standard delivery truck sizes that serve the school use and is requesting a total of three (3) loading spaces 15' x 50' in size, which will be centrally located with Pod A. There is no objection to this request.
6. **Fence/Wall Location within Landscape Buffers:** Section 7.8.5.C.7 of Wellington's LDR states that if a fence or wall is required within a landscape buffer, the landscaping shall be located between the fence or wall and the adjacent property or right-of-way. The proposed PSM proposes that the fence or wall may be located at the property line where the adjacent property has an existing buffer for the west and south property lines for Pod A. There is no objection to this request.
7. **Outside Activity Area Buffers:** Section 6.2.2.F.5 of Wellington's LDR requires schools that have outside activity areas be buffered a minimum of 50 feet in width from adjacent residential. The athletic facilities proposed in Pod A are situated towards the southern portion of the site. A 10-foot-wide Type A/F landscape buffer, which includes one (1) shade tree per 30 linear feet, a continuous hedge, and a fence or wall, is currently proposed along the entirety of the south property line with plantings both within this perimeter buffer and along the north side of the driveway. Additionally, a 25-foot-wide landscaped buffer exists beyond the southern property line of Pod A on the adjacent Farmington Estates neighborhood. There is no objection to this request as the intent of the outside activity area buffer requirement will be met through the existing landscape on the adjacent Farmington Estates property and the proposed site design.
8. **Type A Buffer (North adjacent to Church parcel):** A modified 10-foot-wide Type A buffer, which includes the elimination of the required shade trees for every 30 linear feet, is requested along the north side of the Pod A access easement south of the Church site (shown as the red line in Image D). The applicant is requesting the deviation due to the existing overhead utility lines along the access strip, which impact the ability to properly plant shade trees at the required height established in the LDR. FPL imposes "Right Tree, Right Place" requirements, which limit the type of species and height that are planted in areas that are beneath the overhead lines. As such, the proposed deviation requested allows compliance with FPL requirements. A continuous hedge and Native ornamental grasses will still be planted. There is also existing buffer material on the church property that further screens the access drive within this area. There is no objection to this request.
9. **Type A/F Buffer (South Property Line):** A landscape buffer deviation is requested to eliminate the required fence within Type A/F buffer for the eastern portion of the southern property line between the proposed control access gates and SR 7 (shown as blue line in Image D). A modified Type A/F buffer is provided along the south side of Pod A, consisting of a proposed masonry wall at a minimum of 6 feet in height located at the property line and terminating at the southern gate access into Pod



A. The eastern portion of the southern property line, where a wall is not proposed, does not contain any building square footage or active uses; it consists solely of a driveway that provides access to the school site from the roundabout located on the Church property and landscape open space consisting of a dry detention swale. Additionally, an existing landscape buffer along the northern property line of Farmington Estates, the residential community to the south, provides further screening for the areas where a wall is not proposed. The applicant is also proposing a masonry wall in lieu of the required fence, providing a higher quality aesthetic and improved site security compared to the fence that is typically required in a Type A/F buffer. There is no objection to this request.

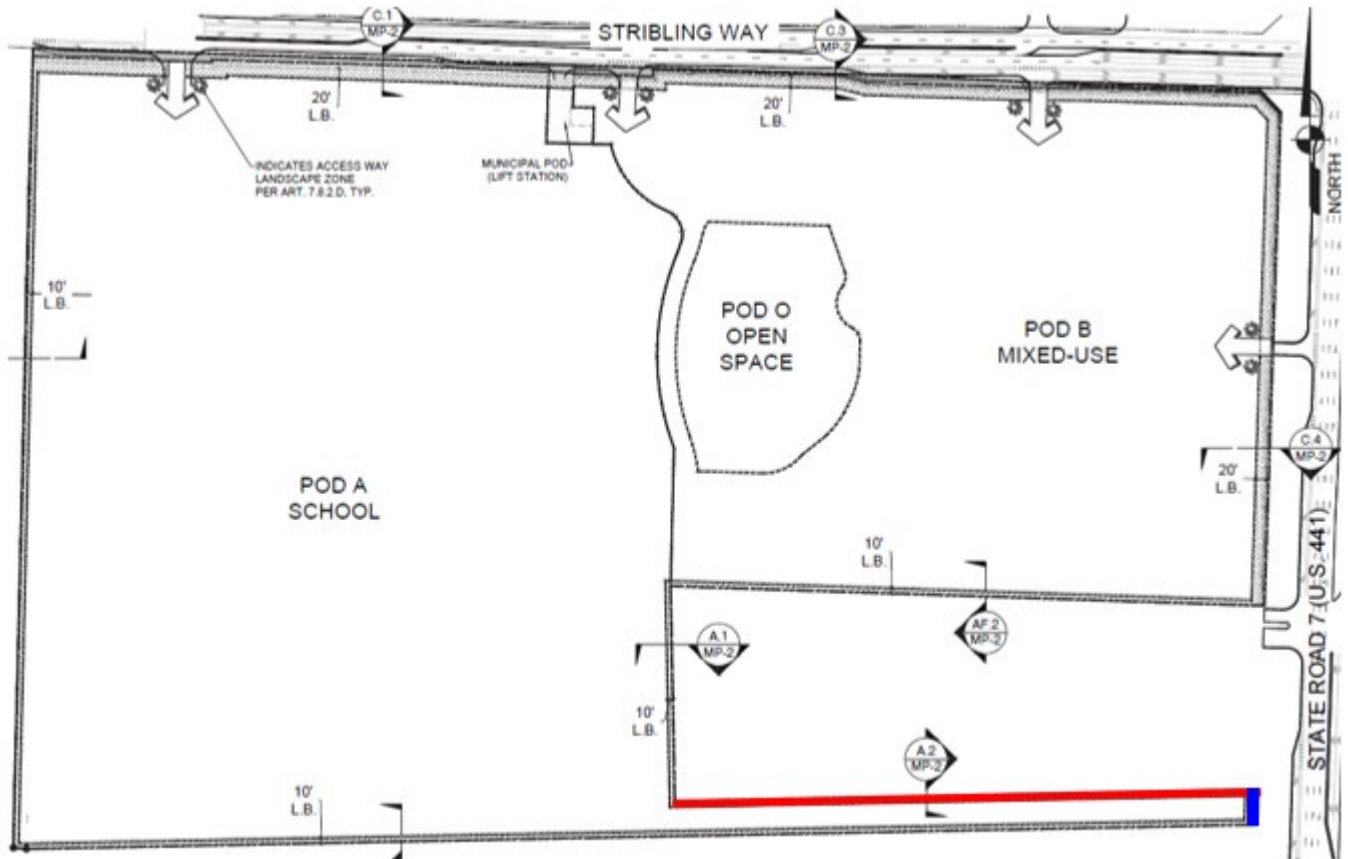


Image D: Landscape Buffers

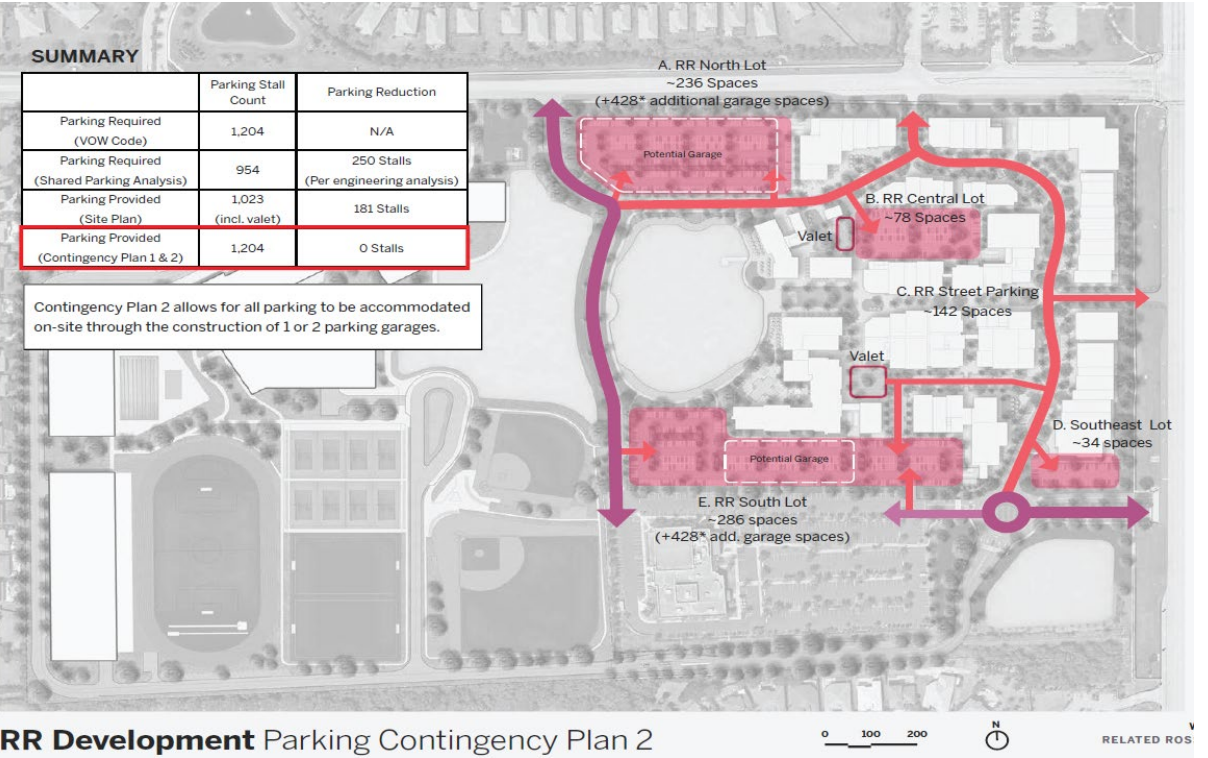
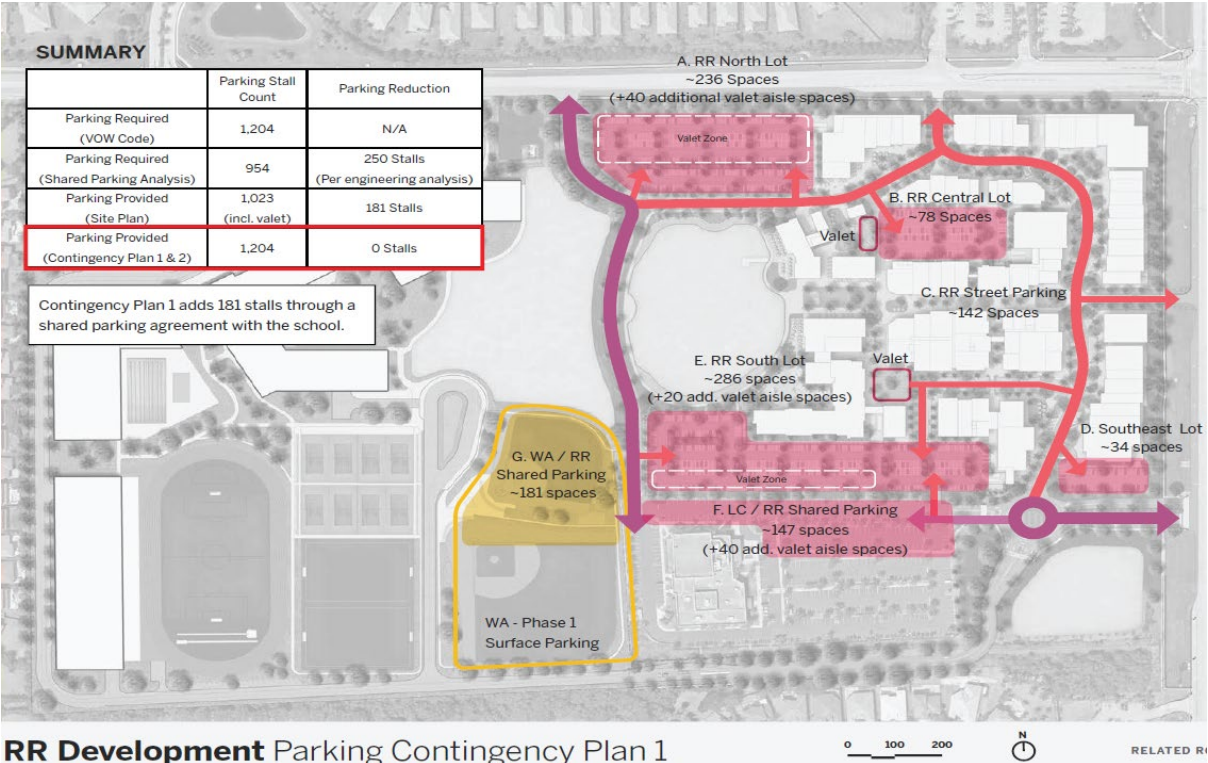
10. **Landscape Buffer Perimeter Path:** Per Section 6.5.4.7 of Wellington's LDR, a minimum width of 10 feet shall be added to the minimum width of a perimeter landscape buffer to accommodate the path's construction. This requirement would apply to the north and eastern perimeter of the MUPD where adjacent to Stribling Way and State Road 7. The Pod A buffer adjacent to Stribling Way proposes a buffer width of 20 feet and a 10-foot perimeter utility easement. In lieu of an additional 10 feet, the project proposes to maintain the existing sidewalk widths within the adjacent ROW's, as there is an existing pathway. There is no objection to this request.
11. **Foundation Planting:** Per Section 7.8.2.B.2 of Wellington's LDR, foundation plantings shall be provided along the front, side, and rear facades of non-residential structures with a minimum depth of five feet for each building story or a maximum of 25 feet. The combined length of the required foundation planting shall be no less than 40 percent of the total length of the applicable side of the structure. The proposed PSM proposes a deviation to provide the equivalent square footage of planting area as required, provided at or near the vicinity of the building to allow for an interconnected

network of walkways, courtyards, and circulation for the site to accomplish the “campus” feel while still providing the equivalent plantings from a square footage standpoint. There is no objection to this request as it meets the intent of the foundation planting provision.

12. **Internal Lot Trees:** A deviation from the required internal lot tree requirement is requested. Per Section 7.8.2.B.1. of Wellington’s LDR, one (1) tree and three (3) shrubs shall be planted for every 1,500 square feet of a non-residential lot, not including trees, shrubs, or hedges required to be planted in perimeter buffers and vehicular areas. In addition, MUPDs are required to exceed the landscape standard of the LDR by a minimum of 30% of the general standard (both quantity and size). Given the acreage of the perimeter buffer, lakes, and sports fields, these areas were allowed to be excluded from the internal lot area calculation, resulting in a reduced requirement of 1,403 trees. Pods A and B are proposed to include a total of 1,050 trees throughout the site, resulting in a 25% reduction of the base requirement. This quantity does not account for the additional 30% required for a MUPD. As a reduction was already provided by removing areas from the calculation, a condition of approval is recommended that requires the applicant to either amend the landscape plan and include the required quantity, or contribute an in-lieu fee of \$600 per tree (\$600 x 353) prior issuance of the first building permit.

Pod B Deviations:

1. **Required Parking:** A deviation to the required parking for Pod B is requested. The LDR requires a total of 1,204 parking spaces, as both the retail and office parking rates are based on net square feet and not gross square feet. A parking study was prepared by Simmons & White and determined that the anticipated maximum parking demand for Pod B is 954 spaces (includes an additional 10% of spaces as a conservative buffer). Two (2) contingency parking plans were provided, shown in the images below, which include a shared parking agreement with Wingrove Academy (Pod A) within the adjacent future site for ball fields and the construction of a parking garage within Pod B. As a result, a monitoring condition is included in Resolution No. R2026-03, which requires the submission of an annual parking utilization monitoring study for a period of 10 years after the issuance of a certificate of occupancy for more than 150,000 square feet. If a parking deficiency is identified, the property owner will be required to construct the contingency parking within 18 months.



2. **Parking Stall Width:** Required parking stall width is 9.5 feet. The proposed Pod B PSM includes a parking stall width of 9 feet, which is not considered appropriate for high turnover areas. The areas considered in the “high Turn-Over Zone” shown in Image E are anticipated to be the most utilized and desirable parking spaces for Pod B and are located along the Main Street corridor and the central parking lots closest to the storefronts. The parking lots west of Building A1 and south of the Hotel building are located in non “High-Turnover Zones” and are the subject of the deviation request to allow parking spaces 9 feet in width. There are no objections to this request.

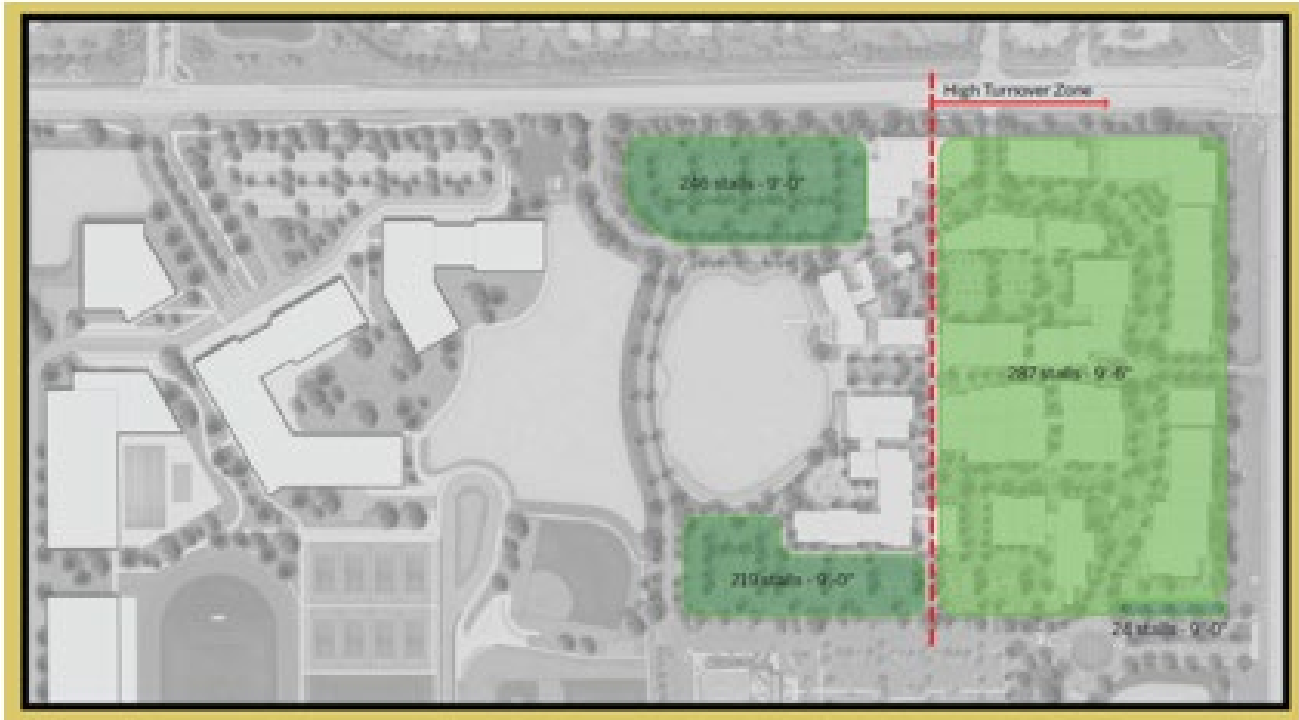


Image E: High-Turnover Zone

3. **Required Bicycle Parking:** Section 7.5.1.D requires one (1) bicycle parking space per 2,000 square feet of gross floor area. Based on the non-residential square feet of Pod B, a total of 283 bicycle parking spaces would be required. The applicant is proposing to exclude the square footage of the hotel as guests typically arrive by vehicle or some other form of transportation. This would reduce the required bicycle parking to 200 spaces. A few bicycle parking spaces will be required by the hotel for employee use which will be confirmed during site plan review. There are no objections to this request



Image F: Parallel Parking

4. **Parallel Parking Stall Width:** A deviation to the required parallel parking stall width within Pod B is requested. Section 7.5.4 requires parallel parking spaces to be 10 feet in width; however, the proposed PSM establishes a minimum width of 9 feet. Parallel parking spaces are proposed along both sides of the driveway extending from the State Road 7 access, as well as along the driveway providing access to the hotel, as shown in Image F. A reduction from a 10-foot to a 9-foot parallel parking space is appropriate within the mixed-use village context due to the

pedestrian-oriented, low-speed design of the internal paved streets. There is no objection to this request.

5. **90-Degree Parking and Angled Parking Drive Aisle Width:** A one (1) foot reduction in drive-aisle width is requested throughout all of Pod B to allow for more efficient site planning and enabling the preservation of pedestrian-oriented open spaces and landscaping, all while remaining safe and efficient circulation. The reduced width also serves as a traffic calming technique for the MUPD. There is no objection to this request.
6. **Loading Space Quantity & Loading Space Dimensions:** Based on the gross square footage, Pod B is required to provide a total of 21 loading spaces 55' x 15' in size. The applicant is requesting a reduction of both size and quantity of loading spaces as shown in Image F below. Lerch Bates provided a Building Logistics Analysis Support Letter (Exhibit N), which supports the need for the reduction. The loading zones are placed in centralized locations to service each building. There is no objection to the request.

| HOTEL - 175,000 GSF | | | |
|-------------------------------------|----------------|----------------------|----------------|
| Zoning | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' x 55' @ 1/50,000 SF OVER 100K | 2 | | |
| TOTAL HOTEL | 9 | 14' X 50' | 2 |
| RETAIL + F&B + OFFICE - 355,000 NSF | | | |
| ZONING | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | 14' X 40' | 2 |
| 15' x 55' @ 1/50,000 SF OVER 100K | 5 | 14' X 50' | 9 |
| TOTAL RETAIL + F&B + OFFICE | 12 | | 13 |
| TOTAL REQUIRED POD B | 21 | TOTAL PROPOSED POD B | 13 |

Image G: Pod B Loading Space Analysis

7. **Buffer Type A/F (South):** A landscape buffer deviation is requested to eliminate the required fence within Type A/F buffer for the southern property line adjacent to the Life Church property (shown as red line in Image H) at the request of Life Church. There is no objection to the request.

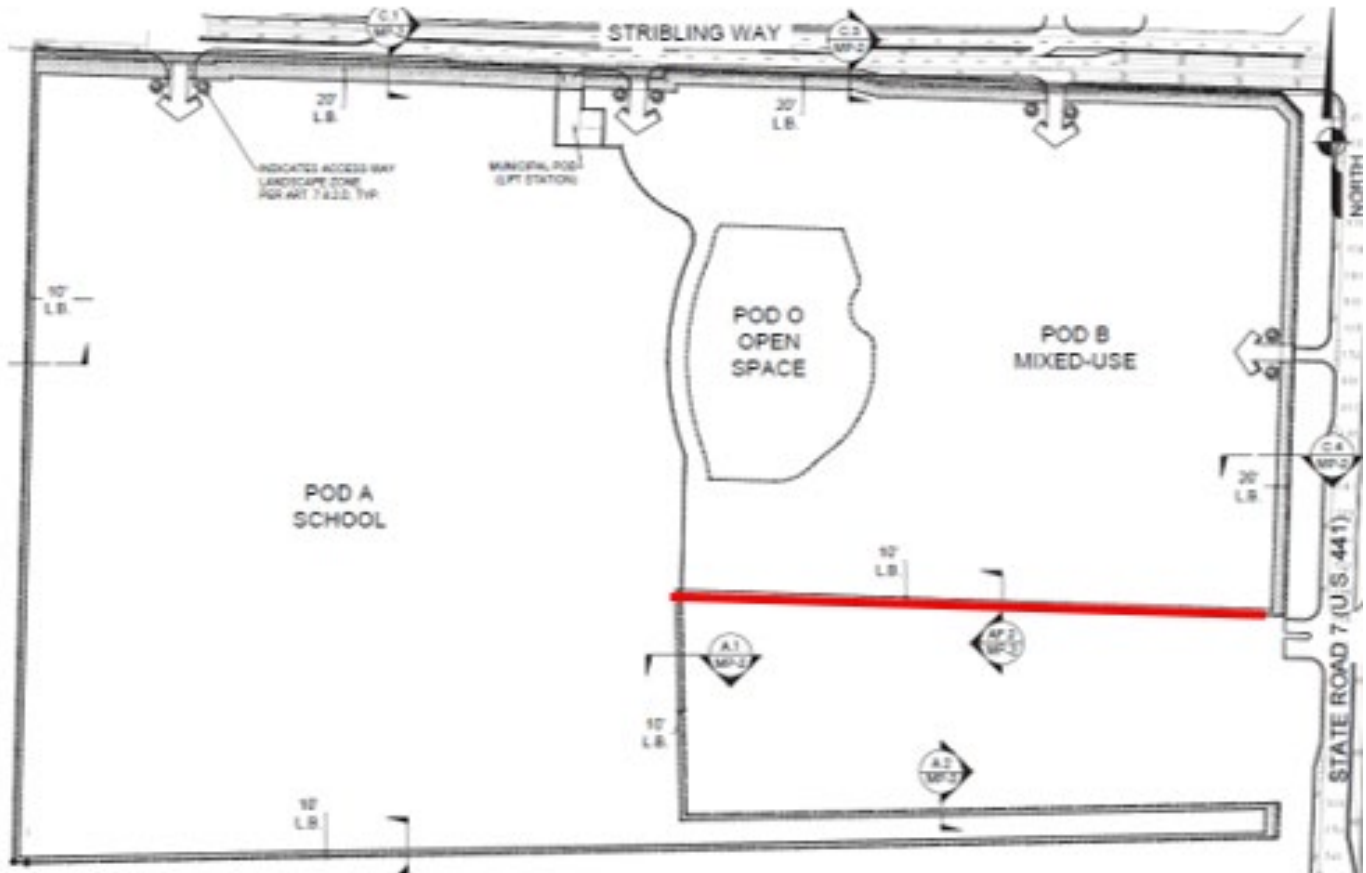


Image H: Pod B Landscape Buffer

8. **Landscape Buffer Perimeter Path:** Per Section 6.5.4.7 of Wellington's LDR, a minimum width of 10 feet shall be added to the minimum width of a perimeter landscape buffer to accommodate the path's construction. Pod B provides a minimum 8 feet clear pedestrian path interior to the site along the storefronts of the mixed-use buildings. The pedestrian circulation interior to Pod B is also designed to connect to the existing pathways along State Road 7 and Stribling Way. There is no objection to this request.
 9. **Foundation Planting:** Per Section 7.8.2.B.2 of Wellington's LDR, foundation plantings shall be provided along the front, side, and rear facades of non-residential structures with a minimum depth of five feet for each building story or a maximum of 25 feet. The combined length of the required foundation planting shall be no less than 40 percent of the total length of the applicable side of the structure. The PSM proposes a deviation to provide the equivalent square footage of planting area as required, provided at or near the vicinity of the building, to allow for an interconnected network of walkways, courtyards, and circulation at the site while still providing the equivalent plantings from a square footage standpoint. There is no objection to this request as it meets the intent of the foundation planting provision. This deviation does not apply to buildings that get approval for a building height exceeding 35 feet.
 10. **Internal Lot Trees:** Refer to the explanation in Pod A above.
- C. **Whether the proposed request is compatible and consistent with existing uses and the zoning surrounding the subject land and is the appropriate zoning district for the land.**

The proposed rezoning is compatible with the existing uses surrounding the subject property and along State Road 7, which includes a mix of commercial to the north, residential to the west and south, and institutional uses to the south and east. Although the current zoning district of Community Facilities is consistent with all land use categories, the district is established for public and institutional facilities such as schools, government offices, assembly, utility services, and other infrastructure-related facilities. The MUPD district is a more appropriate and compatible zoning district for the proposed mixed-use development.

D. That the design of the proposed request minimizes adverse effects, including visual impact and intensity of the proposed use on adjacent lands.

The overall design minimizes adverse effects, including visual impact and intensity of the proposed use on adjacent lands. Pod A (school) is located on the western portion of the MUPD adjacent to Oakmont Estates and Farmington Estates. These developments have existing mature landscape buffers that include shade trees and a wall, creating a thick screen. An additional landscape buffer will be required to be installed along all property boundaries, even where neighboring buffers exist. Commercial and hotel uses (Pod B) are placed along State Road 7, where higher intensities are more appropriate. Commercial uses will not be adjacent to existing residential land uses. The proposed buildings throughout will be reviewed for compatibility with the architectural character and scale of the existing surrounding buildings and will be required to provide increased minimum height standards for trees/palms that will help to mitigate any visual impacts.

D. Whether there exist changed conditions which require a rezoning.

In January 2025, Wellington executed a Purchase and Sale Agreement with Related Ross and Education in Motion to purchase the property upon receiving entitlements for a mixed-use development. In April 2025, Council approved a Comprehensive Plan Map Amendment for the K Park properties, changing the land use designation of the project to Mixed Use (MU). The land use change was necessary to support the development of the property as a proposed mixed-use project with multifamily residential, retail, hotel, restaurant, and a school. The MUPD district is a more appropriate and compatible zoning district for the proposed mixed-use development.

E. Whether and the extent to which the proposed amendment would result in significantly adverse impacts on the natural environment.

The proposed development will not result in significantly adverse impacts on the natural environment. The site is currently used for low-intensity agricultural purposes and has been cleared of any significant vegetation or tree plantings that would be impacted by the proposed development.

F. That the proposed request would result in a logical and orderly development pattern.

The proposed master plan results in a logical and orderly development pattern by converting farmland into a mixed-use community with a complementary selection of uses. By clustering commercial uses along a major transportation corridor and placing institutional and residential uses internally with appropriate transitions, the plan avoids land use conflicts and supports efficient access to goods, services, schools, and employment. Interconnectivity is provided amongst neighboring properties to better facilitate circulation throughout the site and along the adjacent roadways, including a shared access drive to a signalized light. The site was identified in the SR7 Corridor study initiated by Wellington in 2006 in conjunction with other local and state organizations as a significant vacant property along SR7 that could be an area for business, job creation, and residential development. Contrary to the name K Park, the vision was always to develop this site as a mixed-use development.

G. That the proposed request is consistent with applicable neighborhood plans.

Not applicable as there is no neighborhood plan for this area.

H. Whether the proposed complies with Article 2, Concurrency Management.

The project is proposed within an existing Urban Service Area, with water and sewer connections available in the surrounding area. On/off-site improvements/upgrades to existing lift stations, water distribution systems, sanitary systems, and force main systems will be necessary to support the project, and conditions of approval have been included in the Master Plan Resolution.

As previously indicated, the School District has recommended that the development contribute to the District \$31,346.00 for Phase 1 and \$62,692.00 for Phase 2, which have been included in the conditions of approval of the Master Plan Resolution.

Wellington's Engineering and Utilities Departments have reviewed the request for compliance with requirements for public facilities. Wellington's Traffic Consultant and Palm Beach County's Traffic Engineer have determined this project meets the Traffic Performance Standards and adopted Level of Service. Roadway improvements will be required and included in the Conditions of Approval of the Master Plan resolution.

ANALYSIS – CONDITIONAL USE (SCHOOL):

The applicant is requesting a Conditional Use to allow a private school (primary and secondary) with a maximum of 1,750 students and an outdoor sports and aquatic center within Pod A of K Park MUPD. The Conditional Use application was reviewed for consistency with both Wellington's Comprehensive Plan and Land Development Regulations (LDR) and is found consistent with the following:

A. The request is consistent with the purposes, goals, objectives, and policies of the Comprehensive Plan:

The CU is consistent with the Comprehensive Plan as explained above in the Comprehensive Plan staff analysis.

B. The request is in compliance with Section 6.2 (Supplementary Regulations) of the Land Development Regulations (LDR):

5. School, Secondary or Primary:

- a. For primary schools, the proposed site shall have direct access to at least a minor collector road. Outdoor recreational facilities and similar support facilities shall be located and buffered on the proposed site to minimize impacts on the adjacent properties.***

A School Traffic Operational Management Plan was submitted and reviewed by Wellington's Traffic Engineer. The proposed circulation shows direct access to the primary school, grades K-1 on the northern portion of the campus, is from Stribling Way, which is a minor collector roadway. Grades 2-4 will be from a signalized shared access drive on the Life Church property to SR 7, which is a principal arterial roadway. Egress will be permitted on both Stribling Way and SR 7. The outdoor recreational facilities for the primary school are located internal to the campus and buffered by buildings, and will not impact the adjacent properties.

- b. For secondary schools, the proposed location shall have direct access to at least a major collector road. Stadiums, outdoor recreational facilities, and similar support facilities shall be located and buffered on the proposed site to minimize impacts on adjacent properties.***

The School Traffic Operational Management Plan provided shows that access to the secondary school, located on the southern portion of the campus, will be from a signalized shared access drive on the Life Church property to SR 7, which is a principal arterial roadway. Egress will be permitted on both Stribling Way and SR 7. Outdoor recreational facilities are located on the southern boundary adjacent to Farmington Estates' retention area and main access roadway. Fields will be separated approximately 200 feet from a residential structure, and landscaping will be required in addition to the mature landscape buffer that exists within Farmington Estates.

- c. Prior to approval of a building permit, a pedestrian access/bike path and cross-walk plan shall be submitted by the applicant showing access to the school site from surrounding neighborhoods. This system shall be integrated with existing or proposed pedestrian/bike path systems in the area, and shall be subject to the approval by the Wellington Engineer.***

A significant number of pedestrians or bicyclists entering the site are not expected due to the school being a private and its location on SR 7 and Stribling. Pedestrian pathways have been provided throughout the site to connect to the multi-purpose pathway along Stribling Way and within Pod B.

- d. Vehicular circulation shall be designed to provide an independent traffic flow for school employees, visitors, and deliveries from the bus loading area, and parent drop-off area, where applicable.***

The School Traffic Operational Management Plan provides a detailed plan for all traffic flow including buses, students, parents, and employees. Crossing guards and traffic control personnel will be strategically placed throughout the campus to ensure safe and organized circulation. School start times will be staggered to reduce the traffic impact around the campus.

- e. Dumpster and trash receptacles shall be located a minimum of 100 feet from residential property and screened from view with a six (6) foot solid wood fence and hedge combination unless part of a pre-existing commercial development.***

The dumpster and loading areas are located near the bus loop central to the campus. Adequate screening will be required during site plan review.

- f. A six (6) foot high security fence shall be installed around the entire perimeter of the outside activity area to limit access.***

The proposed site plan illustrates a six (6) foot high security fence around the entire perimeter of the outside activity area.

- g. Outside activity areas shall be located away from adjacent residential areas, whenever possible. Outside activity areas located adjacent to developed residential properties because of site design constraints shall provide a 50-foot buffer. This landscape buffer shall be supplemented with a six (6) foot high hedge or hedge/berm combination and a double row of native canopy trees, spaced an equivalent of one (1) tree per 20 linear feet of landscape buffer.***

The outside activity area is located on the southern boundary adjacent to Farmington Estates' retention area and their main access roadway. Although a 50-foot-wide buffer is not provided, the intent of this provision is met as the outside activity area is screened from view by a 10-foot-wide Type A landscape buffer with a fence and additional trees line the north side of the access road to the school adjacent to Farmington Estate's existing landscape buffer creating an area wider than 50 feet.

C. The request will ensure general compatibility with adjacent properties and other properties in the district.

The proposed private school is compatible with the surrounding residential neighborhood as schools are a common use that neighbor residential communities. The operational characteristics of the school include staggered start times, controlled access, and centralized outdoor play areas, bus loops and loading areas will further compatibility with the surrounding residential properties.

D. The design of the request will minimize adverse effects, including visual impact and intensity on the proposed use on adjacent lands.

The school campus has been designed to mitigate any adverse impacts to adjacent properties through the use of landscape buffering, building orientation, and separation from residential uses. Photometric plans will be reviewed during the site plan process to ensure lighting will not spill over to the neighboring properties. Traffic improvements will be required as a result of the K Park project, resulting in improved intersection circulation for not just for the project, but for the surrounding area. Traffic improvement conditions will be included in the Resolution of the Master Plan.

E. That satisfactory provisions have been made for public facilities.

The project is proposed within an existing Urban Service Area, with water and sewer connections available in the surrounding area. On/off-site improvements/upgrades to existing lift stations, water distribution systems, sanitary systems, and force main systems will be necessary to support the project and necessary conditions of approval have been included in the Master Plan Resolution.

F. That the design of the proposed request will minimize environmental impacts, including but not limited to water, air, stormwater management, wildlife, vegetation, wetlands and natural functioning of the environment.

No adverse impacts to the natural environment are expected to occur as a result of the proposed modifications. The developer shall obtain all necessary permits or permit modifications from all agencies with jurisdiction for future improvements.

ANALYSIS – CONDITIONAL USE (HOTEL):

The applicant is requesting a Conditional Use to allow a Conditional Use of a 180-room hotel within Pod B of K Park MUPD. The Conditional Use application was reviewed for consistency with both Wellington's Comprehensive Plan and Land Development Regulations (LDR) and is found consistent with the following:

A. The request is consistent with the purposes, goals, objectives, and policies of the Comprehensive Plan:

The CU is consistent with the Comprehensive Plan as explained above in the Rezoning and Master Plan staff analysis.

B. The request is in compliance with Section 6.2 (Supplementary Regulations) of the Land Development Regulations (LDR):

- a. A hotel may have an accessory lounge not to exceed to 25% of the gross floor area of the hotel, exclusive of parking.***

A condition has been added to the Resolution that limits the size of the accessory lounge within the hotel, which will be confirmed during the site plan review.

b. Must be located on a minimum of two (2) acres.

The proposed hotel is located within Pod B of K Park, which is 24.05 acres in size.

C. The request will ensure general compatibility with adjacent properties and other properties in the district.

The proposed master plan and site plan shows the hotel located within Pod B of K Park just north of Life Church over 600 feet away from residential uses. Service areas will be in a centralized location not to interfere with the surrounding properties, buildings, or uses. A hotel use is compatible with the proposed Mixed Use zoning request and is a frequent use seen within a vertically integrated mixed-use development.

D. The design of the request will minimize adverse effects, including visual impact and intensity on the proposed use on adjacent lands.

The hotel is proposed to be the focal point of the Mixed-Use development, and potentially the tallest building within the project. Service areas will be in a centralized location not to interfere with the surrounding properties, buildings, or uses. Architectural guidelines are provided in the PSM, however, approval by the Architectural Review Board will be required for all elevations, colors, materials, and signage and approval by Council for buildings taller than 35 feet. Traffic improvement conditions will be included in the Resolution of the Master Plan.

E. That satisfactory provisions have been made for public facilities.

The project is proposed within an existing Urban Service Area, with water and sewer connections available in the surrounding area. On/off-site improvements/upgrades will be necessary to existing lift stations, water distribution systems, sanitary systems, and force main systems to support the project and necessary conditions of approval have been included in the Master Plan Resolution.

F. That the design of the proposed request will minimize environmental impacts, including but not limited to water, air, stormwater management, wildlife, vegetation, wetlands and natural functioning of the environment.

No adverse impacts to the natural environment are expected to occur as a result of the proposed modifications. The owners shall obtain all necessary permits or permit modifications from all agencies with jurisdiction for future improvements.

FINDINGS OF FACT:

All required application documentation has been reviewed by the Development Review Manager and Wellington Departments to determine compliance with the Comprehensive Plan and Land Development Regulations, as presented. The staff analysis concludes that a finding of fact that the proposed requests comply with the requirements for a Rezoning, Master Plan, and Conditional Uses is supported. Conditions of approval for the Master Plan and Conditional Uses are recommended in Resolution Nos. R2026-03, R2026-04, and R2026-05, respectively.

Lists of Exhibits:

- Exhibit A – Resolution No. R2026-03 K Park MUPD
- Exhibit B – Resolution No. R2026-04 Conditional Use – School
- Exhibit C – Resolution No. R2026-05 Conditional Use - Hotel
- Exhibit D – Current Zoning Map
- Exhibit E – Proposed Zoning Map
- Exhibit F – K Park MUPD Master Plan
- Exhibit G – Circulation Plan
- Exhibit H – Conceptual Site Plan
- Exhibit I – Traffic Study (Excludes Appendices)
- Exhibit J – FDOT Approval Letter
- Exhibit K – LifeChurch Letter
- Exhibit L – Wellington Traffic Engineer Approval Letter
- Exhibit M – Shared Parking Study
- Exhibit N – School Traffic Operational Plan
- Exhibit O – School Phasing Plan
- Exhibit P – SCAD Letter
- Exhibit Q – Loading Waste and Logistics Letter
- Exhibit R – Drainage Statement
- Exhibit S – Water and Sewer Statement
- Exhibit T – Justification Statements

PROJECT STANDARDS MANUAL

K-PARK MUPD



DEVELOPMENT PROGRAM

TOTAL DEVELOPMENT SUMMARY:

Maximum Allowable FAR (MU Land Use):
71.3 Acres - .5 FAR (1,292,500 SF)

Total Gross Square Footage:
Up to 1,300,000 SF (Proposed FAR - 0.42 FAR)

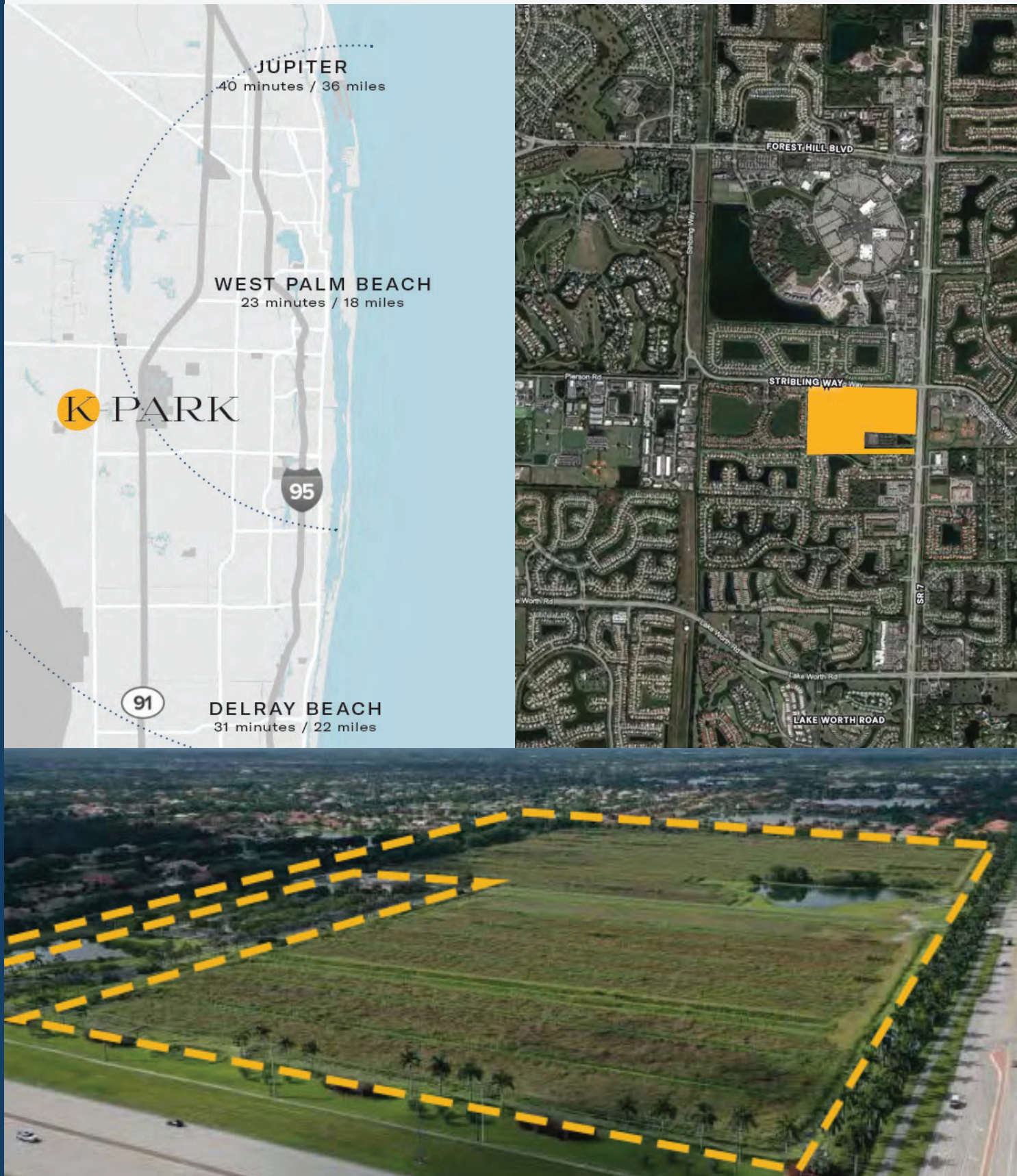
Development Program Data:

| Land Use | Proposed Program | FLUM Amendment |
|---|------------------|----------------|
| Pod A School GSF | 405,000 | 300,000 |
| Pod A Students | 1,750 | 1,700 |
| Retail GSF | 210,000 | 250,000 |
| Restaurant GSF(Excl Outdoor Seating) | 105,000 | 82,478 |
| Retail + Restaurant GSF (Total)(Excl Outdoor Seating) | 315,000 | 332,478 |
| Office GSF | 75,000 | - |
| Hotel Keys | 180 | 150 |
| Hotel GSF | 175,000 | 200,000 |
| Commercial GSF (Total) | 565,000 | 532,478 |
| Residential Units (Multifamily - Mid Rise) | 15 | |
| Residential GSF (Total) | 30,000 | |
| Residential Units (Phase 2) | 200 | |
| Residential GSF (Phase 2) | 300,000 | |
| Residential Units Total | 215 | 600 |
| Residential GSF Total | 330,000 | 720,000 |
| Pod B Commercial & Residential GSF | 895,000 | 1,252,478 |
| Project GSF (Total) | Up to 1,300,000 | 1,552,478 |
| Zoning By Right (71.3 AC 0.5 FAR) | | 1,552,478 |
| Surplus (+) / Shortfall (-) | 252,478 | 0 |

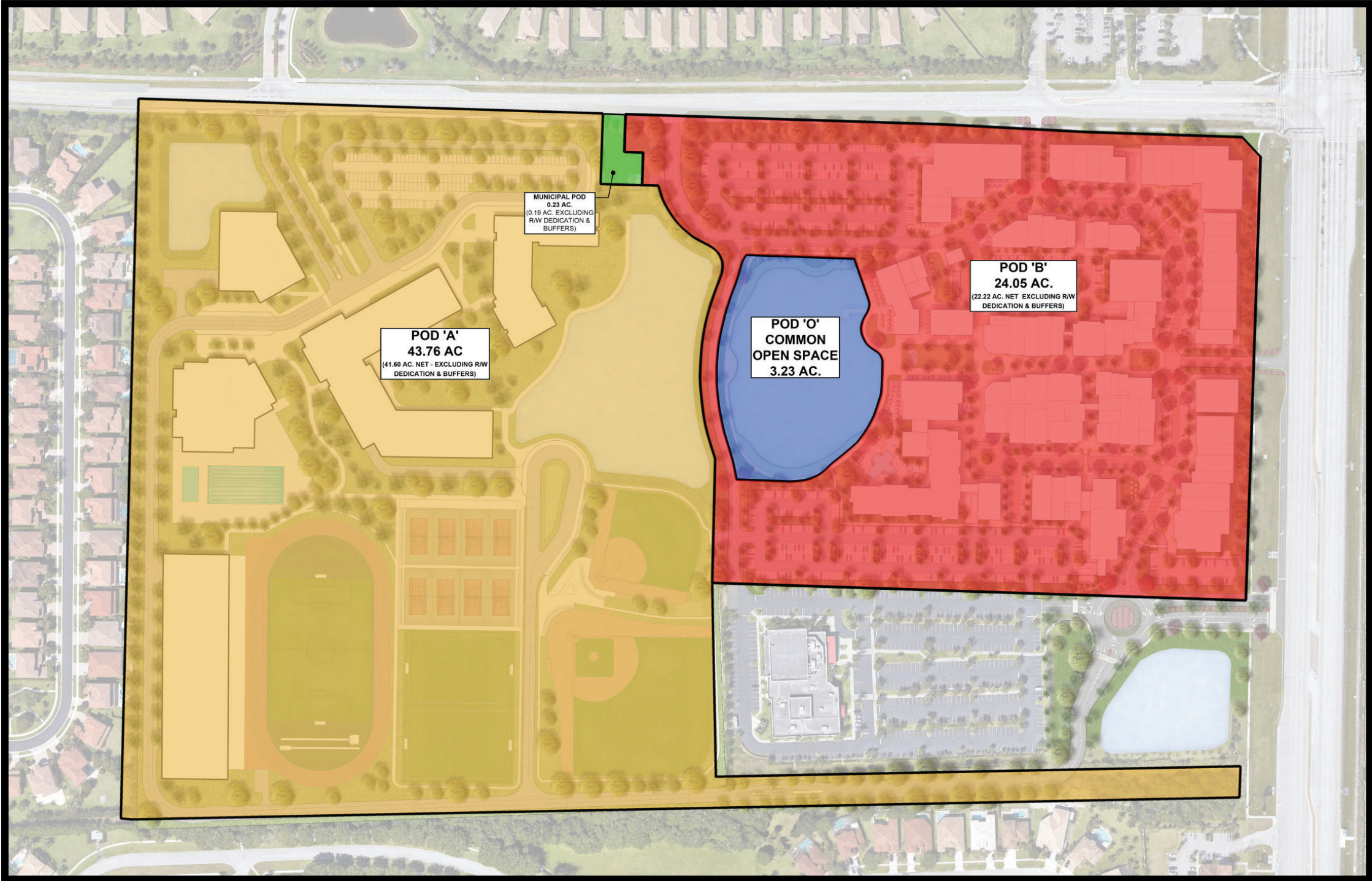
DEVELOPMENT DESCRIPTION

Located at the southwest corner of Stribling Way and State Road 7, the K-Park site will be comprised of a complimentary mix of uses that will serve to benefit the Village of Wellington by offering a premier location for residents to live, work, learn, and play. The mix of uses offered at the K-Park site will be comprised of a non-profit private school and a mixed-use retail village designed with vibrant restaurants, arts and culture, shopping, residences, private & public gardens and walkways. The K-Park Project is in a prime location easily accessible as a popular destination for Wellington residents and visitors.

The 71.27 Acre Site consists of a Mixed Use(MU) Future Land Use Designation, and is proposed to be rezoned to the Mixed Use Planned Development (MUPD) Zoning designation.



Master Plan:



CHAPTER 1:

POD A





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Chapter 1: Pod A

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ARCHITECTURAL GUIDELINES

WINGROVE ACADEMY

The Wingrove Academy campus will have a cohesive architectural identity while also including variation in the architectural language of individual buildings that reflects the occupancy and program within each building. The buildings will be designed with a variety of materials to create a human scale by breaking down their massing. Each elevation will be designed in consideration of its solar orientation and may include exterior solar shading to mitigate solar heat gain. Materials used will be contextually appropriate, sustainable, and incorporate colors of nature and natural materials. Exterior materials may include but are not limited to wood, brick, concrete, metal and glass curtainwall. These buildings will be designed to be beautiful and performative while also functioning as learning tools to reinforce the school's mission of creating a green, sustainable environment with a strong connection to nature.

POD A: SCHOOL

Design Elements

The project emphasizes a strong sense of human scale throughout its design, ensuring that students, faculty, and visitors experience the campus as welcoming and well-proportioned. The layout of the buildings forms a series of internal courtyards that create moments of openness and calm, offering shaded outdoor spaces for learning, gathering, and play. These courtyards serve as focal points throughout the campus, fostering community interaction and a strong connection to the landscape.

Adjacent to the school buildings, water retention - originally required for site drainage - has been integrated as a design feature. This acts as a natural buffer and contribute to campus security while enhancing the overall identity of the project. More than functional, it helps root the buildings in the landscape, creating a cohesive environment that balances safety, environmental sensitivity, and privacy.

Façade and Wall Surfaces

The building façades are designed to reinforce the school's connection to nature and its landscape-driven approach with a contemporary architectural language. Upper levels feature naturally-toned surfaces articulated with a repeated rhythms. Openings are positioned within this system to respond to orientation, optimizing daylight while minimizing glare and heat gain. At the ground level, façades become more transparent to allow for visual connectivity between interior spaces and the surrounding courtyards, lake, and open landscapes.

ARCHITECTURAL GUIDELINES

POD A: SCHOOL

Massing

The school's massing strategy is composed to harmonize with its expansive site in Wellington, Florida, and to respect the scale of the surrounding residential neighborhoods. The four primary buildings - the Lower School to the north, the centrally placed Upper School, and the Sports and Aquatic Center and Theater to the west - are integrated within the site in a way that minimizes visual impact and enhances campus security. The tallest structure, the three-story Upper School, is centrally located and set back from the property lines, reinforcing the idea of the campus as a cohesive and protected environment nestled within the landscape.

Building volumes are arranged to shape a sequence of outdoor spaces, including internal courtyards and lakeside edges, encouraging interaction and movement across the site. The upper floors of the academic buildings feature facades delineated by a regular rhythm of patterns repeated across all buildings to reinforce their cohesion. Openings respond to solar orientation, balancing daylight access and thermal comfort. Ground floor levels are more transparent, creating visual connections to courtyards and lakes, and emphasizing the project's openness, safety, and a strong relationship to the natural environment.

Height & Site Layout

The campus is organized around four main buildings: the two-story Lower School to the north, the three-story Upper School at the center, the two-story Sports and Aquatic Center to the west, and the two-story theater north of the Sports and Aquatic Center. This configuration creates a spatial hierarchy and maintains a modest overall height profile, concentrating taller elements toward the center of the site and away from adjacent neighborhoods.

The site layout is structured around a series of natural and designed buffers that protect the site's perimeter and nest the buildings within layers of landscape. To the north, a green buffer separates the campus from Stribling Ave; to the west, a zone of landscape buffer, access road, and parking creates a separation from residential properties. Athletic fields to the south and a large lake to the east further reinforce this approach, contributing to a sense of openness and providing natural boundaries that enhance safety and privacy. No structures are placed directly on property lines, highlighting the project's sensitivity to its context and its commitment to creating a secure, integrated learning environment.

The following conceptual architectural materials and design inspiration as it relates to the Pod A School are provided -
subject to ARB approval by the Village of Wellington.



VISION GLAZING



ALUMINUM



EXPOSED CONCRETE
OFF-WHITE



VEHICULAR
RATED CONCRETE



SIDEWALK CONCRETE
AND HEADER



BRICK PAVERS

The following conceptual site amenities as it relates to the Pod A School are provided - subject to ARB approval by the Village of Wellington.



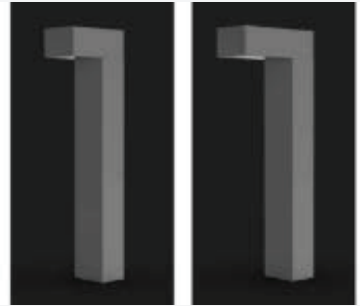
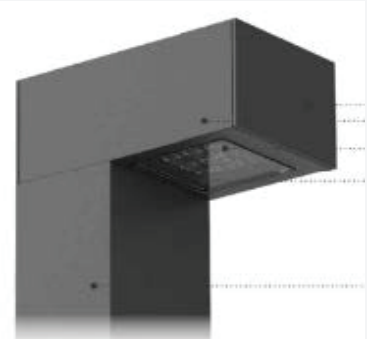
BIKE RACK



TRASH CAN



BENCH



LIGHT BOLLARD

DEVELOPMENT REGULATIONS

POD A SCHOOL

| Project Development Standards ⁽¹⁾ | | |
|--|--|--|
| MUPD (Pod A School) | | |
| | ULDC (Required) | Requested |
| Minimum Lot Size | 5 acres | 71.27 acres |
| Minimum Lot Width | (2) | 949.58 |
| Minimum Lot Depth | (2) | 1,716.01 |
| Maximum Building Coverage | 45% | 45% |
| Setbacks | | |
| Front | (2) | 30' |
| Side | (2) | 30' |
| Rear | (2) | 30' |
| Parking | | |
| Required Parking | School, Elementary - 1 space per classroom, 1 space per 200 NSF of building not accounted for in class or hall areas. School, Secondary - 0.25 per student, 1 per 200 NSF of building not accounted for in class or hall areas. | 1 space / Faculty and Staff 1 visitor space / 50 Students 1 space / 10 students in grade 11 1 space / every student in grade 12 ⁽³⁾⁽⁴⁾ |
| Standard Parking Stall Width | 9.5' | 9' in Parking Garage ⁽³⁾ |
| 90 Degree Parking Drive Aisles | 25' | 24' ⁽³⁾ |
| Loading Space Quantity | 13 | 3 ⁽³⁾ |
| Loading Space Dimensions | 15' x 55' | 15' x 50' ⁽³⁾ |
| Landscape | | |
| Fence/Wall Location | Landscaping shall be located between the barrier and the adjacent property or ROW | Fence or wall may be located at the property line where the adjacent property has an existing buffer ⁽³⁾ |
| Outside Activity Areas Buffer | 50' | 50' Separation ⁽³⁾ |
| Buffer Type A (East) | 10' | 10' |
| Buffer Type A (North – Church) | 10' | 10' |
| Buffer Type A (North – Church) | 1 Shade Tree / 30 L.F. | Elimination of code required buffer trees ⁽³⁾ |
| Buffer Type A/F (West) | 10' | 10' with opaque wall |
| Buffer Type A/F (South) | 10' | 10' without fence for portion of southern property line ⁽³⁾ |
| Buffer Type C (North) | 20' | 20' |
| Landscape Buffer Path | 10' width in addition to Required Buffer | Provide 20' buffer and sidewalk in ROW at 8' width ⁽³⁾ |
| Internal Lot Trees | 1403 Trees – Pod A and Pod B | 1050 Trees – 75% of Requirement – Pod A and Pod B ⁽³⁾ |
| Foundation Planting | 5' for each building story or a maximum of 25' | 5' for each building story or a maximum of 25' – or equivalent planting area provided at or near vicinity of building |
| Planned Development Landscape Requirements | 30% increase in size or quantity per PUD requirements | 30% increase in size or quantity per PUD requirements |
| Additional Palm/Tree Plantings | 1 tree per 20 linear feet in required landscape buffer for pods with buildings above 35' | 1 tree per 20 linear feet in required landscape buffer for pods with buildings above 35' |
| (1) Any development standards not specifically outlined and/or requested/approved within the PSM shall be per the LDR, as determined by Wellington for the specific standard and/or use/structure. | | |
| (2) Determined by Development Order | | |
| (3) Deviation from standard Village of Wellington Development Regulations | | |
| (4) Subject to School Operational Plan | | |

PARKING

POD A - SCHOOL

Required Parking

- Required off-street parking for the Pod A School shall be provided in accordance with the minimum parking supply established in the Parking Study prepared by Simmons & White, utilizing ITE Parking Generation, 6th Edition and ULI Shared Parking, 3rd Edition, as approved by the Village.
- The code requirement to park Elementary Schools at one space per classroom, plus one space per 200 NSF of building not accounted for in class or hall areas, and Secondary Schools at 0.25 per student, plus one per 200 NSF of building not accounted for in class or hall area shall not apply within Pod A;
- The Applicant is proposing to calculate required parking for the primary and secondary school and accessory uses based on the following per the School Operational Plan:

Proposed Parking Metrics

- 1 space for each faculty and staff member (426 faculty/staff members) = 426
- 1 visitor space for every 50 students (1750 Students) = 35
- 1 space for every 10 students in Grade 11 (168 Students) = 17
- 1 space for every student in Grade 12 (168 Students) = 168
- Total Parking Required = 646 Spaces

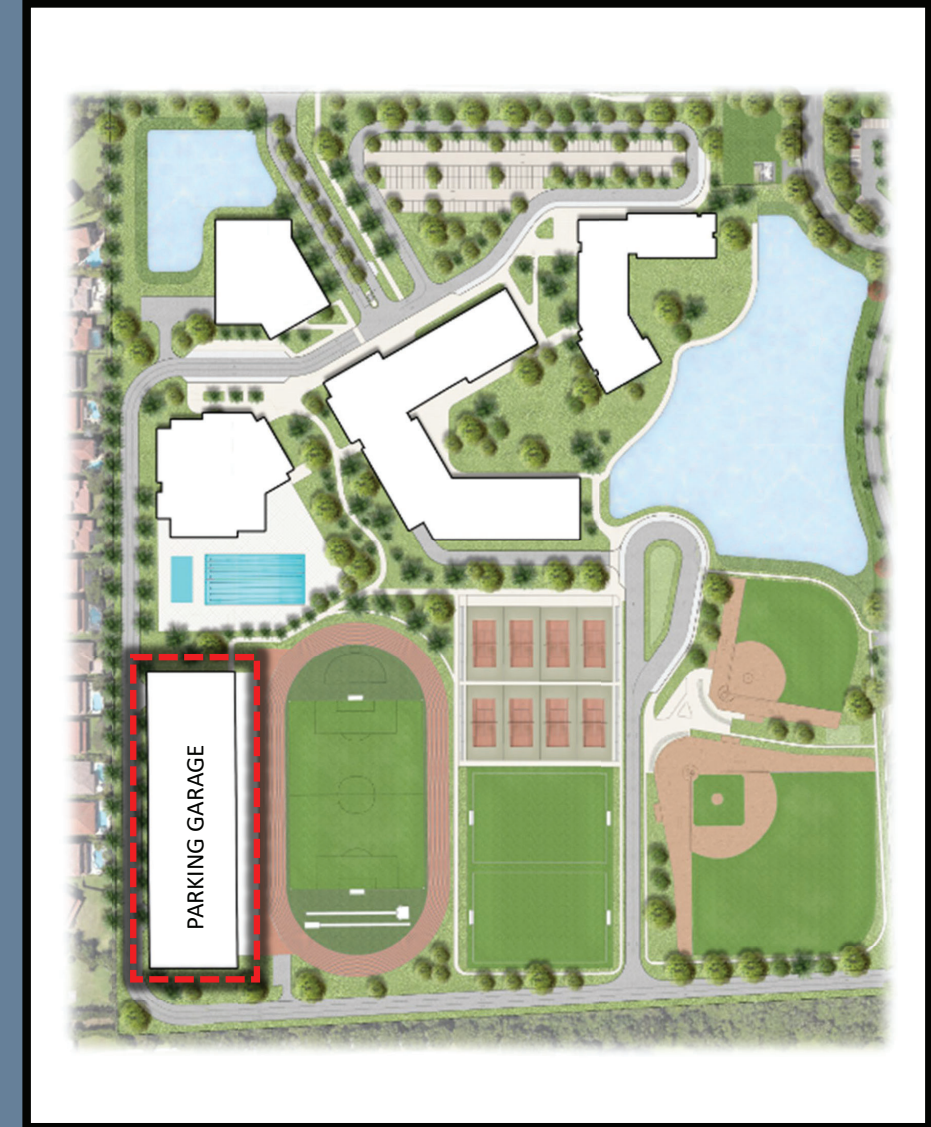
As shown above, the required parking based on the proposed parking metrics is 646 spaces. The school is currently proposing a total of 646 parking spaces.

PARKING

POD A - SCHOOL

Parking Stall Width - Parking Garage

- All surface parking spaces in Pod A shall maintain a minimum stall width of 9.5 feet.
- Within the Pod A School parking garage only, standard parking stall width may be reduced to a minimum of 9.0 feet. This standard applies solely to the garage and does not modify the stall width requirements for surface lots elsewhere in the MUPD.

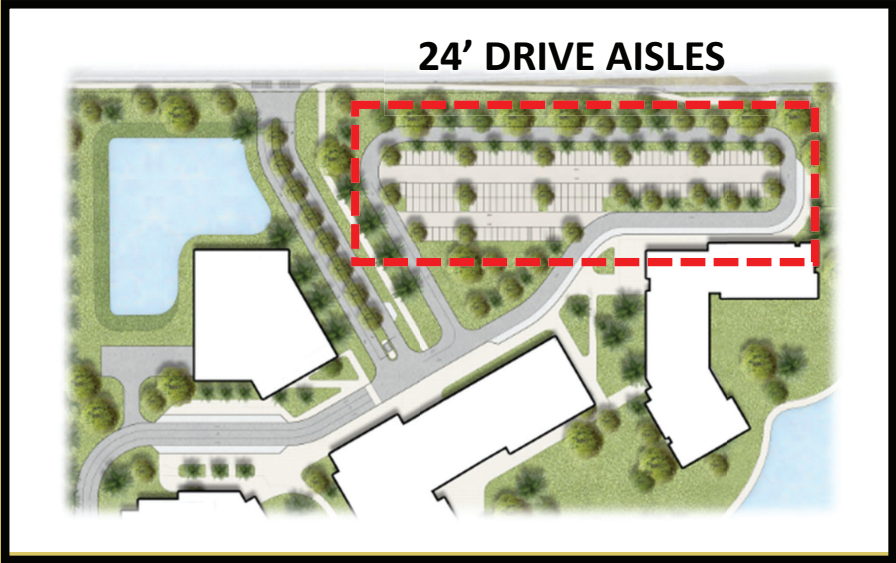


PARKING

POD A - SCHOOL

90-Degree Parking Drive Aisles

- In order to promote efficient site planning, traffic calming, preservation of pedestrian-oriented open spaces and landscaping without compromising the maneuverability of passenger vehicles or the functionality of parking areas within Pod A, the minimum two-way drive aisle width serving 90-degree parking spaces shall be 24 feet, in lieu of the 25-foot minimum required by Table 7.5.2.



Loading Space Quantity and Dimensions

- Loading for the Pod A School (approximately 405,000 GSF) shall be provided in accordance with the following modified standards, in lieu of Section 7.5.3 and Table 7.5.3:

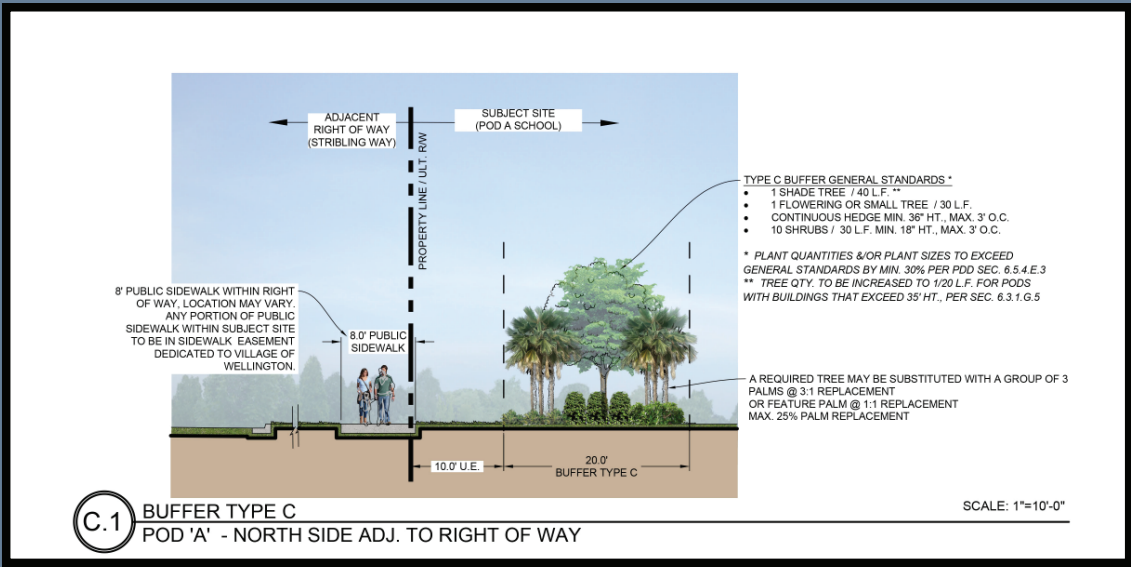
| SCHOOL – 405,000 GSF | | | |
|------------------------------------|----------------|---------------|----------------|
| ZONING | REQUIRED COUNT | PROPOSED SIZE | PROPOSED COUNT |
| 15' X 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' X 55' @ 1/50,000 SF OVER 100K | 6.1 | | |
| TOTAL | 13 | 15'X50' | 3 |

- The minimum number of loading spaces provided for the Pod A School shall be three (3), each with minimum dimensions of 15 feet by 50 feet.
- The Code requirement for thirteen (13) loading spaces at 15 feet by 55 feet shall not apply within Pod A.

LANDSCAPE STANDARDS

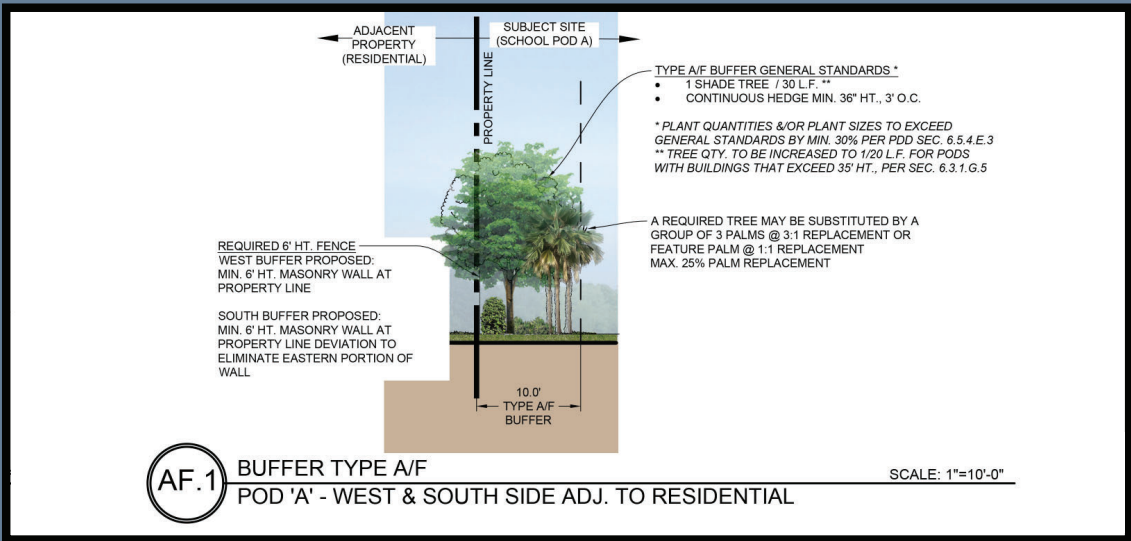






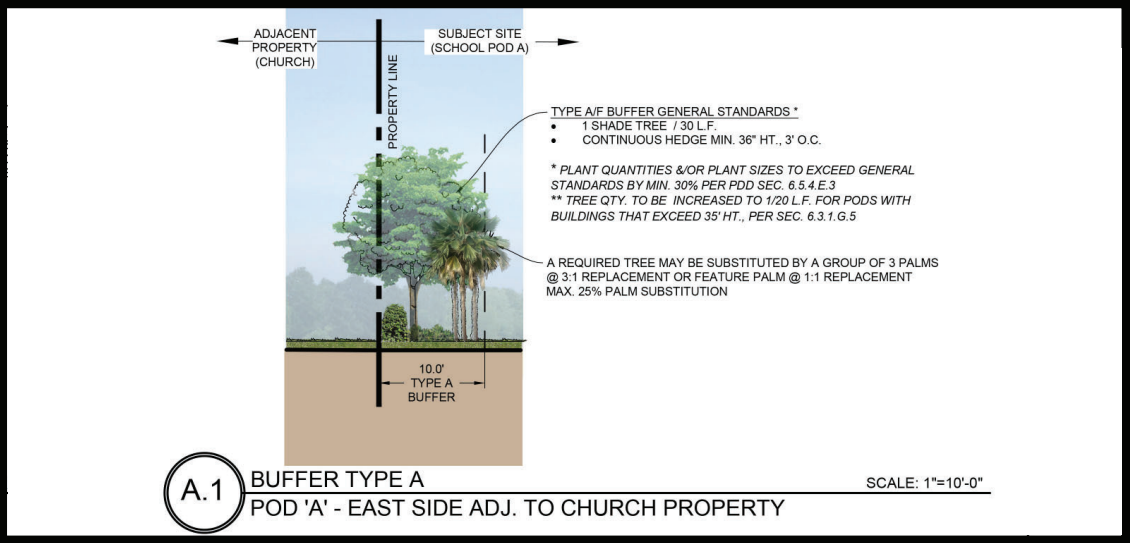
Buffer Type C.1

A 20' Type C perimeter buffer plus a 10' Utility Easement is proposed along the north side of Pod A adjacent to Stribling Way. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the Applicant proposes to maintain the width of the existing sidewalk within the Stribling Way ROW at 8' with locations to vary. The portion of the 8' sidewalk that lands within the subject site shall be dedicated to the Village of Wellington via Sidewalk Easement



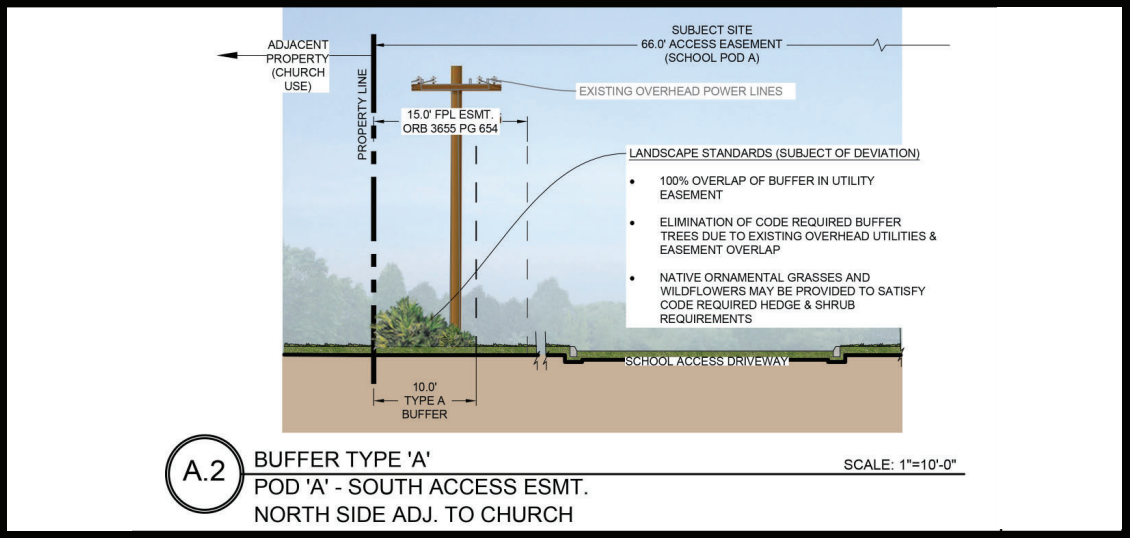
Buffer Type AF.1

Whereas a 10' perimeter buffer is required for a Type A/F buffer, a modified 10' perimeter buffer is provided along the west & south side of Pod A adjacent to the residential neighborhoods of Oakmont Estates and Farmington Estates. The modified buffer retains the existing fence along the western property line to remain and proposes a Masonry Wall at a minimum 6' in height at the property line to further secure the Pod A School. Pod A also includes a proposed 6-foot-tall masonry wall along the southern property line, terminating at the gate access leading into the Pod A School. Accordingly, a deviation is requested to eliminate the eastern portion of the required wall along this southern boundary. An additional deviation is requested to allow the wall to be placed directly on the property line where the adjacent properties to the west and south already provide an existing buffer, rather than following the standard Type A/F buffer requirement that landscaping be located between the barrier and the adjacent property or right-of-way.



Buffer Type A.1

Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer is provided along the east side of Pod A, adjacent to the church property.



Buffer Type A.2

Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer consisting of a 100% overlap of an existing FPL utility easement is proposed for Pod A along the north side of the access easement adjacent to the Church. This will result in an elimination of code required buffer trees due to existing overhead utilities & the proposed easement overlap. Native ornamental grasses and wildflowers may be provided to satisfy code required hedge & shrub requirements.

TREES



Bursera simaruba
Gumbo Limbo

Clusia rosea
Pitch-apple

Conocarpus erectus
Green Buttonwood

Ficus aurea
Strangler Fig

Lysiloma latisiliquum
Wild Tamarind

Quercus virginiana
Live Oak

PALMS



Coccoloba diversifolia
Pigeon Plum

Eugenia rhombea
Red Stopper

Myrcianthes fragrans
Simpson's Stopper

Myrcia zuzygium
Myrtle of the River

GROUND COVERS



Andropogon virginicus
Broomsedge Bluestem

Eragrostis elliottii
Elliott's Lovegrass

Muhlenbergia capillaris
Muhley Grass

Tripsacum dactyloides
Fakahatchee Grass

Tripsacum floridanum
Florida Gammagrass

UNDERSTORY TREES



Bursera simaruba
Gumbo Limbo

Clusia rosea
Pitch-apple

Conocarpus erectus
Green Buttonwood

Ficus aurea
Strangler Fig

Lysiloma latisiliquum
Wild Tamarind

Quercus virginiana
Live Oak

SHRUBS



Andropogon virginicus
Broomsedge Bluestem

Eragrostis elliottii
Elliott's Lovegrass

Muhlenbergia capillaris
Muhley Grass

Tripsacum dactyloides
Fakahatchee Grass

Tripsacum floridanum
Florida Gammagrass

GRASSES



Andropogon virginicus
Broomsedge Bluestem

Eragrostis elliottii
Elliott's Lovegrass

Muhlenbergia capillaris
Muhley Grass

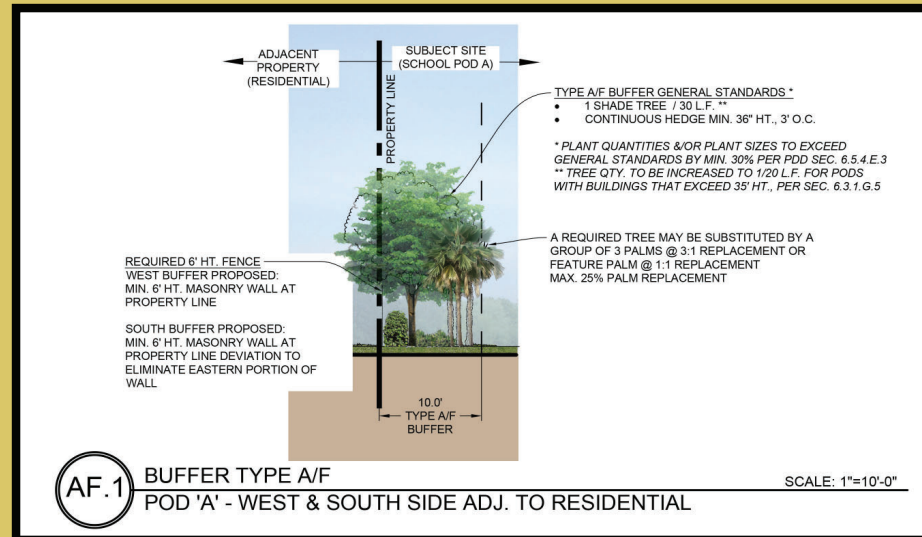
Tripsacum dactyloides
Fakahatchee Grass

Tripsacum floridanum
Florida Gammagrass

LANDSCAPE DEVIATIONS

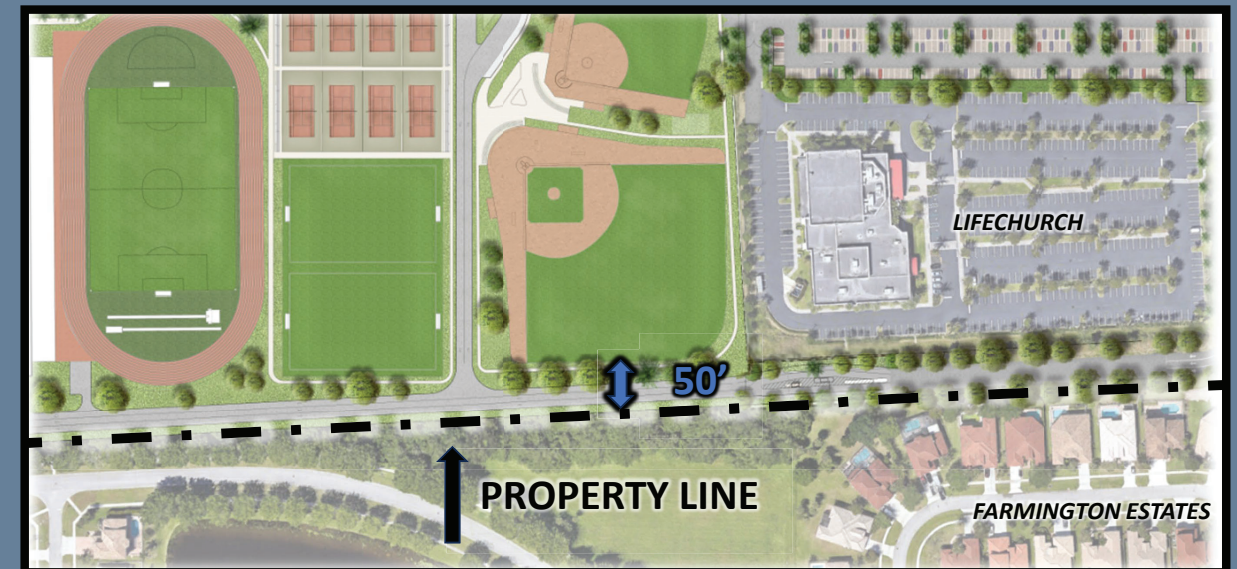
Fence/Wall Location within Landscape Buffers

- Along the west property line of Pod A adjacent to Oakmont Estates, a masonry wall with a minimum height of six (6) feet may be located at the property line, with required landscaping permitted on the Pod A side and/or supplemented by existing landscaping on the adjacent residential property.
- Along the south property line of Pod A adjacent to Farmington Estates, a masonry wall with a minimum height of six (6) feet may be located at the property line and may terminate at the Pod A access gate.
- Along the east property line of Pod A adjacent to the church site, a fence or wall may be located at the property line, with required landscaping located on the Pod A side and/or in combination with existing landscape material.
- These standards supersede the requirement in Section 7.8.5.C.7 that landscaping be located between the barrier and the adjacent property or right-of-way.



Outside Activity Areas Buffer

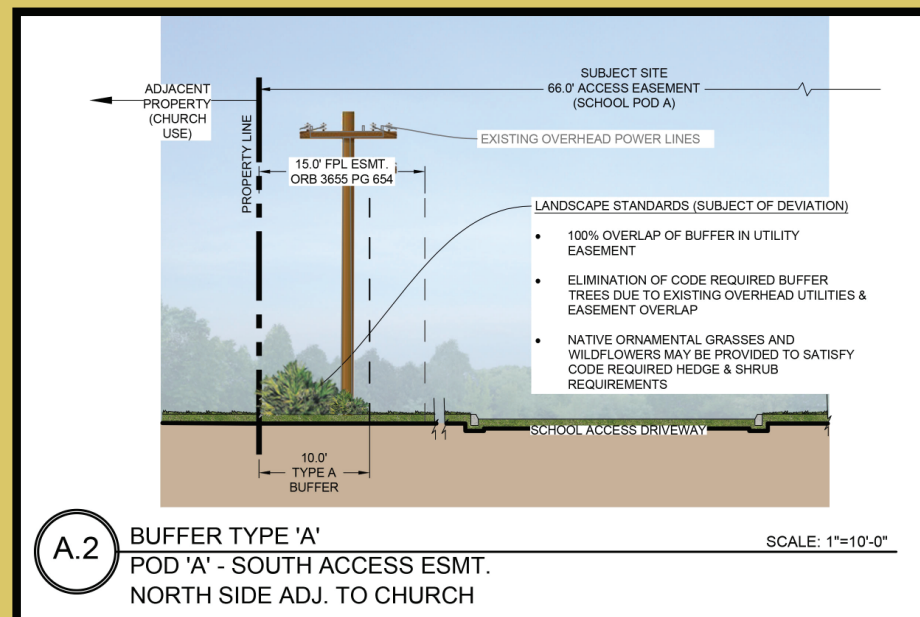
- For school outside activity areas and athletic facilities in Pod A located along the south property line, a minimum separation of fifty (50) feet shall be provided between the south property line and the nearest outside activity area, in lieu of the 50-foot outside activity area buffer standard in Section 6.2.2.F.5.
- This separation shall include, at a minimum: (a) a 10-foot Type A/F landscape buffer along the Pod A south property line and (b) internal driveway and/or landscaped open space between the buffer and the outside activity areas.
- The existing ± 25 -foot-wide landscape buffer on the adjacent residential property further contributes to the overall effective separation.



LANDSCAPE DEVIATIONS

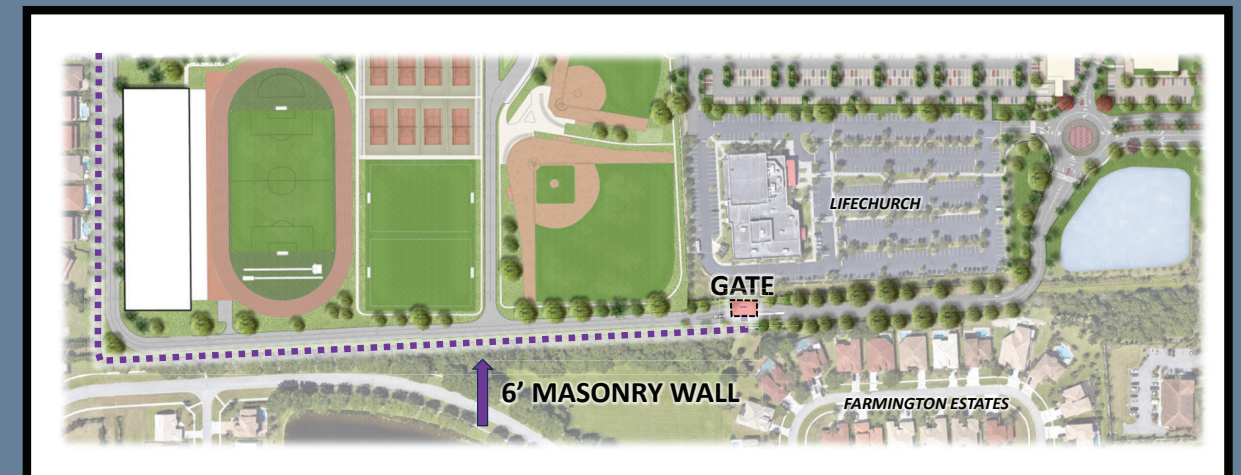
Type A Buffer – North Adjacent to Church Parcel

- Along the north side of the Pod A access easement, immediately south of the church parcel, the required Type A buffer shall be modified as follows: shrubs and hedges shall be provided in compliance with Type A standards, but shade trees otherwise required at a ratio of one (1) per 30 linear feet may be omitted where constrained by overhead utility lines and applicable FPL "Right Tree, Right Place" requirements.
- Existing buffer material on the church property may be considered as contributing supplemental screening along this segment of the access drive.



Type A/F Buffer – South Property Line

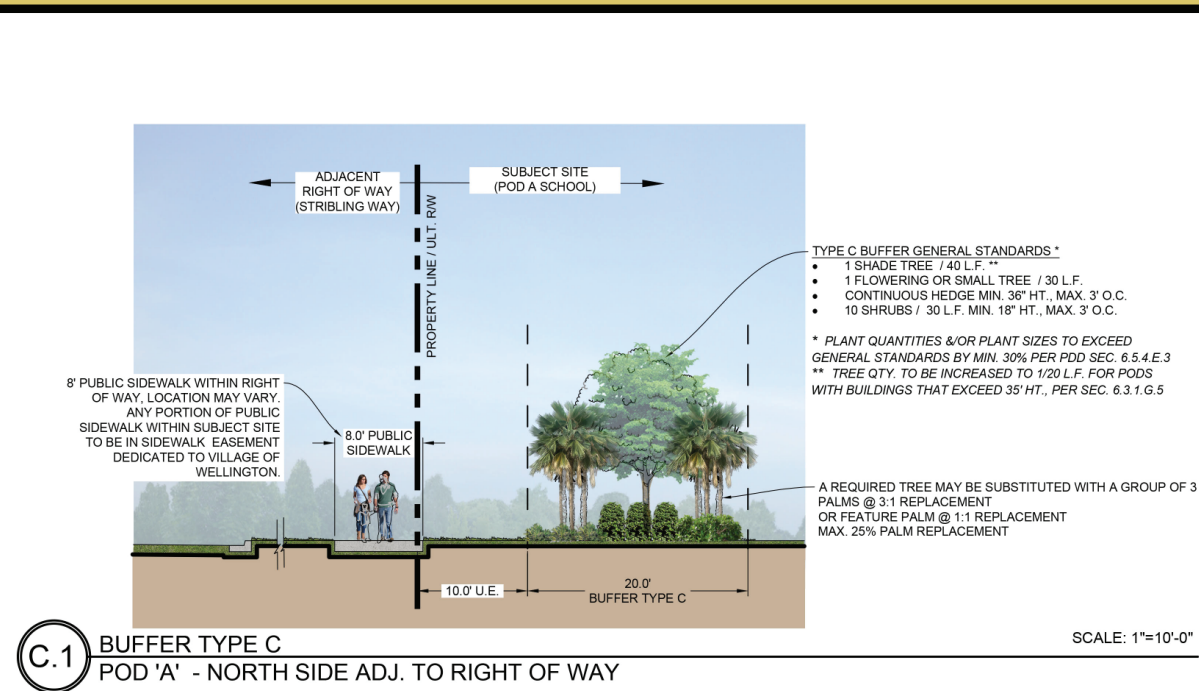
- Along the south property line of Pod A, a modified Type A/F landscape buffer shall be provided as follows:
- A masonry wall with a minimum height of six (6) feet is encouraged in lieu of a fence along the portion of the south property line extending from the southwest corner east to the Pod A access gate.
- East of the Pod A access gate, where only a driveway providing access from the church roundabout is located and no building square footage or active use areas are proposed, the fence or wall component of the Type A/F buffer may be omitted.
- The existing landscape buffer along the northern boundary of Farmington Estates provides additional screening where a wall is not proposed.



LANDSCAPE DEVIATIONS

Landscape Buffer Perimeter Path

- Along Stribling Way and State Road 7, a minimum 20-foot perimeter landscape buffer and a 10-foot general utility easement shall be provided, with up to five (5) feet of overlap between the buffer and easement permitted.
- In lieu of the additional ten (10) feet of buffer width as required to accommodate a perimeter circulation path pursuant to Section 6.5.4.7., pedestrian circulation along these frontages shall be accommodated by sidewalks within the adjacent rights-of-way, including an existing 8-foot-wide sidewalk along Stribling Way.



Foundation Planting- Pod A

- Foundation planting requirements for non-residential structures within Pod A may be satisfied by providing an equivalent total square footage of planting area associated with each building, located at or near the building, rather than as a continuous strip of minimum depth adjacent to each façade as prescribed by Section 7.8.2.B.2.
- The combined planting area for each building shall be at least equal to the total square footage that would otherwise be required by the foundation planting provisions, and may be configured as courtyards, landscape beds adjacent to circulation routes, or other functional planting areas integral to the campus layout.

Internal Lot Trees: Site-wide Standard

- The internal lot tree requirement for the K-Park MUPD shall be applied on a site-wide basis of qualifying open space area, excluding perimeter buffer acreage, lake acreage, and acreage dedicated to sports fields.
- Based on this area, the standard requirement of one (1) tree per 1,500 square feet would yield a total of 1,403 internal trees. This standard is modified to require a minimum of 1,050 internal trees throughout the MUPD, representing a 25 percent reduction. Shrub quantities associated with internal lot planting shall continue to meet the Code requirement..

CHAPTER 2:

POD B

Commercial:

Hotel: 180 Rooms/ 175,000 SF

Office: 75,000 SF

Retail: 210,000 SF

Restaurant: 105,000 SF (Plus up to 20,000 SF of Outdoor Dining)

Residential:

Multi-Family 15 Units/ 30,000 SF Phase 1

Multi-Family 200 Units/ 300,000 SF Phase 2





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Chapter 2: Pod B

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ARCHITECTURAL GUIDELINES

Pod B: Mixed-Use

Design Intent

The Shops and Hotel at Wellington Village are planned around a pedestrian-friendly street including traffic calming and roadway elements such as on-street parking and chicanes. In addition, public spaces such as pedestrian promenades, plazas, parklets, pathways, wide sidewalks, gardens and key focal points will be incorporated through the plan. Site amenities will include street trees, decorative landscape planters, shading, trash receptacles, and benches. Portions of the Street may be fully pedestrianized for community events such as farmer's markets, live music, street fairs, holiday celebrations, and seasonal programming. Sufficient parking will be provided, located to be convenient, accessible and designed to minimize occupied land area. The Project embraces a compact, walkable planning philosophy that would allow users to park once and meet several daily needs.



ARCHITECTURAL GUIDELINES

Massing & Height

Building masses are located in order to be compatible in height, volume, and overall scale to surrounding developments. The Shops will include retail, dining, professional office, boutique grocer, and fitness in one (1), two (2) and (3) story tall buildings. The proposed extended stay hotel will be a building seven (7) stories in height.

The Hotel will be centrally located on the site, and the design will include transitional massing elements that mitigate the change in height and elevation with the surroundings. The height of the Hotel is proposed to be 88' in order to encourage passive wayfinding by creating a focal element in the heart of the Project. Hotel amenities may include a pool and outdoor dining area.

Setbacks

Locations of buildings and open space will be distributed in order to enhance walkability. Mid-block pedestrian-only paseos will be located creating connectivity and visual sightlines throughout the site. Natural features and landscaped areas will be incorporated to provide shade and comfort.

Landscaping and architectural articulation will be maintained facing State Road 7 in order to meet the intent of the buffer requirement.

Design Intent

The architectural character, material palette, and building designs will be contextual with South Florida and responsive to climate and place. In order to establish unified architectural character and image within Wellington Village, the use of similar, but not necessarily identical forms, colors, materials, and details will be incorporated. Drawing inspiration from Wellington's vernacular architecture, traditional and contemporary designs will be balanced in order to create recognizable forms while allowing for integration of modern design philosophies. All commercial facades will contain architectural details on all sides to avoid blank walls. Breaks in roof planes, building planes, and building massing will be utilized. Varied materials, colors, and textures will be incorporated to allow for individual storefront expressions and to create a sense of scale. All commercial buildings will have highly defined customer entrances incorporating elements such as canopies, architectural banding, display windows, upper-level terraces, and recessed entries, in addition to detailed features such as balconies, awnings, light fixtures, sculptured artwork, and signage.

Façade, Colors & Wall Surfaces

Primary exterior materials may include but are not limited to stucco, wood, brick, stone, and metal. The primary color palette will include light tones such as white, sand, cream, and light grey. A wider palette of accent colors will be considered in order to accommodate tenant expression. All exterior finishes, colors, and materials shall be approved by the Village of Wellington Architectural Review Board prior to issuance of building permits. Design of accessory buildings and structures will be consistent with the materials, colors, and styles of the principal structures.

ARCHITECTURAL GUIDELINES

Fenestration

Commercial buildings will feature recessed and/or covered doors at exterior public access locations. Placement and sizing of doors and windows will be designed to enhance façade articulation. Commercial display windows will include elements like frames, sills, trim and awnings in complementary colors and materials. Openings may include arches, sidelites, and transoms in order to promote natural light. Fenestration may include mullions and muntins to introduce scale in larger openings. Primary facades will incorporate windows, at a minimum, 40 percent of the elevation.

Roof Design & Materials

Buildings will feature a mix of flat and pitched roofs. Pitched roof materials may include metal or tile. Roofing materials and color will be selected to be compatible with the principal building architectural character. Similarly, compatible screening elements or parapets will be included at rooftop mechanical equipment locations. Additional roof articulation may be included through the selective use of clock / bell towers, cupolas, and chimneys.

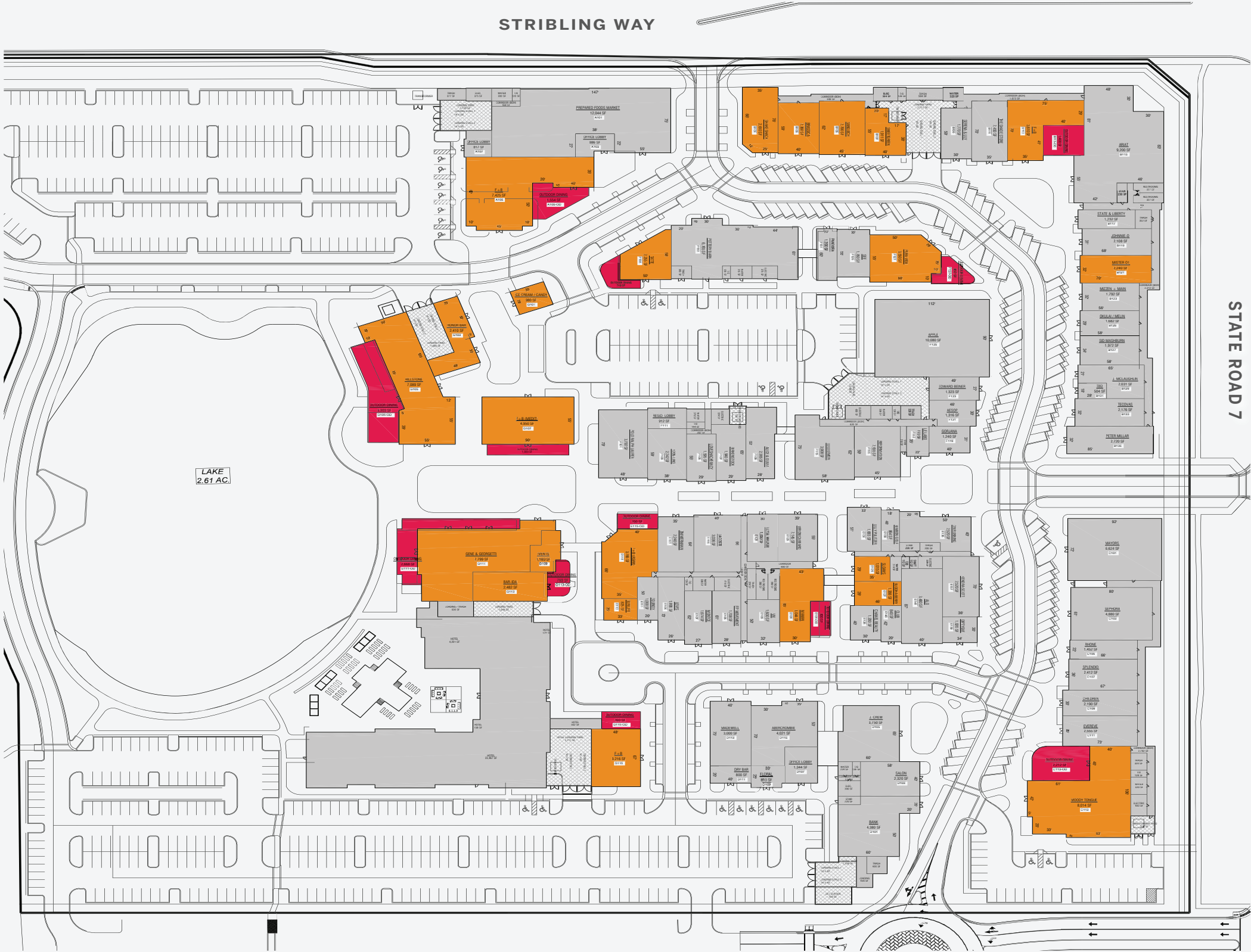
Ground Surfaces & Paving

Public walkways will feature primary surface materials such as brick, tile, or stone at a scale corresponding with adjacent buildings. Separation between pedestrian and vehicular zones will be clearly delineated.

Exterior Patio and Dining Areas

Tenants with outdoor patios or dining areas shall be subject to the following regulations

- **Perimeter Fence or Enclosures.** Tenants shall be permitted to install perimeter fences, planters, separators, and/or enclosures subject to approval. All structures shall be self-supporting and designed to be compatible with the design, materials, colors, and style of the principal structure.
- **Outdoor Furnishings.** Outdoor furnishings including but not limited to furniture, planters, lighting, and umbrellas should be complementary to the primary architecture of the restaurant. Furniture for outdoor dining should be built for outdoor use and should be sufficiently sturdy to withstand severe variations in weather and wind. Furnishings shall comply with local jurisdictional requirements and are subject to approval by Management.
- **Dog-friendly dining.** Dog-friendly dining within designated restaurant outdoor area(s) may be allowed with approval of a special use permit. A food/drink menu for patrons’ dogs may be provided. Dogs food/drink shall only be served in single-use disposable recyclable containers, and shall be handled separately from all dishes, utensils, and any other items used for food service operations. All restaurants with outdoor area(s) designated for dog-friendly dining shall provide perimeter/separation barrier.
- **Sound Levels.** No sound amplification will be allowed for the purpose of commercial advertising. Sound levels that cross property lines at a volume that is unreasonably loud will be prohibited. This section shall not apply to any special events such as parades or festivals but shall apply to restaurants and lounges.



Outdoor Dining

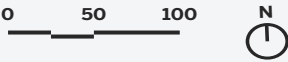
Program Summary

| | |
|----------------|-------------|
| Retail + F&B | 290,000 NSF |
| Outdoor Dining | 20,000 SF |

Key

F+B

Outdoor Dining



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RELATED ROSS • ELKUS MANFREDI ARCHITECTS



Main Street Outdoor Dining Precedents

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SUNNY'S MIAMI



ABA AUSTIN



MIRATE LOS ANGELES



BACARI LOS ANGELES

Main Street Outdoor Dining Precedent



CONCEPTUAL RENDERING

Main Street Outdoor Dining View

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CARNEROS FARM NAPA VALLEY



ABA TEXAS



Main Street Outdoor Dining Precedents - Covered

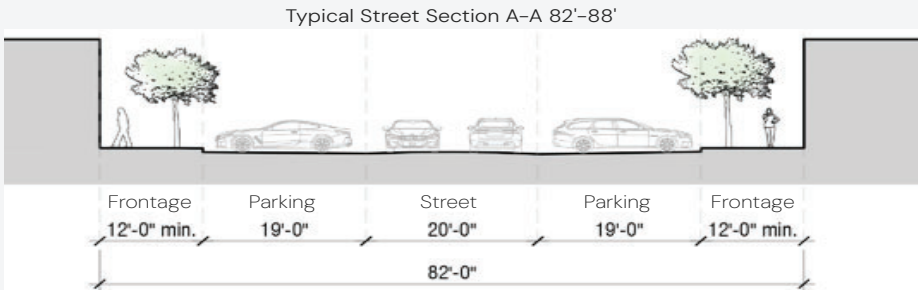
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RELATED ROSS • ELKUS MANFREDI ARCHITECTS

POD B MIXED-USE VILLAGE

The Pod B Mixed-Use Village shall be organized around three primary character corridors:

Main Street, the Promenade, and the Hotel. Main Street functions as the central retail and dining spine, with active ground-floor storefronts, outdoor café seating, and a lush, shaded streetscape supported by on-street parking and wide sidewalks. The Promenade is envisioned as a pedestrian-only, mid-block corridor that provides a 600-foot-long landscaped connection between Main Street and the lakefront, lined with shops, boutiques, and residential entries to create an intimate, walkable environment. The Hotel Street/Frontage shall frame the signature lakeside hotel with a formal valet motorcourt, enhanced landscape and hardscape, and direct connections to the lakefront park and dining terraces, establishing the village's hospitality gateway and reinforcing its role as a regional destination.





A lively street lined with shops, cafes, and restaurants within a lush and welcoming landscaped environment.

Main Street Enlarged Plan



Main Street Precedents

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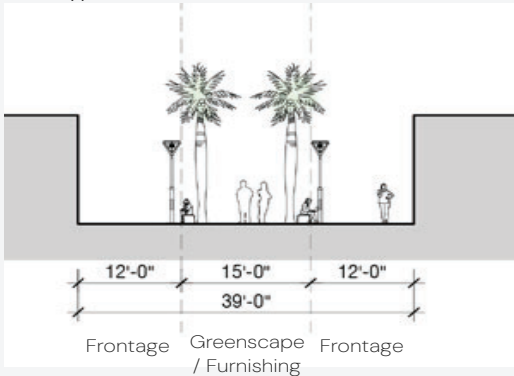


KEY PLAN



CONCEPTUAL VIEW

Typical Promenade Section A-A 30' - 39'



The project will feature a 600' foot long pedestrian-only path linking Main Street to the Lake. Designed as a vibrant public space blending residential, commercial, and recreational elements, the promenade will be lined with palm trees, native landscaping, and shade, and will include a variety of ground-level shops, cafes and boutiques inviting leisurely strolls. A series of intimate paseos will enhance site connectivity and creates opportunity for exploration.

Promenade Enlarged Plan



Promenade Precedents

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Hotel Site Plan

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Hotel Street View

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Hotel Valet Motorcourt Precedents

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THE DEPICTED IMAGES ARE INSPIRATIONAL CONCEPTS OF THE TYPE OF SITE FURNISHINGS THAT WILL BE USED FOR POD B.

INSPIRATION



SITE FURNISHING

THE FOLLOWING ARE INSPIRATIONAL IMAGES OF THE TYPE OF SITE FURNISHINGS THAT WILL BE USED FOR POD B.



Bollards



Bicycle Racks



Lighting



Trash



Benches



Benches



Benches



THE DEPICTED IMAGES ARE INSPIRATIONAL CONCEPTS OF THE TYPE OF SITE FURNISHINGS THAT WILL BE USED FOR POD B.

DEVELOPMENT REGULATIONS

POD B: Mixed Use

| Project Development Standards ⁽¹⁾ | | |
|---|--|--|
| MUPD (Pod B Mixed-Use) | | |
| | ULDC (Required) | Requested |
| Minimum Lot Size | 5 acres | 71.27 acres |
| Minimum Lot Width | (2) | 949.58 |
| Minimum Lot Depth | (2) | 1,716.01 |
| Maximum Building Coverage | 45% | 45% |
| Setbacks | | |
| Front | (2) | 25' |
| Side/Corner | (2) | 25' |
| Rear | (2) | 30' |
| Parking | | |
| Required Parking | Retail/Restaurant/Office: 1/250 SF Up to 80,000 SF 1/500 Over 80,000 SF Residential: 2.25 / DU | 867 per ITE Parking Generation Publication and ULI Shared Parking Publication ⁽³⁾⁽⁴⁾ |
| Standard Parking Stall Width | 9.5' | 9 ⁽³⁾ in non-high turnover areas |
| Parallel Parking Stall Width | 10' | 9 ⁽³⁾ |
| 90 Degree Parking Drive Aisles | 25' | 24 ⁽³⁾ |
| Angled Parking Drive Aisles | 25' | 20 ⁽³⁾ |
| Loading Space Quantity | 21 | 13 ⁽³⁾ |
| Loading Space Dimensions | 15' x 55' | 14' x 40 ⁽³⁾ |
| Landscape | | |
| Buffer Type A/F (South) | 10' with fence | 10' without fence along southern property line ⁽³⁾ |
| Type C (North) | 20' | 20' |
| Type C (East) | 20' | 20' |
| Landscape Buffer Path | 10' width in addition to Required Buffer | Provide 20' buffer Sidewalk in Stribling ROW 8' width Sidewalk in SR 7 ROW existing 5' width ⁽³⁾ |
| Planned Development Landscape Requirements | 30% increase in size or quantity per PUD requirements | 30% increase in size or quantity per PUD requirements |
| Additional Palm/Tree Plantings | 1 tree per 20 linear feet in required landscape buffer for pods with buildings above 35' | 1 tree per 20 linear feet in required landscape buffer for pods with buildings above 35' |
| Foundation Planting | 5' for each building story or a maximum of 25' | 5' for each building story or a maximum of 25' – or equivalent planting area provided at or near vicinity of building ⁽³⁾ |
| Internal Lot Trees | 1403 Trees – Pod A and Pod B | 1050 Trees – 75% of Requirement – Pod A and Pod B ⁽³⁾ |
| ⁽¹⁾ Any development standards not specifically outlined and/or requested/approved within the PSM shall be per the LDR, as determined by Wellington for the specific standard and/or use/structure. ⁽²⁾ Determined by Development Order ⁽³⁾ Deviation from standard Village of Wellington Development Regulations ⁽⁴⁾ Subject to Shared Parking Study | | |

PARKING

Pod B - Mixed Use

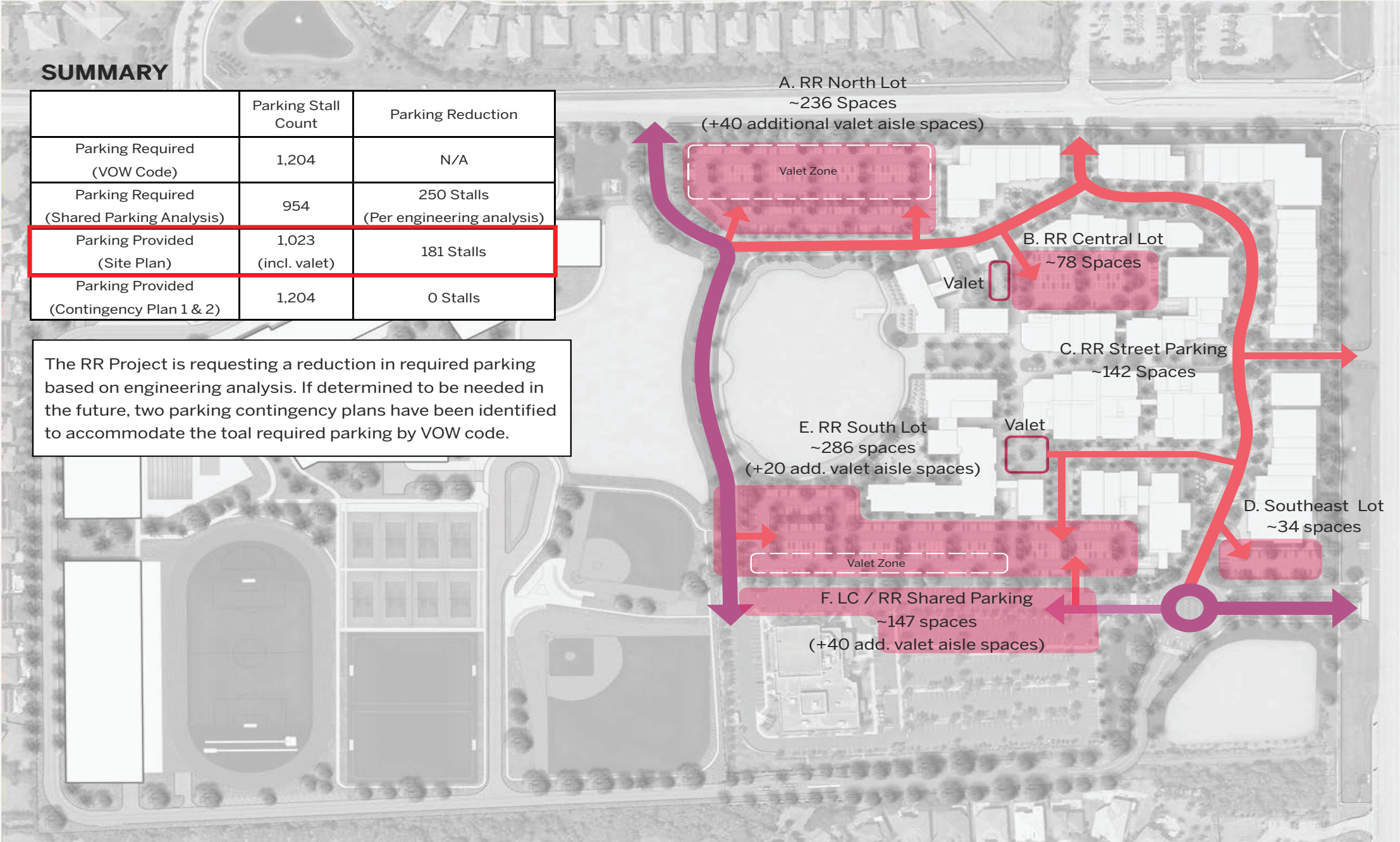
Required Parking

- For Pod B, required off-street parking shall be provided in accordance with the minimum parking supply established in the Shared Parking Study prepared by Simmons & White, utilizing ITE Parking Generation, 6th Edition and ULI Shared Parking, 3rd Edition, as approved by the Village.
- Per the Village Code, both the retail and office parking rates are based on net square feet and not gross square feet. Village Code also allows for parking to be calculated at 1 space per 500 SF for all area above 80,000 SF within a MUPD.

The proposed uses, sizes and code required parking for Phase 1 of the approved development may be summarized as follows:

- Retail/Office Up to 80,000 SF - (1 Space / 250 SF)
- Retail/Office Over 80,000 SF (1 Space / 500 SF)
- Hotel - (1.25 Spaces / Room + 1 per employee)
- Residential – 2.25 Spaces per Unit
- TOTAL = 1,204 Spaces

- The anticipated maximum parking demand and supply (867 spaces without a 10% buffer; 954 spaces with a 10% buffer) as documented in the Shared Parking Study shall serve as the controlling minimum for Pod B.
- Pod B is designed to provided 1,023 parking spaces, inclusive of 147 shared spaces located within the LifeChurch parking lot south of Pod B, and 100 valet spaces dispersed throughout the lot north of the Pod O lake, south of the Pod O lake, and within the LifeChurch parking lot.
- Should it be determined that the Shared-Parking Study isn’t effectively providing required parking to serve the visitor demand, the Applicant has prepared two (2) contingency plans to provide the ULDC code prescribed minimum parking of 1,204 spaces. The first contingency plan consists of providing 181 shared parking on the Pod A School site where the baseball and softball fields are located to meet the requirement. The second contingency plan consists of providing two (2) potential parking garages located at the lot north of the Pod O lake and south of the Pod O lake within Pod B.
- The Parking Contingency Plans for Pod B are as follows:



Parking - VOW Code

| Use | Size | Total |
|----------------|--|-------|
| Retail/ Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/ Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (1.25 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Shared Parking Analysis

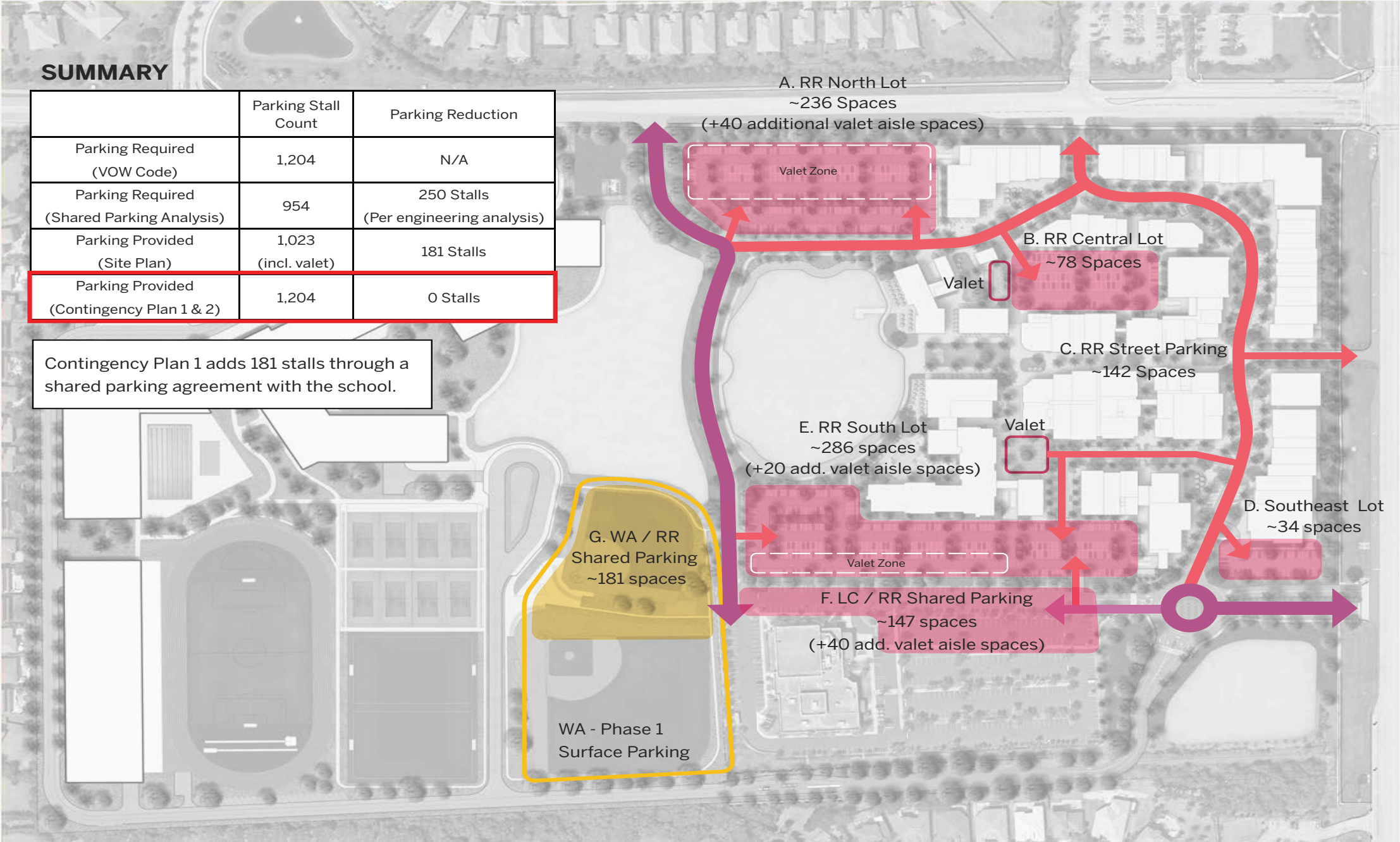
| |
|---------------------------------------|
| 867 Peak Parking Demand |
| 954 Parking Demand (incl. 10% buffer) |

Parking - Site Plan

| Zone | Parking Stalls | Valet | Total |
|-------|----------------|-------|-------|
| A | 236 | 40 | 276 |
| B | 78 | | 78 |
| C | 142 | | 142 |
| D | 34 | | 34 |
| E | 286 | 20 | 306 |
| F | 147 | 40 | 187 |
| TOTAL | 923 | 100 | 1023 |

RR Development Parking Plan





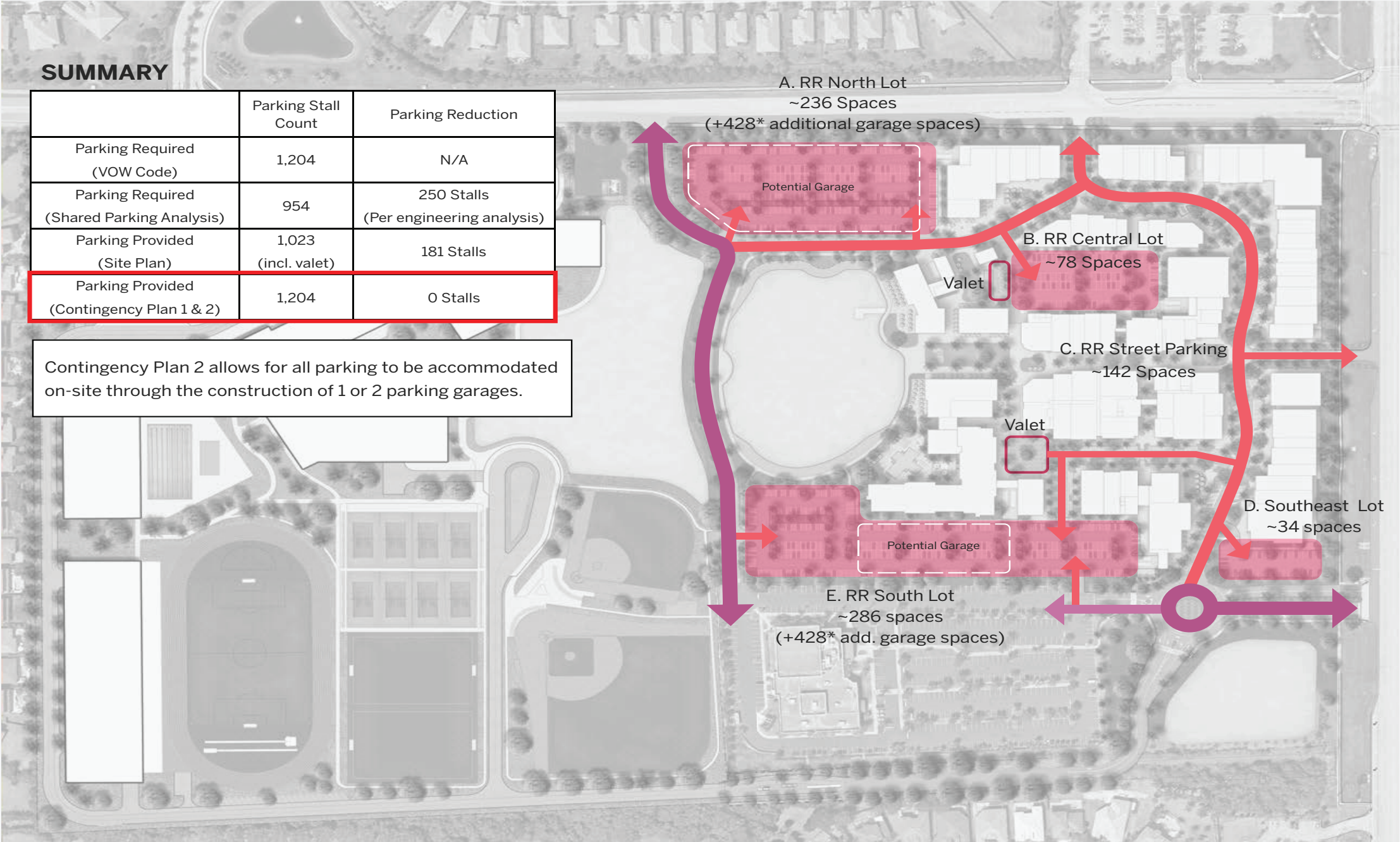
Parking - VOW Code

| Use | Size | Total |
|----------------|--|-------|
| Retail/ Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/ Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (1.25 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Contingency Plan 1

| Zone | Parking Stalls | Valet | Shared | Total |
|-------|----------------|-------|--------|-------|
| A | 236 | 40 | | 276 |
| B | 78 | | | 78 |
| C | 142 | | | 142 |
| D | 34 | | | 34 |
| E | 286 | 20 | | 306 |
| F | 147 | 40 | | 187 |
| G | | | 181 | 181 |
| TOTAL | 923 | 100 | 181 | 1204 |

RR Development Parking Contingency Plan 1



Parking - VOW Code

| Use | Size | Total |
|----------------|--|-------|
| Retail/ Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/ Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (1.25 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Contingency Plan 2

| Zone | Parking Stalls | Garage / Valet | Total |
|-------|----------------|----------------|--------|
| A | 236 | (428*) | (236)* |
| B | 78 | | 78 |
| C | 142 | | 142 |
| D | 34 | | 34 |
| E | 286 | (428)* | (286)* |
| TOTAL | 776 | 428 | 1204 |

* 428 stalls to be added in one or two garages in Zones A and/or E

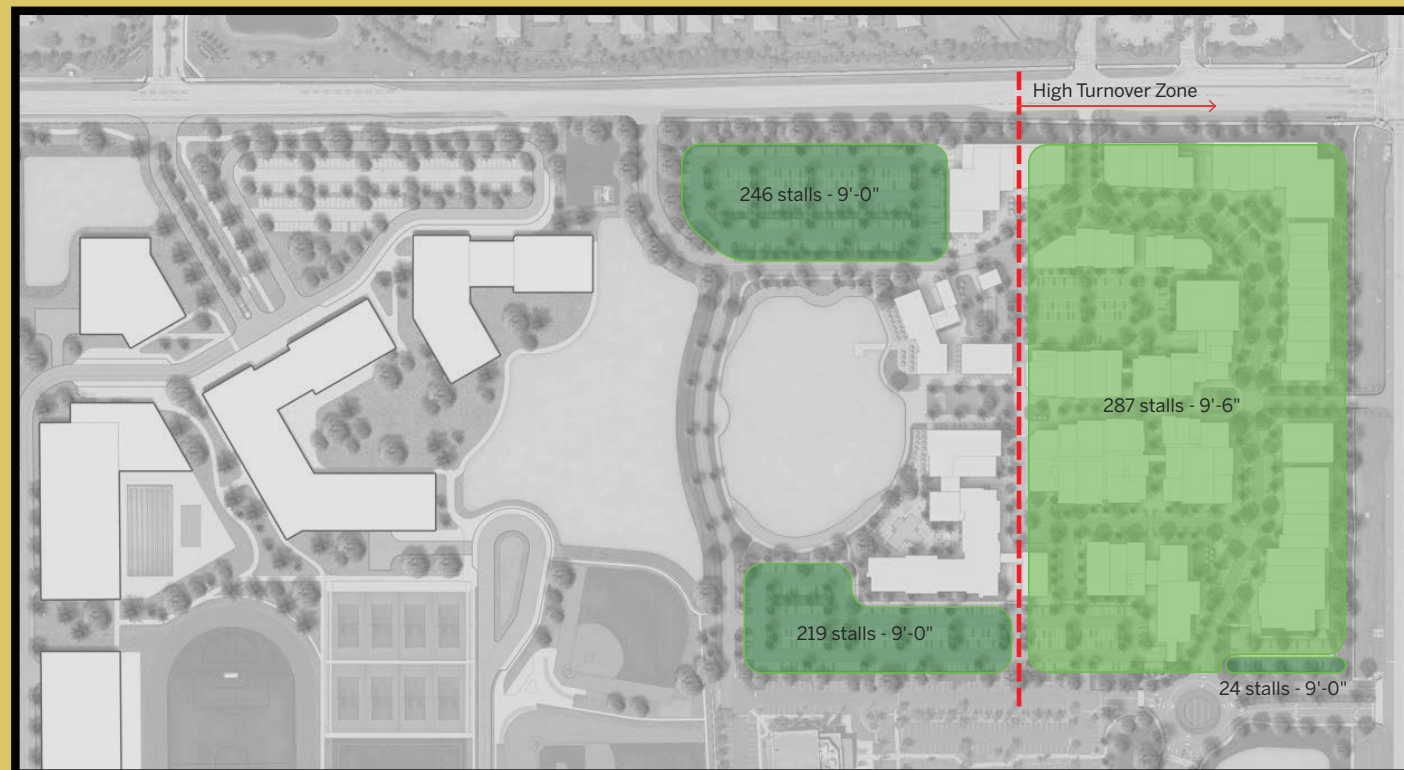
RR Development Parking Contingency Plan 2



PARKING

Parking Stall Width: Non High-Turnover Zones

- All parking spaces within designated High-Turnover Zones in Pod B (including the Main Street corridor and the primary retail storefront parking fields) shall maintain a minimum width of 9.5 feet.
- In parking areas designated as Non High-Turnover Zones on the below exhibit, including parking lots west of Building A1 and south of the hotel, standard stall width may be reduced to a minimum of 9.0 feet.
- High-Turnover Zones are defined as the most trafficked and valuable parking areas within Pod B—those closest to key destinations and most convenient for short-term retail visitors rather than employees or regular users. These areas are intended to support frequent turnover and quick access to storefronts.



Parallel Parking Stall Width

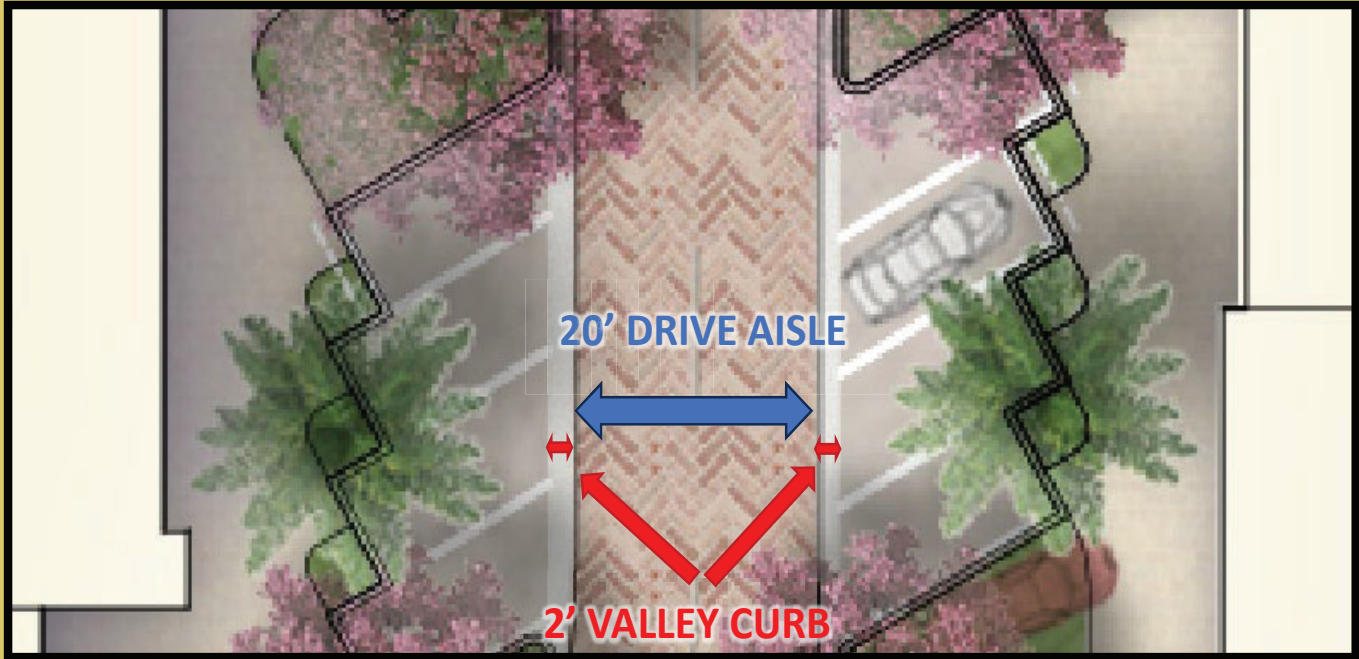
- Parallel parking spaces located on internal Pod B streets shall have a minimum width of 9.0 feet, in lieu of the 10-foot width required by Section 7.5.4.
- This standard applies to parallel parking along the internal driveway extending from the State Road 7 access and along the driveway providing access to the hotel.
- The narrower stall width remains functional and safe given the slower travel speeds, traffic-calmed environment, and shorter vehicle maneuvering distances typical of a village-scale street network. Additionally, reducing the width to 9 feet allows for a more efficient allocation of right-of-way, supporting wider sidewalks, enhanced streetscape features, and improved pedestrian connectivity.



PARKING

90-Degree and Angled Parking Drive Aisles

- In order to promote efficient site planning, traffic calming, preservation of pedestrian-oriented open spaces and landscaping without compromising the maneuverability of passenger vehicles or the functionality of parking areas within Pod B, the minimum two-way drive aisle width serving 90-degree parking spaces shall be 24 feet, in lieu of the 25-foot width required by Table 7.5.2.
- For 60-degree angled parking spaces, the minimum two-way drive aisle width shall be 20 feet, measured between faces of valley curb. When including two (2) feet of valley curb on each side, the effective cross-section is approximately 24 feet, consistent with typical industry standards.

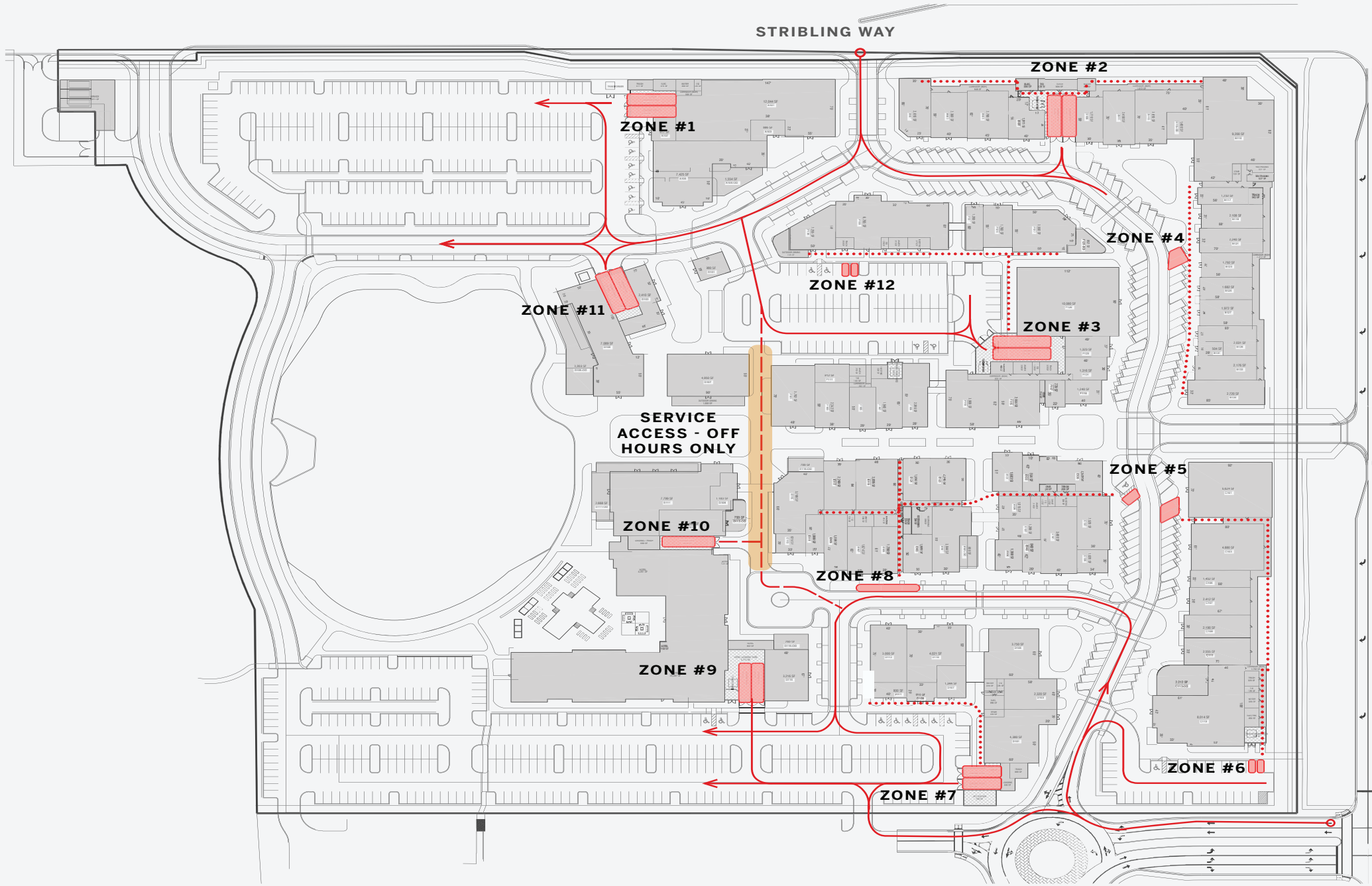


Loading Space Quantity and Dimensions – Pod B

- Loading for the Pod B mixed-use program shall be provided in accordance with the following modified standards, in lieu of Section 7.5.3 and Table 7.5.3.

| HOTEL - 175,000 GSF | | | |
|-------------------------------------|----------------|----------------------|----------------|
| Zoning | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' x 55' @ 1/50,000 SF OVER 100K | 2 | | |
| TOTAL HOTEL | 9 | 14' X 50' | 2 |
| RETAIL + F&B + OFFICE - 355,000 NSF | | | |
| ZONING | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | 14' X 40' | 2 |
| 15' x 55' @ 1/50,000 SF OVER 100K | 5 | 14' X 50' | 9 |
| TOTAL RETAIL + F&B + OFFICE | 12 | | 13 |
| TOTAL REQUIRED POD B | | | |
| | 21 | TOTAL PROPOSED POD B | 13 |

- The minimum loading provision for Pod B shall therefore be thirteen (13) loading spaces, with the dimensions and allocations shown above. The standard requirement for twenty-one (21) spaces at 15 feet by 55 feet shall not apply within Pod B.
- The proposed reduction in loading space and loading quantity are directed by Applicant coordination with internal operations teams and consultants as supported in the Building Logistics Analysis Support Letter prepared by Lerch Bates which is based off of established use patterns and knowledge of standard delivery truck sizes that regularly serve these types of tenants. The proposed loading deviation represents the actual operational needs of the project and avoids intrusion on potential land area for pedestrian comfort and open space.
- A portion of deliveries will also be made via parcel / courier / mail trucks / and vans that will make brief deliveries in the standard parking spaces offered on site.
- The proposed loading locations for Pod B are as follows:



- Zone 1 - 2 * 14' x 50' Loading Bays
- Zone 2 - 2 * 14' x 50' Loading Bay
- Zone 3 - 2 * 14' x 50' Loading Bays
- Zone 4 - 2 * 9' x 18.5' Van Loading*
- Zone 5 - 3 * 9' x 18.5' Van Loading*
- Zone 6 - 2 * 9' x 18.5' Van Loading*
- Zone 7 - 2 * 14' x 40' Loading Bays
- Zone 8 - Parallel Parking / Lay-by
- Zone 9 - 2 * 14' x 50' Loading Bays
- Zone 10 - 1 * 14' x 50' Loading Bays
- Zone 11 - 2 * 14' x 50' Loading Bays
- Zone 12 - 2 * 9' x 18.5' Van Loading*

Totals: 23 Loading Bays

- 11 * 14'x50' Loading Bays
- 2 * 14'x40' Loading Bays
- 9 * 9' x 18.5' Loading Bays*
- 1 * Lay-By

*Van Loading hours limited

All anticipated vehicles sized to fit in provided stall sizes.

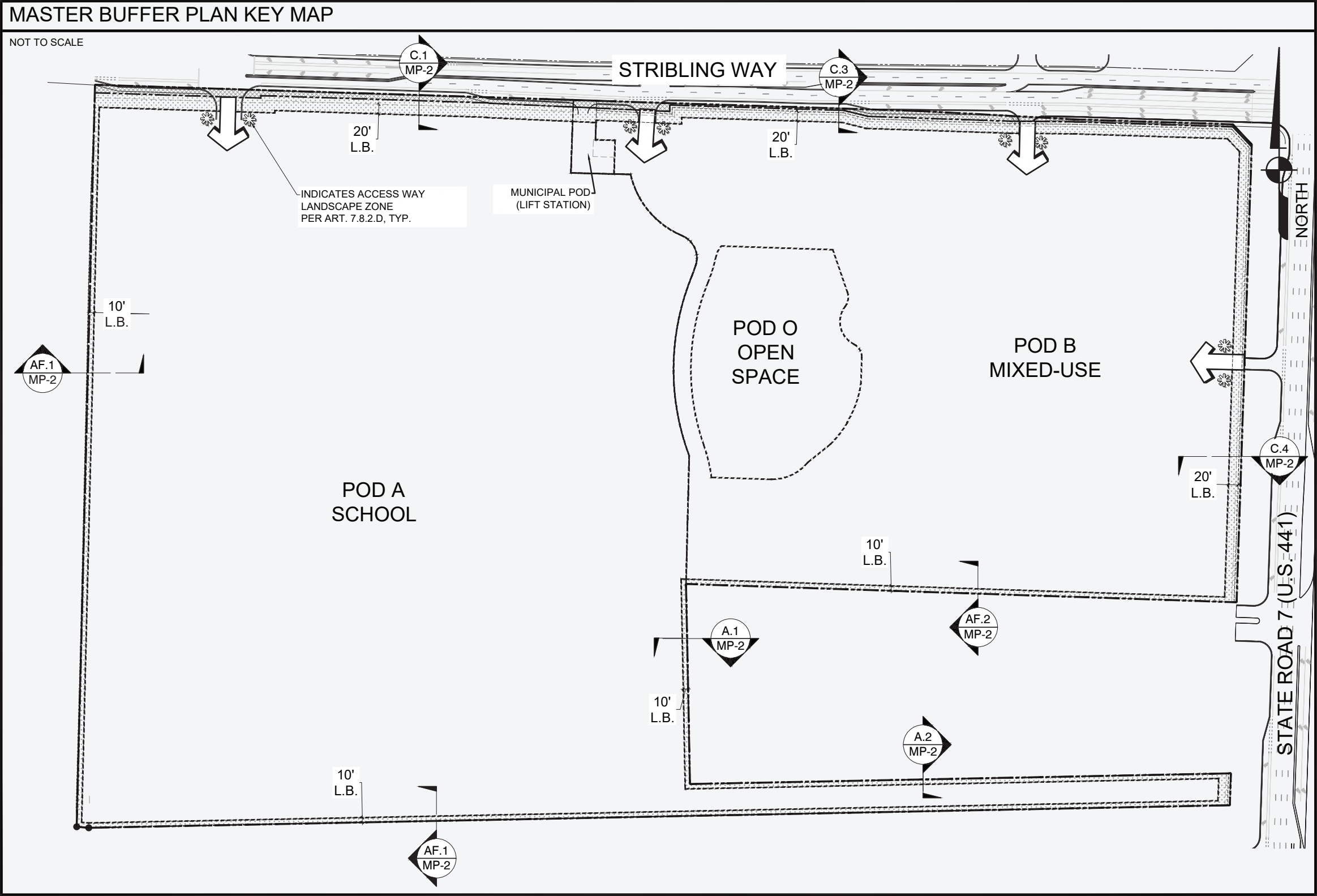
- LOADING ZONE
- LOADING CORRIDOR / PATH
- VEHICULAR SERVICE CIRCULATION

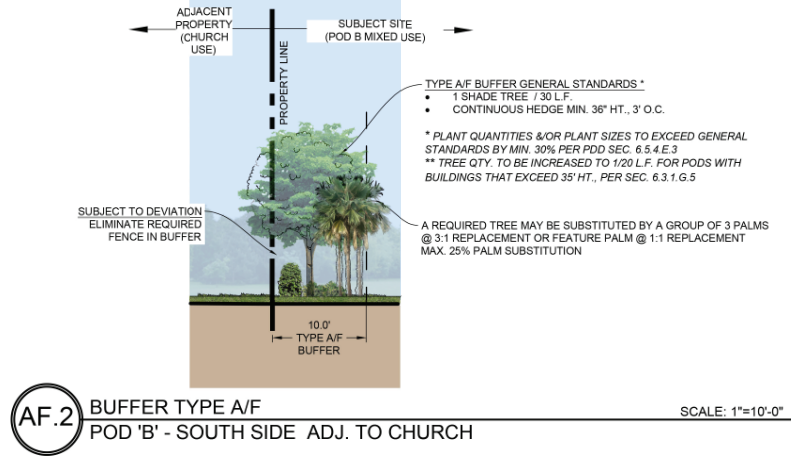




LANDSCAPE STANDARDS

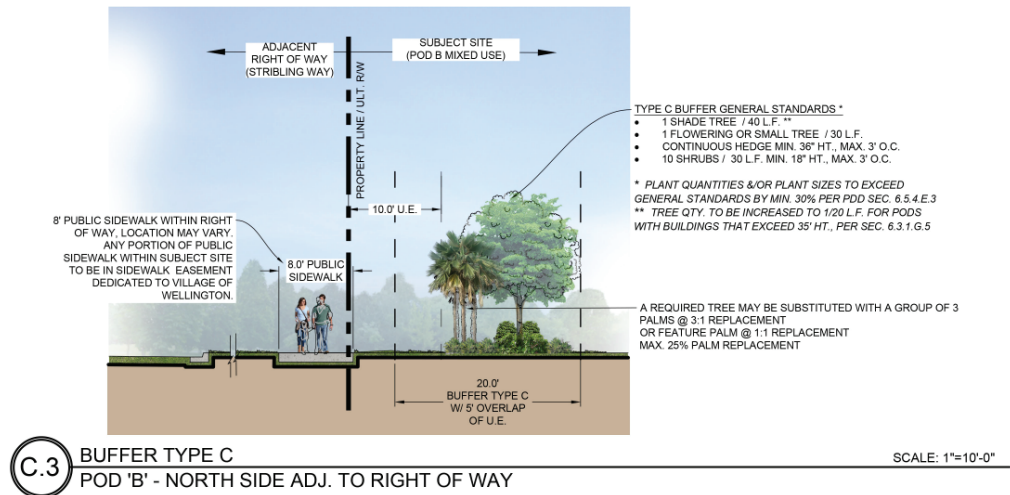
As part of the requested MUPD, a master buffer plan has been provided for the MUPD site perimeter, assessing the use and surrounding property for each pod. Section 6.5.4.E.3, which prescribes that landscape standards shall exceed the Article 7 landscape standards of the LDR by a minimum of 30 percent of the general standard, will be met through a combination of 30% increases in plant size or 30% increases in plant quantity as applicable for each pod. The 30% increases will be further detailed on the conceptual landscape plans provided at time of Site Plan approval or Conditional Use approval for each pod.





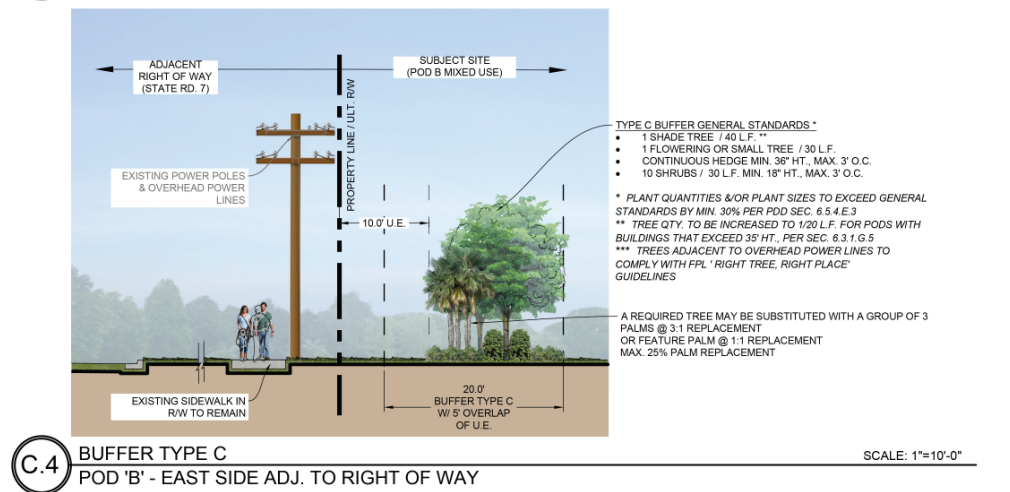
Buffer Type AF.2

- Whereas a 10' perimeter buffer is required for a Type A/F buffer, a 10' perimeter buffer is located at the shared property line is provided along the south side of Pod B adjacent to the church property. A deviation to eliminate the required fence within the buffer is requested in order to allow integration between the Mixed-Use Village and the Church property.



Buffer Type C.3

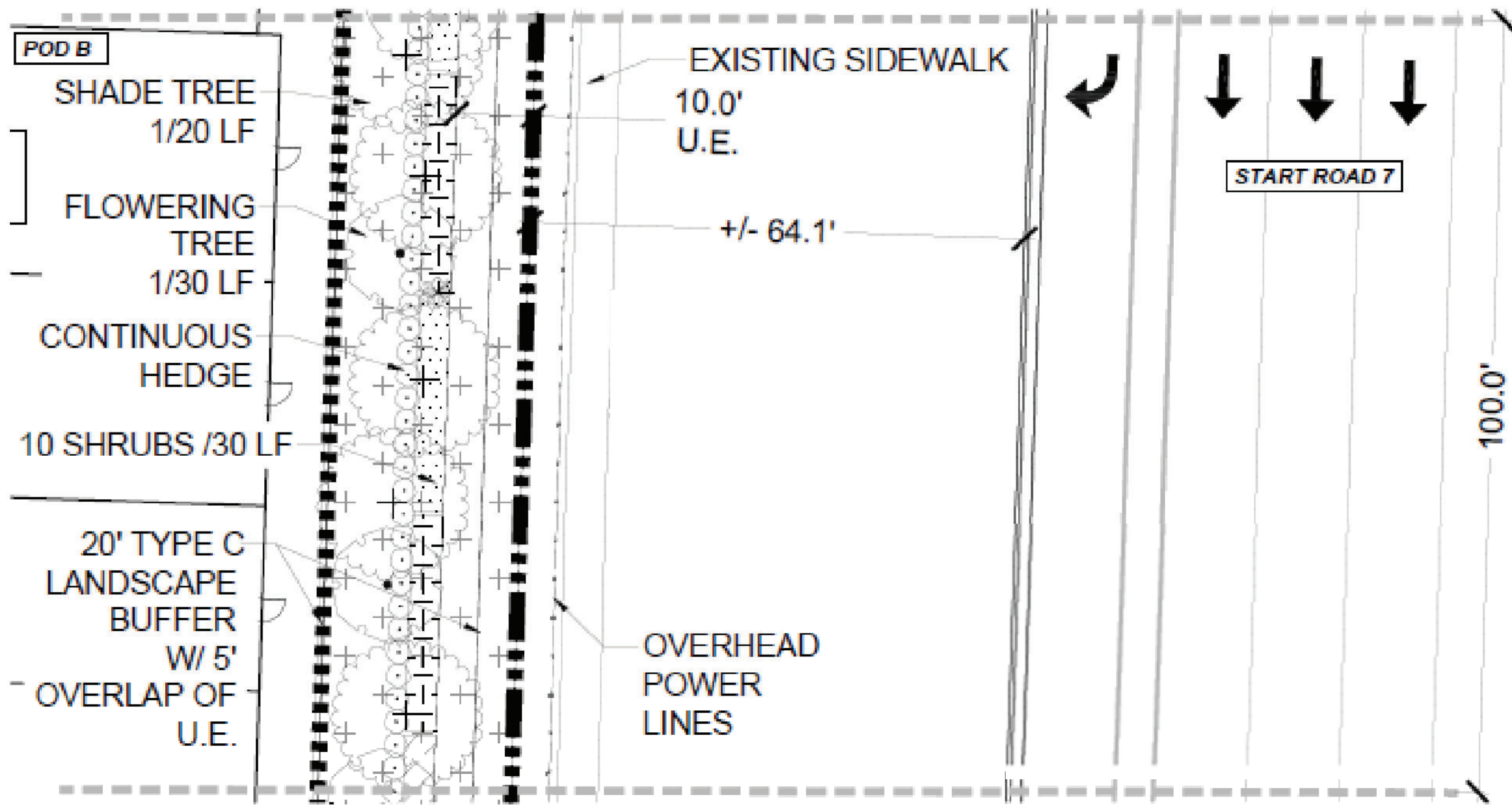
- A 20' Type C buffer with a 5' utility easement overlap is provided along the north side of Pod B adjacent to the Stribling Way ROW. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the Applicant proposes to maintain the width of the existing sidewalk within the Stribling Way ROW at 8' with locations to vary. The portion of the 8' sidewalk that lands within the subject site shall be dedicated to the Village of Wellington via Sidewalk Easement.



Buffer Type C.4

- A 20' Type C buffer with a 5' utility easement overlap within the buffer is provided along the east side of Pod B adjacent to the State Road 7 ROW. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the existing sidewalk within the ROW will connect into Pod B where a minimum 8' wide publicly accessible sidewalk shall be provided along the storefronts of the commercial buildings.

POD B - EAST LANDSCAPE BUFFER (100' CROSS SECTION) PLAN VIEW



T R E E S



Bursera simaruba
Gumbo Limbo



Clusia rosea
Pitch Apple



Conocarpus erectus
Green Buttonwood



Conocarpus erectus var. sericeus
Silver Buttonwood



Coccoloba uvifera
Seagrape



Elaeocarpus decipiens
Japanese Blueberry



Filicium decipiens
Japanese Fern Tree



Juniperus virginiana
Juniper Brodie



Ligustrum japonicum
Japanese Privet



Pandanus utilis
Screw Pine



Plumeria
Plumeria



Quercus virginiana
Live Oak



Sweetenia mahogani
Mahogany



Tabebuia caraiba
Trumpet Tree



Tibouchina granulosa
Tibouchina

P A L M S



Archontophoenix alexandrae
King Alexander Palm



Bismarckia nobilis
Bismarck Palm



Cocos nucifera
Coconut Palm



Cycas revoluta
King Sago



Dypsis lutescens
Areca Palm/ Bamboo Palm



Livistona chinensis
Chinese Fan Palm



Phoenix dactylifera 'Medjool'
Medjool Date Palm



Phoenix reclinata
Senegal Date Palm



Phoenix sylvestris
Sylvester Date Palm



Ravenala madagascariensis
Traveler's Palm



Rhaps excelsa
Lady Palm



Roystonea regia
Royal Palm



Sabal palmetto
Sabal Palm



Seroeno repens 'Cinera'
Silver Saw Palmetto



Washingtonia robusta
Washingtonia Palm

SHRUBS, GRASSES & GROUNDCOVERS



Neomarica carulea 'Regina'
Giant Apostle's Iris



Nephrolepis exaltata
Boston Fern



Philodendron selloum
Philodendron



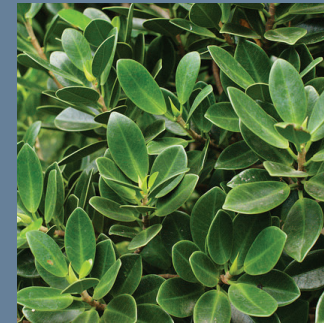
Philodendron x 'Xanadu'
Xanadu Cut Leaf Philodendron



Chrysobalanus icaco 'Horizontal'
Horizontal Cocoplum



Podocarpus macrophyllus
Japanese Yew



Ficus macrocarpa
Green Island Ficus



Hamelia patens
Firebush



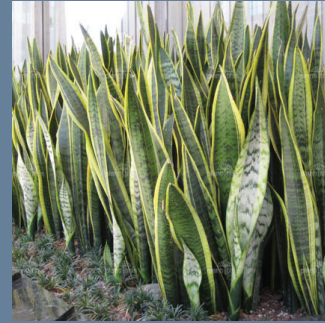
Lantana montevidensis
Purple Weeping Lantana



Microsorium scolopendrium
Wart Fern



Philodendron 'Rojo Congo'
Red Congo



Sansevieria trifasciata
Snake Plant



Strelitzia reginae
Bird of Paradise



Viburnum odoratissimum
Sweet Viburnum



Zamia furfuracea
Cardboard palm



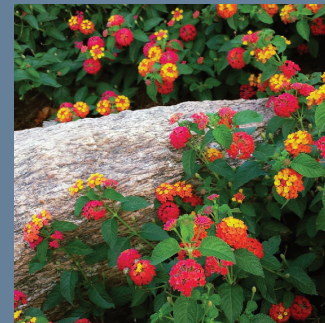
Zamia pumila
Coontie



Gardenia jasminoides 'Miami Supreme' - Cape Jasmine



Helianthus Debilis
Dune Sunflower



Lantana camara
Lantana



Spathoglottis plicata
Purple Ground Orchid



Tripsacum dactyloides
Fakahatchee Grass



Pennisetum setaceum 'Alba'
White Fountain Grass



Trachelospermum asiaticum
Asiatic Jasmine

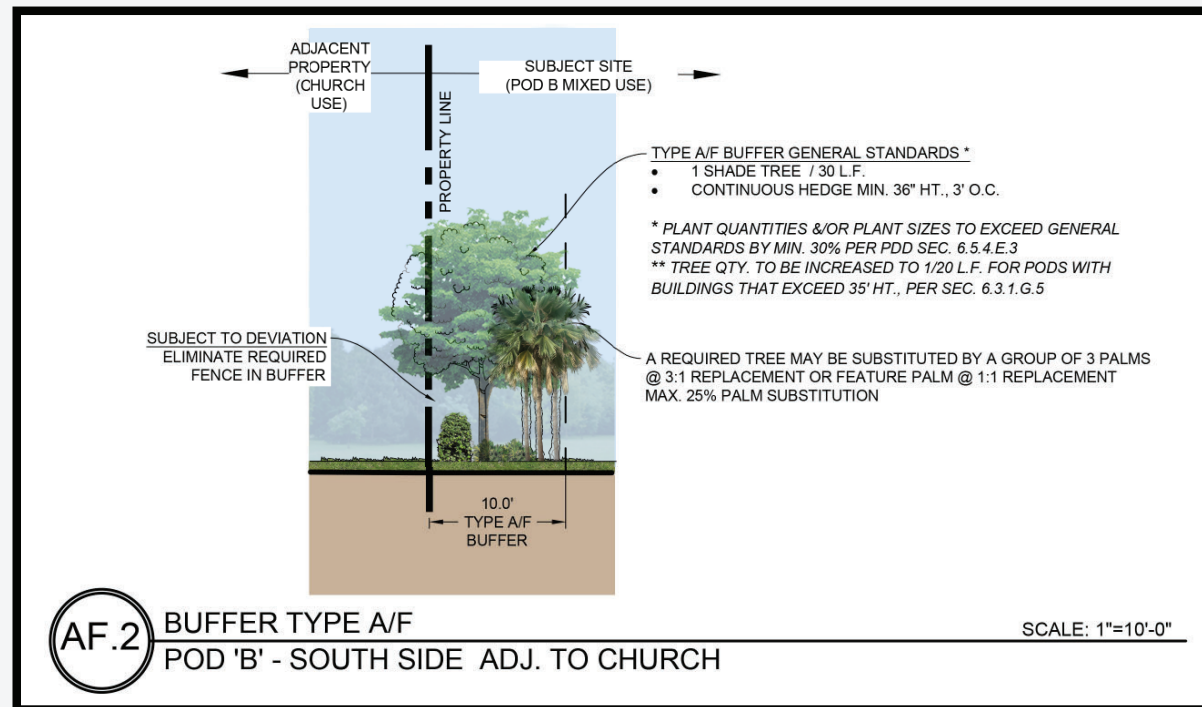


Trachelospermum jasminoides
Confederate Jasmine

LANDSCAPE DEVIATIONS

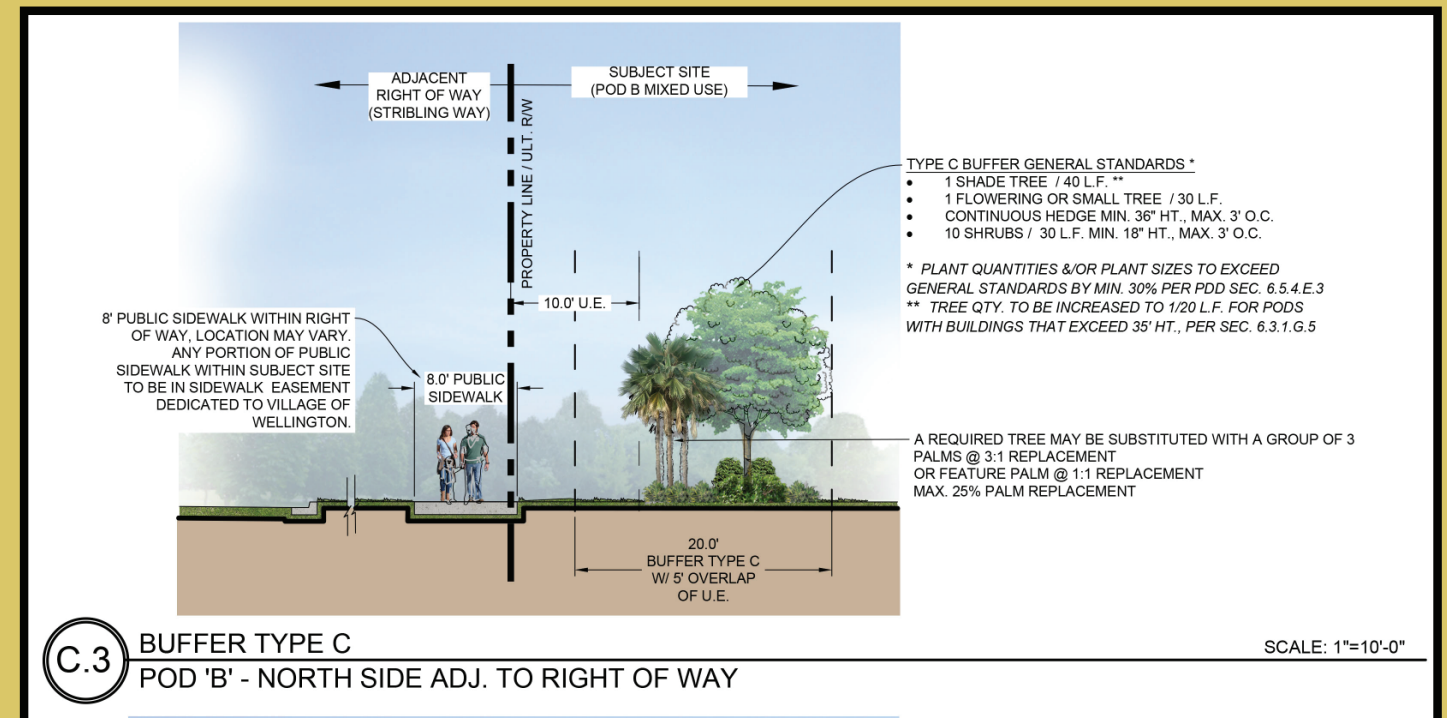
Buffer Type A/F – South (Life Church)

- At the request of Life Church, the Applicant is proposing to remove the fence to promote a more integrated and connected transition between the church and the Mixed-Use Village
- Along the south property line of Pod B adjacent to the Life Church parcel, a 10-foot-wide Type A/F buffer shall be provided without the requirement for a fence or wall. The existing landscaping on the Life Church property shall function in combination with the Pod B buffer to provide screening and transition between the properties.
- This standard supersedes the fence requirement typically associated with a Type A/F buffer along this shared boundary.



Landscape Buffer Perimeter Path

- Within Pod B, no additional ten (10) feet of landscape buffer width shall be required for a perimeter circulation path pursuant to Section 6.5.4.7.
- A minimum 8-foot clear pedestrian path shall be provided along the mixed-use storefronts and internal streets, with direct connections to the sidewalks within the State Road 7 and Stribling Way rights-of-way. This internal and external network shall collectively satisfy the perimeter circulation intent of the Code.

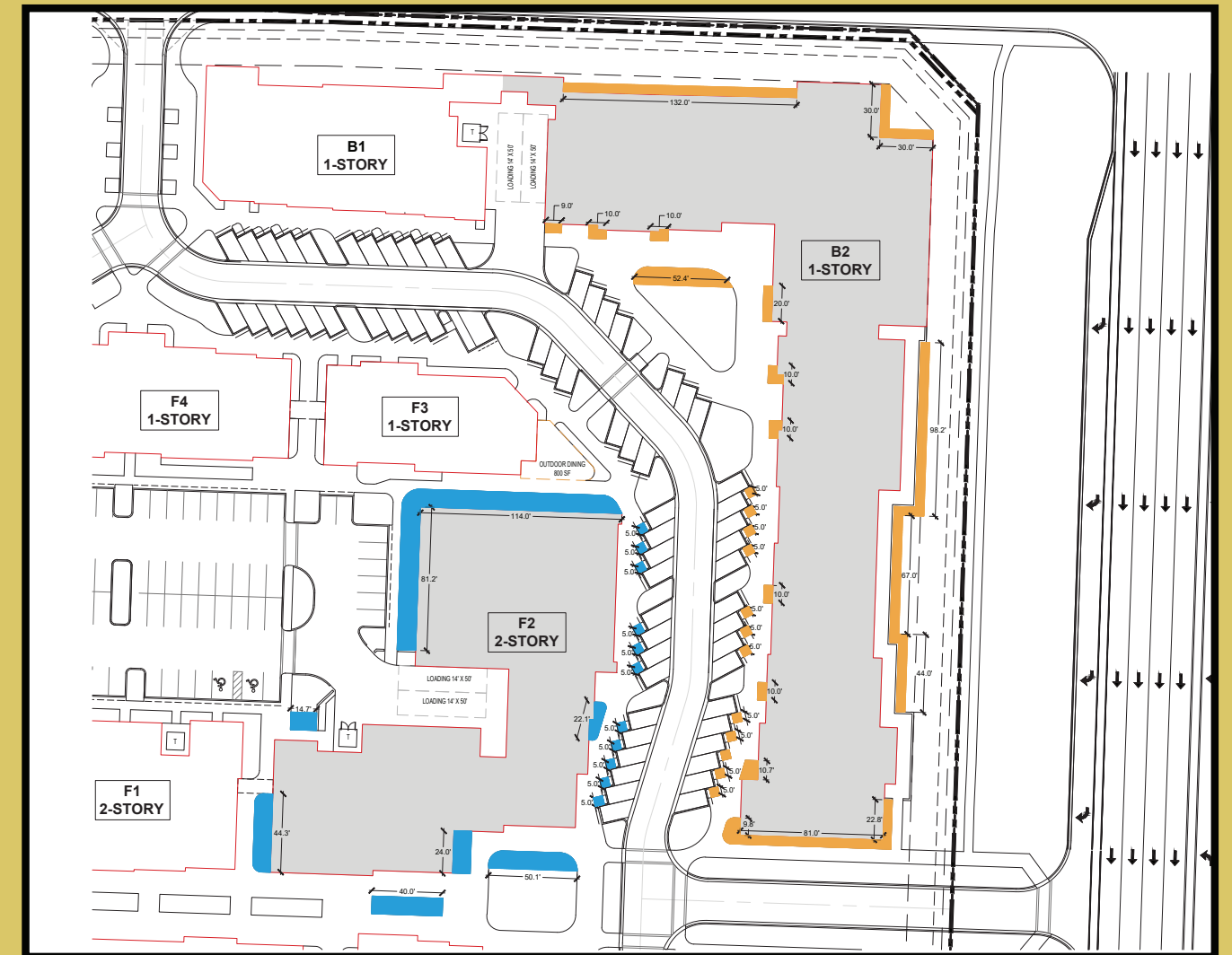


LANDSCAPE DEVIATIONS

Foundation Planting- Pod B

- Foundation planting requirements for buildings within Pod B may be met by providing an equivalent aggregate square footage of landscape area per building, located at or in visual and functional association with the corresponding building frontage, rather than as continuous planting strips directly adjacent to each façade as required by Section 7.8.2.B.2.
- Planters, shade trees, shrubs, and groundcover located across pedestrian walkways—while maintaining an 8-foot clear pedestrian path—may be counted toward the required foundation planting area where they create a landscaped separation between vehicular travel lanes and walkways and are coordinated with the building architecture.
- The total planting area associated with each building shall be at least equal to the area otherwise required by Section 7.8.2.B.2.
- A typical configuration of foundation planting for Pod B is as follows:

K - PARK MUPD



Internal Lot Trees: Site-Wide Standard

- The internal lot tree standard identified for the overall MUPD shall apply to Pod B as part of the total minimum of 1,050 internal trees provided across Pods A and B.
- The final distribution of internal trees within Pod B shall be established through the approved landscape plans, with shrubs meeting or exceeding the Code requirement.

LAND DEDICATIONS

The required land dedications for the two (2) proposed residential phases in the KPARK MUPD are outlined in the table below.

| Sec 6.5.2 Land Dedication Regulations | | |
|---|----------------|------------------------------|
| RESIDENTIAL CALCULATIONS | | |
| PERSONS PER UNIT | DWELLING UNITS | RESIDENTS |
| 2.81 | 15 | 42 |
| 2.81 | 215 | 604 |
| PRIVATE RECREATION REQUIREMENTS – Must Be provided – Can credit 10% to Public Rec | | |
| CALCULATIONS | RESIDENTS | AREA REQUIRED (ACRES) FEE |
| 110 SF/Person | 42 | 0.11 Acres |
| | 604 | 1.53 Acres |
| PUBLIC RECREATION REQUIREMENTS – Can be Paid in Lieu | | |
| CALCULATIONS | RESIDENTS | AREA REQUIRED (ACRES) |
| 5 ACRES / 1,000 PERSONS | 42 | 0.21 Acres |
| | 604 | 3.02 Acres |
| CIVIC REQUIREMENT – Can be Paid In Lieu | | |
| CALCULATIONS | RESIDENTS | AREA REQUIRED (ACRES) |
| 1 ACRE / 1,000 PERSONS | 42 | 0.042 Acres |
| | 604 | .604 Acres |

The table outlines the Village’s required land-dedication calculations for both the 15-unit Phase 1 program and the full 215-unit buildout. Using the standard rate of 2.81 persons per dwelling unit, Phase 1 generates 42 residents, while full buildout generates 604 residents. These population figures are then used to determine the private recreation, public recreation, and civic land requirements.

Private Recreation (must be provided on-site)

- Requirement: 110 sq ft per person
- Phase 1 (43 residents): 0.10 acres
- Full buildout (604 residents): 1.53 acres

Public Recreation (may be paid in lieu)

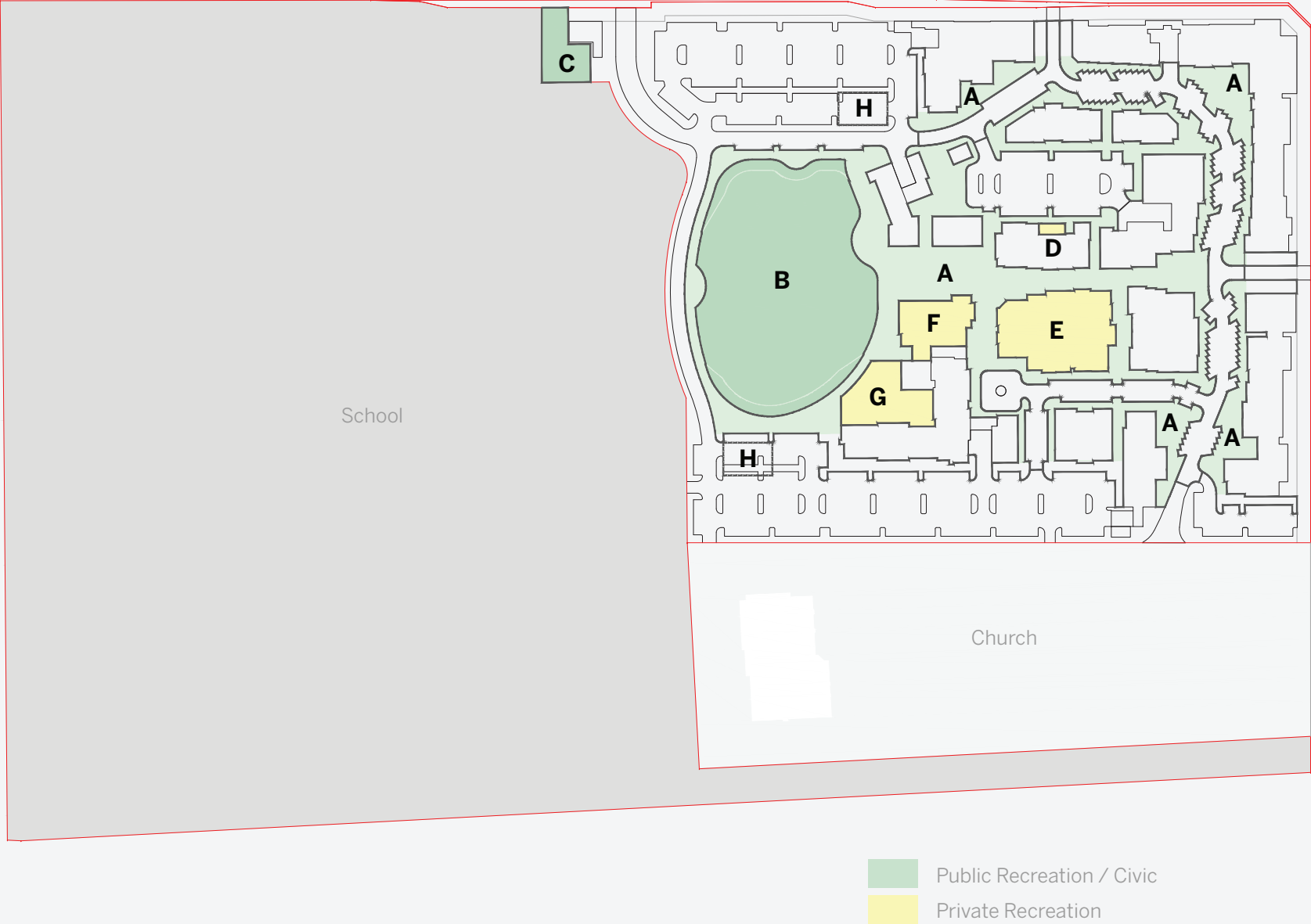
- Requirement: 5 acres per 1,000 residents
- Phase 1: 0.21 acres
- Full buildout: 3.02 acres

Civic Land (may be paid in lieu)

- Requirement: 1 acre per 1,000 residents
- Phase 1: 0.042 acres
- Full buildout: 0.604 acres

Overall, the table provides the formula-driven acreage requirements needed to satisfy the Village’s recreation and civic-dedication standards at both the initial and full buildout stages of the project.

The KPark MUPD required land dedication requirements are met as depicted in the following graphic:



| Residential Calculations | | | | | |
|--|--------------|----------------|----------|--------------|----------------|
| | Phase 1 | Phase 2 | Total | | |
| Units | 15 | 200 | 215 | | |
| Residents | 42 | 562 | 604 | | |
| Public/Civic & Private Recreation | | | | | |
| | Type | Use | Phase | Area (Acres) | Credit (Acres) |
| A | Public/Civic | Streetscape | Phase 1 | 5.07 | 3.8 |
| B | Public/Civic | Lake | Phase 1 | 2.9 | 1.45 |
| C | Public/Civic | Utility | Phase 1 | 0.23 | 0.23 |
| D | Private | Resid. Amenity | Phase 1 | 0.03 | 0.03 |
| E | Private | Fitness | Phase 1 | 0.67 | 0.67 |
| F | Private | Hotel Pool | Phase 1 | 0.34 | 0.34 |
| G | Private | Event Space | Phase 1 | 0.25 | 0.25 |
| H | Private | Resid. Amenity | Phase 2 | 0.26 | 0.26 |
| Public/Civic & Private Recreation Requirements | | | | | |
| | | Phase | Required | Provided | Difference |
| | Public/Civic | Phase 1 | 0.25 | 5.48 | 5.23 |
| | | Phase 2 | 3.62 | 5.48 | 1.86 |
| | Private | Phase 1 | 0.11 | 1.29 | 1.18 |
| | | Phase 2 | 1.53 | 1.55 | 0.02 |



Future Expansion Zones For Up to 200 Additional Residential Units (Or Equivalency)

RECREATION

The project will feature a dynamic Main Street styled shopping and dining experience with generous and pedestrian friendly sidewalks, promenades, and recreation areas. The pedestrian oriented public realm includes public plazas, green spaces and gathering places with lush landscaping, ample hardscape, water features, and public art that will be integrated throughout the site. These spaces can be activated for various uses such as community events, programming, and leisure. On a daily basis the public realm will include many passive and interactive components oriented to families, children, and teens. Conveniently located public parking connecting to an internal pedestrian circulation network will be provided. The internal network will link to the existing Wellington pedestrian and bicycle path system, including connections to adjacent properties. A multi-use lakeside trail for pedestrian use will be integrated with and connected to the retail public realm network. Clear wayfinding will be provided by use of signage throughout the site and at primary project entries.

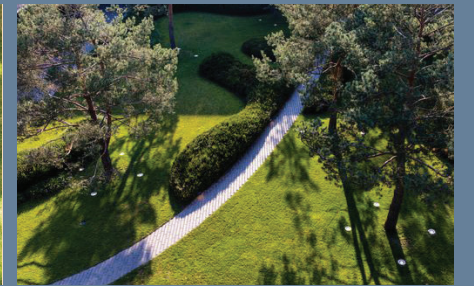


POD O

Pod O: Common Open Space

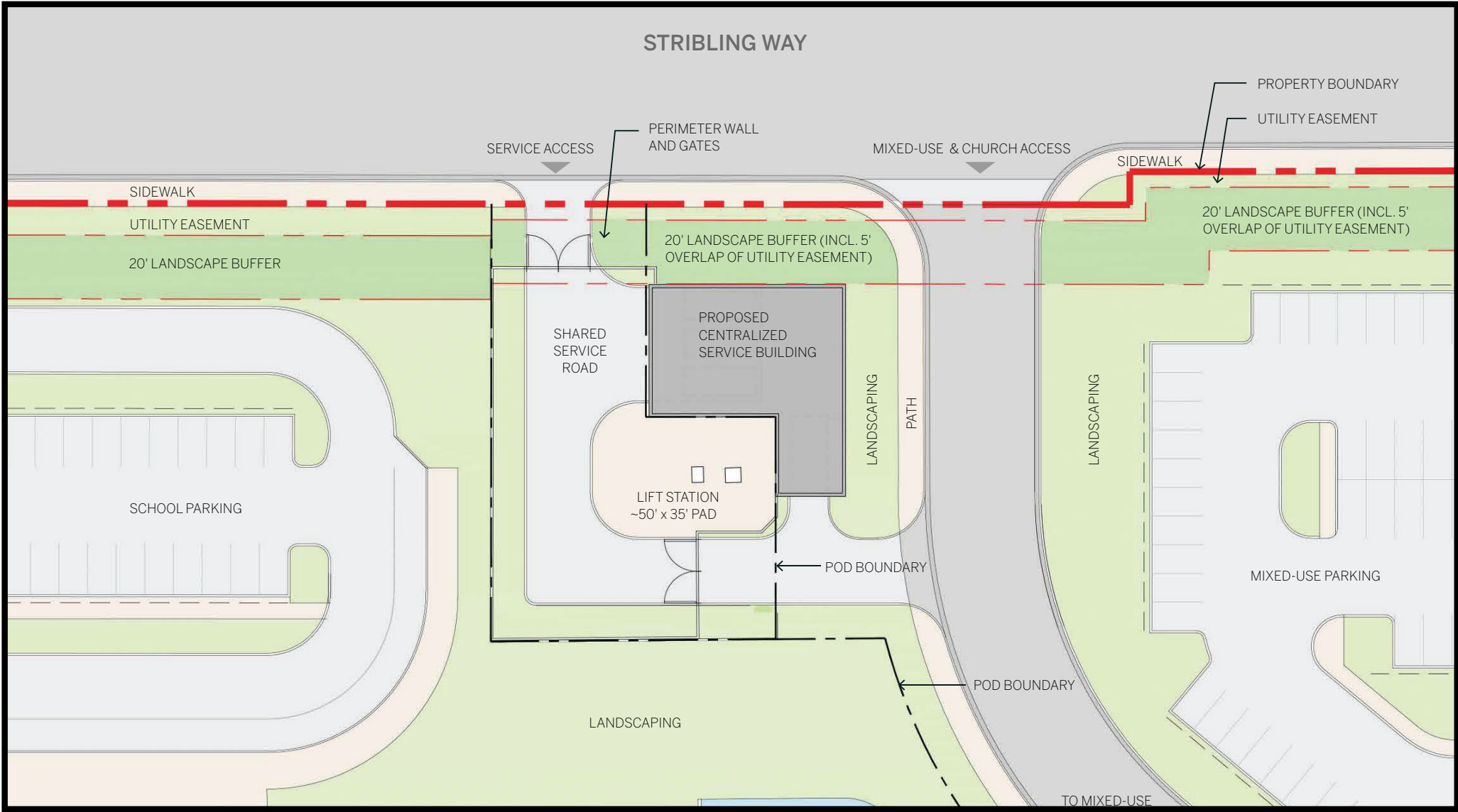
Pod O includes 3.23 acres of publicly accessible open space, centered around a 2.61-acre lake. An 8-foot-wide pedestrian pathway will loop continuously around the lake, serving as a primary recreational and circulation feature. The lake and surrounding trail will be framed by a mix of native and adaptive plantings, including trees, shrubs, and groundcover. Passive recreational amenities, such as scenic overlooks, parklets, informal seating areas, and outdoor dining spaces will be strategically located along the trail to promote walkability, encourage public gathering, and support flexible community use. Aerating fountains may be included in the lake design to enhance visual character and improve water quality as part of the overall stormwater and landscape management strategy.

Centrally located within the K-Park site, the lakeside trail will function as a key pedestrian circulation route within the MUPD development. Direct cross-access to both the adjacent school and commercial mixed-use village will be included. In addition, the trail will connect to the existing Wellington sidewalk network along Stribling Way and will include proposed cross access connections to the Life Church property.



The municipal parcel within the MUPD totals .19 acres excluding ROW dedication & buffer area. The municipal parcel is carved out to allow for the existing public lift-station located on Stribling Way to remain. The access and configuration of the municipal parcel is further detailed in the Project Standards Manual for the MUPD.

The below graphic depicts the anticipated design of the lift station parcel

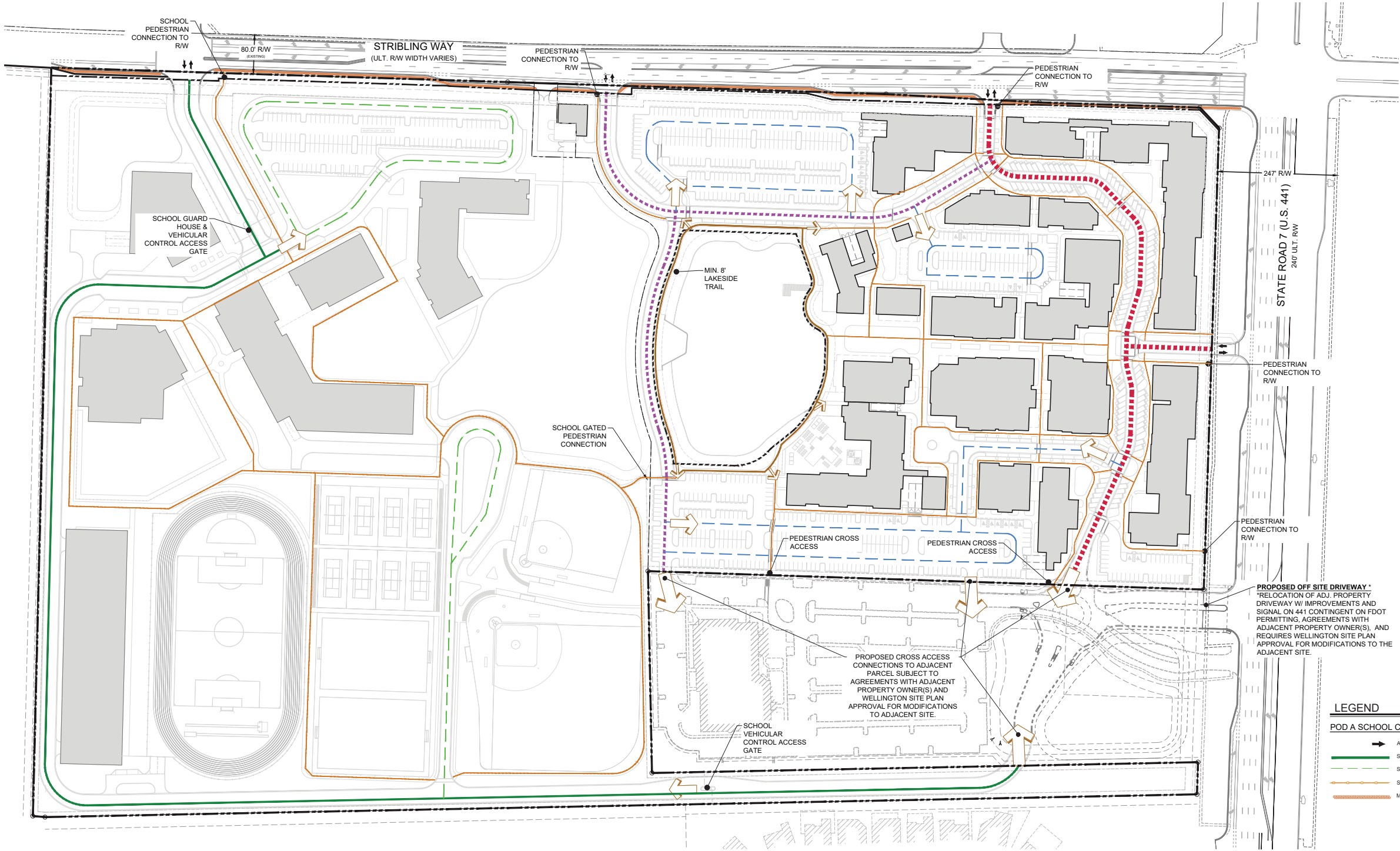


CHAPTER 3:

CIRCULATION



CIRCULATION



LEGEND

POD A SCHOOL CIRCULATION

- ACCESS POINT
- SCHOOL PRIMARY VEHICULAR CIRCULATION
- SCHOOL PARKING CIRCULATION
- SCHOOL PEDESTRIAN CIRCULATION
- MULTI-MODAL PERIMETER SIDEWALK

POD B MIXED USE VILLAGE CIRCULATION

- ACCESS POINT
- 'MAINSTREET' VEHICULAR CIRCULATION
- PRIMARY VEHICULAR CIRCULATION
- PARKING CIRCULATION
- PRIMARY PEDESTRIAN CIRCULATION
- MULTI-MODAL PERIMETER SIDEWALK

Exhibit B
RESOLUTION NO. R2026-04

A RESOLUTION OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A CONDITIONAL USE (PETITION 2025-0001-CU) FOR CERTAIN PROPERTY FORMERLY KNOWN AS K PARK, TOTALING APPROXIMATELY 71.27 ACRES, MORE OR LESS, LOCATED ON THE SOUTHWEST CORNER OF STRIBLING WAY AND US441/SR7, AS MORE SPECIFICALLY DESCRIBED HEREIN, TO ALLOW A 1,750-STUDENT PRIVATE SCHOOL WITH OUTDOOR RECREATIONAL FACILITIES WITHIN POD A OF THE WELLINGTON VILLAGE MUPD; PROVIDING AN EFFECTIVE DATE.

WHEREAS, the Wellington's Council, as the governing body of Wellington, Florida, pursuant to the authority in Chapter 163 and Chapter 166, Florida Statutes, and the Wellington Land Development Regulations, is authorized and empowered to consider petitions related to zoning and development orders; and

WHEREAS, the notice of hearing requirements as provided in Article 5 of the Wellington Land Development Regulations have been satisfied; and

WHEREAS, the Future Land Use Map designation for the subject property was recently changed to Mixed-Use by Ordinance No. 2025-04; and

WHEREAS, the property owner has applied for a companion Rezoning (Ordinance No. 2026-06) and Master Plan Amendment applications (Resolution No. R2026-03) to change the Zoning designation to Multiple Use Planned Development (MUPD) and to adopt the Wellington Village MUPD Master Plan and Project Standards Manual; and

WHEREAS, the development program for Wellington Village MUPD includes a private school within Pod A, which also requires a Conditional Use approval; and

WHEREAS, the Planning, Zoning and Adjustment Board, sitting as the Local Planning Agency, after notice and public hearing on January 21, 2026, recommended _____ of the Conditional Use application with a ____ - ____ vote; and

WHEREAS, the Wellington Council has taken the recommendations of the Local Planning Agency, the Findings of Facts provided in the staff report, and the evidence and testimony presented by the Petitioner and other interested parties, and comments from the public into consideration when considering the proposed Conditional Use; and

WHEREAS, Wellington's Council has made the following Findings of Fact:

1. Environmental Considerations: There are no environmental issues or concerns.

Exhibit B

2. Surrounding Use Considerations: The proposed school is compatible with the surrounding uses on-site and the adjacent properties.
3. Public Facilities: The water, sanitary, sewer, and drainage systems meet the code requirements. Adequate public facilities are available to serve the subject site.
4. Concurrency: The project meets Wellington's Traffic Performance Standards (TPS).
5. Comprehensive Plan: The use requested is consistent with Wellington's Comprehensive Plan.
6. Land Development Regulations: The subject request is consistent with the stated purposes and intent of the Land Development Regulations.
7. Development Pattern: The request will result in a logical development pattern as it is located within a Mixed-Use development.

NOW, THEREFORE, BE IT RESOLVED BY THE WELLINGTON, FLORIDA'S COUNCIL, THAT:

SECTION 1. The Conditional Use to allow a 1,750-student private school with outdoor recreational facilities within Pod A of Wellington Village MUPD (FKA K Park) as described in Exhibit A, is hereby APPROVED subject to the following conditions of approval:

1. A Site Plan, meeting the requirements of Wellington's Land Development Regulations, and the Wellington Village MUPD Project Standards Manual as adopted by Resolution No. R2026-03, shall be certified prior to the issuance of Land Development Permits. (PLANNING AND ZONING)
2. Architectural Review Board approval is required prior to the issuance of building permits for the elevations, colors, materials, signage, etc. Buildings taller than 35-feet shall require a conditional use approval. (PLANNING AND ZONING AND ENGINEERING)
3. Staggered start and stop times for elementary, middle, and high school students shall be staggered a minimum of 30 minutes apart. (TRAFFIC)
4. School circulation shall be per the operational plan dated November 14, 2025, including required loading/unloading personnel, traffic control personnel, and crossing guards, unless modifications are warranted and approved by the Village Engineer. (TRAFFIC)
5. All lighting shall meet the requirements of Section 7.6.1 of Wellington's Land Development Regulations. Only the lighting within the confines of the outdoor athletic areas may be a maximum of 90 feet in height. Lighting within parking

Exhibit B

95 areas shall be a maximum of 30 feet in height, and a maximum of 15 feet in
96 height within non-vehicular pedestrian areas. Adequate lighting shall be
97 provided on site and at the project access points. All lighting shall not create
98 adverse spillover or glare on adjacent properties or rights-of-way. (PLANNING
99 AND ZONING)

- 100
- 101 6. The developer shall be required to take measures to ensure that during site
102 development dust/debris particles from the development do not become a
103 nuisance to the adjacent properties. (PLANNING AND ZONING/ENGINEERING)
104
- 105 7. Confirmation of feasibility and required design standards for private sewer for the
106 proposed private grinder station meeting Palm Beach County Health Department
107 and Florida Building Code will be required at site plan submittal. The interior area
108 of the school not publicly accessible or behind security, shall have private water
109 and sewer systems unless approved by the Utility. (UTILITIES)
110

111 **SECTION 2.** This Resolution shall become effective immediately upon
112 adoption.

113

114 **PASSED AND ADOPTED** this _____ day of _____ 2026.

115

116

117 **ATTEST:**

WELLINGTON, FLORIDA

118

119 BY: _____
120 Chevelle D. Hall, MMC, Village Clerk

119 BY: _____
120 Michael J. Napoleone, Mayor

121

122

123 **APPROVED AS TO FORM AND**
124 **LEGAL SUFFICIENCY:**

125

126

127 BY: _____
128 Laurie Cohen, Village Attorney

Exhibit A – Legal Description for Pod A of Wellington Village

ALL THAT PIECE AND PARCEL OF LAND LOCATED IN THE NORTHEAST ONE-QUARTER (1/4) OF SECTION 24, TOWNSHIP 44 SOUTH, RANGE 41 EAST, PALM BEACH COUNTY, FLORIDA, BEING TRACT C, OAKMONT ESTATES P.U.D., ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 106, PAGE 2, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, TOGETHER WITH PORTIONS OF TRACTS 3 THROUGH 6, IN BLOCK 26, OF PALM BEACH FARMS CO. PLAT NO. 3, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THAT PART OF SAID PALM BEACH FARMS CO. PLAT NO. 3. ROADWAY NORTH OF TRACTS 3 & 4 AND WEST OF TRACTS 4 & 5, SAID BLOCK 26, AS MORE FULLY DESCRIBED IN OFFICIAL RECORD BOOK 13325 PAGE 1198, LESS THE SOUTH 86 FEET OF SAID ROADWAY AND TRACTS 4 & 5.

SAID PARCEL BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTH ONE-QUARTER (1/4) CORNER OF SAID SECTION 24; THENCE SOUTH 01°24'43" WEST, AS A BASIS OF BEARINGS, ALONG THE WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF SAID SECTION 24, SAID LINE ALSO BEING THE WEST LINE OF BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THE EAST LINE OF TRACT B, OAKMONT ESTATES P.U.D. ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 106, PAGE 2, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, A DISTANCE OF 19.57 FEET TO A POINT ON THE NORTH LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THE TRUE POINT OF BEGINNING;

THENCE SOUTH 88°10'28" EAST, ALONG THE NORTH LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, A DISTANCE OF 992.66 FEET TO A POINT;

THENCE DEPARTING SAID NORTH LINE SOUTH 01°59'05" WEST, A DISTANCE OF 151.22 FEET TO A POINT;

THENCE SOUTH 88°34'08" EAST, A DISTANCE OF 123.90 FEET TO A POINT ON A NON-TANGENT CURVE;

THENCE ALONG A NON-TANGENT CURVE TO THE LEFT, HAVING A RADIUS OF 181.00 FEET, THROUGH A CENTRAL ANGLE OF 54°33'27", FOR AN ARC LENGTH OF 172.35 FEET, BEING SUBTENDED BY A CHORD BEARING SOUTH 40°03'21" EAST, A DISTANCE OF 165.91 FEET TO A POINT OF REVERSE CURVATURE;

THENCE ALONG A REVERSE CURVE TO THE RIGHT, HAVING A RADIUS OF 50.00 FEET, THROUGH A CENTRAL ANGLE OF 84°54'46", FOR AN ARC LENGTH OF 74.10

Exhibit B

176 FEET, BEING SUBTENDED BY A CHORD BEARING SOUTH 24°52'41" EAST, A
177 DISTANCE OF 67.50 FEET TO A POINT OF COMPOUND CURVATURE;
178
179 THENCE ALONG A COMPOUND CURVE TO THE RIGHT, HAVING A RADIUS OF
180 289.00 FEET, THROUGH A CENTRAL ANGLE OF 04°41'19", FOR AN ARC LENGTH OF
181 23.65 FEET, BEING SUBTENDED BY A CHORD BEARING SOUTH 19°55'21" WEST, A
182 DISTANCE OF 23.64 FEET TO A POINT OF REVERSE CURVATURE;
183
184 THENCE ALONG A REVERSE CURVE TO THE LEFT, HAVING A RADIUS OF 511.00
185 FEET, THROUGH A CENTRAL ANGLE OF 42°48'55", FOR AN ARC LENGTH OF 381.85
186 FEET, BEING SUBTENDED BY A CHORD BEARING SOUTH 00°51'34" WEST, A
187 DISTANCE OF 373.03 FEET TO A POINT OF NON-TANGENCY;
188
189 THENCE SOUTH 01°24'49" WEST, A DISTANCE OF 265.78 FEET TO THE
190 NORTHWEST CORNER OF TRACT A, LIFE COVENANT CHURCH WELLINGTON
191 CAMPUS, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT
192 BOOK 115, PAGE 44, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA;
193
194 THENCE ALONG THE WEST LINE OF SAID TRACT A, SOUTH 01°07'58" EAST, A
195 DISTANCE OF 415.00 FEET TO A POINT;
196
197 THENCE ALONG THE SOUTH LINE OF SAID TRACT A, NORTH 88°52'02" EAST, A
198 DISTANCE OF 1,122.03 FEET TO THE SOUTHEAST CORNER OF SAID TRACT A, SAID
199 POINT ALSO BEING A POINT ON THE WESTERLY RIGHT OF WAY LINE OF U.S.
200 HIGHWAY 441 (STATE ROAD No. 7), AS SHOWN ON THE FLORIDA DEPARTMENT
201 OF TRANSPORTATION RIGHT-OF-WAY MAP, SECTION 93210-2519, SHEET 9 OF
202 27, REVISION DATE 02-01-96, AND THE NORTHEAST CORNER OF TRACT C,
203 OAKMONT ESTATES P.U.D., ACCORDING TO THE MAP OR PLAT THEREOF,
204 RECORDED IN PLAT BOOK 106, PAGE 2, PUBLIC RECORDS OF PALM BEACH
205 COUNTY, FLORIDA;
206
207 THENCE ALONG SAID RIGHT-OF-WAY LINE AND ALONG THE EAST LINE OF SAID
208 TRACT C, SOUTH 01°53'57" WEST, A DISTANCE OF 66.09 FEET TO THE
209 SOUTHEAST CORNER OF SAID TRACT C;
210
211 THENCE DEPARTING SAID WEST RIGHT-OF-WAY LINE, SOUTH 88°52'02" WEST, A
212 DISTANCE OF 2,366.99 FEET TO A POINT;
213
214 THENCE NORTH 85°05'57" WEST, A DISTANCE OF 25.05 FEET TO A POINT ON THE
215 AFOREMENTIONED WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF
216 SECTION 24 AND THE WEST LINE OF SAID BLOCK 26, PALM BEACH FARMS CO.
217 PLAT NO. 3; SAID LINE ALSO BEING COINCIDENT WITH THE WEST LINE OF THOSE
218 LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198, OF THE
219 PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND SAID EAST LINE OF
220 TRACT B, OAKMONT ESTATES P.U.D.;
221
222 THENCE ALONG SAID WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF
223 SECTION 24, NORTH 01°24'43" EAST, A DISTANCE OF 1,538.90 FEET TO THE POINT
224 OF BEGINNING.

Exhibit B

225

226 SAID LANDS SITUATE, LYING AND BEING IN PALM BEACH COUNTY, FLORIDA,
227 CONTAINING 1,906,098 SQUARE FEET OR 43.758 ACRES, MORE OR LESS.

Exhibit C
RESOLUTION NO. R2026-05

A RESOLUTION OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A CONDITIONAL USE (PETITION 2025-0002-CU) FOR CERTAIN PROPERTY FORMERLY KNOWN AS K PARK, TOTALING APPROXIMATELY 71.27 ACRES, MORE OR LESS, LOCATED ON THE SOUTHWEST CORNER OF STRIBLING WAY AND US441/SR7, AS MORE SPECIFICALLY DESCRIBED HEREIN, TO ALLOW AN 180-ROOM HOTEL WITHIN POD B OF WELLINGTON VILLAGE MUPD; PROVIDING AN EFFECTIVE DATE.

WHEREAS, the Wellington's Council, as the governing body of Wellington, Florida, pursuant to the authority in Chapter 163 and Chapter 166, Florida Statutes, and the Wellington Land Development Regulations, is authorized and empowered to consider petitions related to zoning and development orders; and

WHEREAS, the notice of hearing requirements as provided in Article 5 of the Wellington Land Development Regulations have been satisfied; and

WHEREAS, the Future Land Use Map designation for the subject property was recently changed to Mixed-Use by Ordinance No. 2025-04; and

WHEREAS, the property owner has applied for a companion Rezoning (Ordinance No. 2026-06) and Master Plan Amendment applications (Resolution No. R2026-03) to change the Zoning designation to Multiple Use Planned Development (MUPD) and to adopt the Wellington Village MUPD Master Plan and Project Standards Manual; and

WHEREAS, the development program for Wellington Village MUPD includes an 180-room hotel within Pod B, which also requires a Conditional Use approval; and

WHEREAS, the Planning, Zoning and Adjustment Board, sitting as the Local Planning Agency, after notice and public hearing on January 21, 2026, recommended _____ of the Conditional Use application with a ____ - ____ vote; and

WHEREAS, the Wellington Council has taken the recommendations of the Local Planning Agency, the Findings of Facts provided in the staff report, and the evidence and testimony presented by the Petitioner and other interested parties, and comments from the public into consideration when considering the proposed Conditional Use; and

WHEREAS, Wellington's Council has made the following Findings of Fact:

1. Environmental Considerations: There are no environmental issues or concerns.

Exhibit C

2. Surrounding Use Considerations: The proposed hotel is centrally located on the 71.27-acre subject site and is compatible with the surrounding uses on-site and adjacent properties.
3. Public Facilities: The water, sanitary, sewer, and drainage systems meet the code requirements. Adequate public facilities are available to serve the subject site.
4. Concurrency: The project meets Wellington's Traffic Performance Standards (TPS).
5. Comprehensive Plan: The use requested is consistent with Wellington's Comprehensive Plan.
6. Land Development Regulations: The subject request is consistent with the stated purposes and intent of the Land Development Regulations.
7. Development Pattern: The request will result in a logical development pattern as it is located within a Mixed-Use development with other commercial and residential uses.

NOW, THEREFORE, BE IT RESOLVED BY THE WELLINGTON, FLORIDA'S COUNCIL, THAT:

SECTION 1. The Conditional Use to allow an 180-room hotel within Pod B of Wellington Village MUPD (FKA K Park), as described in Exhibit A, is hereby APPROVED subject to the following conditions of approval:

1. The hotel development regulations shall comply with the Wellington Village Project Standards Manual adopted as part of Resolution No. R2026-03. (PLANNING AND ZONING)
2. A Site Plan shall be required prior to the issuance of Land Development Permits. (PLANNING AND ZONING)
3. Architectural Review Board approval is required prior to the issuance of building permits for the elevations, colors, materials, signage, etc. (PLANNING AND ZONING AND ENGINEERING)
4. The building shall not exceed 35 feet in height unless approved by Council. (PLANNING AND ZONING)
5. Any accessory lounge within the hotel shall not exceed 25% of the gross floor area of the hotel, exclusive of parking. (PLANNING AND ZONING)

SECTION 2. This Resolution shall become effective immediately upon adoption.

PASSED AND ADOPTED this _____ day of _____ 2026.

ATTEST:

WELLINGTON, FLORIDA

BY: _____
Chevelle D. Hall, MMC, Village Clerk

BY: _____
Michael J. Napoleone, Mayor

**APPROVED AS TO FORM AND
LEGAL SUFFICIENCY:**

BY: _____
Laurie Cohen, Village Attorney

Exhibit A – Legal Description of Pod B

ALL THAT PIECE AND PARCEL OF LAND LOCATED IN THE NORTHEAST ONE-QUARTER (1/4) OF SECTION 24, TOWNSHIP 44 SOUTH, RANGE 41 EAST, PALM BEACH COUNTY, FLORIDA, BEING PORTIONS OF TRACTS 1 THROUGH 3 AND 6 THROUGH 8, IN BLOCK 26, OF PALM BEACH FARMS CO. PLAT NO. 3, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THAT PART OF SAID PALM BEACH FARMS CO. PLAT NO. 3. ROADWAY NORTH OF TRACTS 1 THROUGH 3, SAID BLOCK 26, AS MORE FULLY DESCRIBED IN OFFICIAL RECORD BOOK 13325 PAGE 1198, LESS THE SOUTH 86 FEET OF SAID ROADWAY AND TRACTS 4 & 5. SAID PARCEL BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTH ONE-QUARTER (1/4) CORNER OF SAID SECTION 24; THENCE SOUTH 01°24'43" WEST, AS A BASIS OF BEARINGS, ALONG THE WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF SAID SECTION 24, SAID LINE ALSO BEING THE WEST LINE OF BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THE EAST LINE OF TRACT B, OAKMONT ESTATES P.U.D. ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 106, PAGE 2, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, A DISTANCE OF 19.57 FEET TO A POINT ON THE NORTH LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA;

THENCE SOUTH 88°10'28" EAST, ALONG THE NORTH LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, A DISTANCE OF 1,041.05 FEET TO THE POINT OF BEGINNING;

THENCE CONTINUING ALONG SAID NORTH LINE FOR THE FOLLOWING FOUR (4) COURSES:

(1) SOUTH 88°10'28" EAST, A DISTANCE OF 674.96 FEET TO A POINT;
(2) THENCE SOUTH 86°54'18" EAST, A DISTANCE OF 315.00 FEET TO A POINT;
(3) THENCE SOUTH 88°10'40" EAST, A DISTANCE OF 330.42 FEET TO A POINT;
(4) THENCE SOUTH 43°08'16" EAST, A DISTANCE OF 57.12 FEET TO A POINT ON THE WESTERLY RIGHT OF WAY LINE OF U.S. HIGHWAY 441 (STATE ROAD No. 7), AS SHOWN ON THE FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP, SECTION 93210-2519, SHEET 9 OF 27, REVISION DATE 02-01-96;

THENCE ALONG SAID WESTERLY RIGHT-OF-WAY, SOUTH 01°53'57" WEST, A DISTANCE OF 949.58 FEET TO THE NORTHEAST CORNER OF TRACT A, LIFE COVENANT CHURCH WELLINGTON CAMPUS, ACCORDING TO THE MAP OR PLAT THEREOF, RECORDED IN PLAT BOOK 115, PAGE 44, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; THENCE ALONG THE NORTH LINE OF SAID TRACT A, NORTH 88°05'50" WEST, A DISTANCE OF 1,142.41 FEET TO THE NORTHWEST CORNER OF SAID TRACT A;

Exhibit C

157 THENCE DEPARTING THE NORTH LINE OF SAID TRACT A, NORTH 01°24'49" EAST,
158 A DISTANCE OF 265.78 FEET TO A POINT ON A NON-TANGENT CURVE; THENCE
159 ALONG A NON-TANGENT CURVE TO THE RIGHT, HAVING A RADIUS OF 511.00
160 FEET, THROUGH A CENTRAL ANGLE OF 42°48'55", FOR AN ARC LENGTH OF
161 381.85 FEET, BEING SUBTENDED BY A CHORD BEARING NORTH 00°51'34" EAST,
162 A DISTANCE OF 373.03 FEET TO A POINT OF REVERSE CURVATURE;

163
164 THENCE ALONG A REVERSE CURVE TO THE LEFT, HAVING A RADIUS OF 289.00
165 FEET, THROUGH A CENTRAL ANGLE OF 04°41'19", FOR AN ARC LENGTH OF 23.65
166 FEET, BEING SUBTENDED BY A CHORD BEARING NORTH 19°55'21" EAST, A
167 DISTANCE OF 23.64 FEET TO A POINT OF COMPOUND CURVATURE;

168
169 THENCE ALONG A COMPOUND CURVE TO THE LEFT, HAVING A RADIUS OF 50.00
170 FEET, THROUGH A CENTRAL ANGLE OF 84°54'46", FOR AN ARC LENGTH OF 74.10
171 FEET, BEING SUBTENDED BY A CHORD BEARING NORTH 24°52'41" WEST, A
172 DISTANCE OF 67.50 FEET TO A POINT OF REVERSE CURVATURE;

173
174 THENCE ALONG A REVERSE CURVE TO THE RIGHT, HAVING A RADIUS OF 181.00
175 FEET, THROUGH A CENTRAL ANGLE OF 54°33'27", FOR AN ARC LENGTH OF
176 172.35 FEET, BEING SUBTENDED BY A CHORD BEARING NORTH 40°03'21" WEST,
177 A DISTANCE OF 165.91 FEET TO A POINT OF NON-TANGENCY;

178
179 THENCE NORTH 88°34'08" WEST, A DISTANCE OF 34.35 FEET TO A POINT;

180
181 THENCE NORTH 01°49'32" EAST, A DISTANCE OF 69.79 FEET TO A POINT;

182
183 THENCE NORTH 88°10'28" WEST, A DISTANCE OF 40.74 FEET TO A POINT;

184
185 THENCE NORTH 01°49'32" EAST, A DISTANCE OF 80.81 FEET TO THE POINT OF
186 BEGINNING. SAID LANDS SITUATE, LYING AND BEING IN PALM BEACH COUNTY,
187 FLORIDA, CONTAINING 1,188,368 SQUARE FEET OR 27.281 ACRES, MORE OR
188 LESS.

Village of Wellington
Official Zoning Map



Legend

Zoning Districts

- ER - Equestrian Residential (Low Density)
- RS - Residential Single-family (Low Density)
- RM - Residential (Medium Density)
- RH - Residential (High Density)
- CC - Community Commercial
- CF - Community Facilities
- ECR - Equestrian Commercial Recreation
- PUD - Planned Unit Development
- MUPD - Multiple Use Planned Development
- MCPD - Medical Center Planned Development
- FLEX - Flex Use
- PBC/Other

Overlays

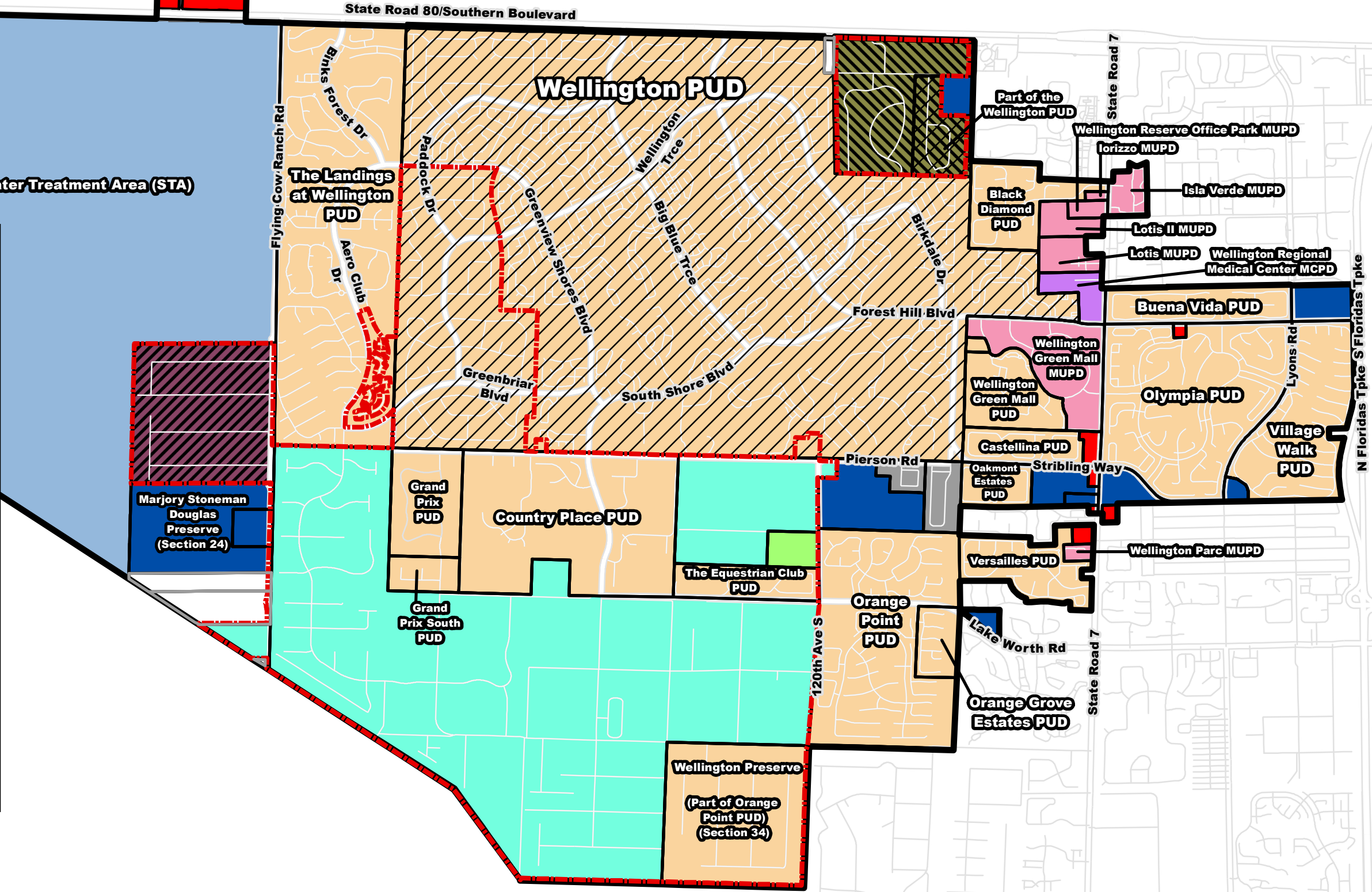
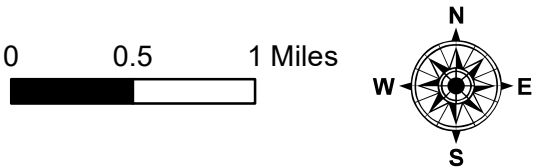
- EOZD - Equestrian Overlay Zoning District
- LROZD - Little Ranches Overlay Zoning District
- RROZD - Rustic Ranches Overlay Zoning District

Municipal Boundary

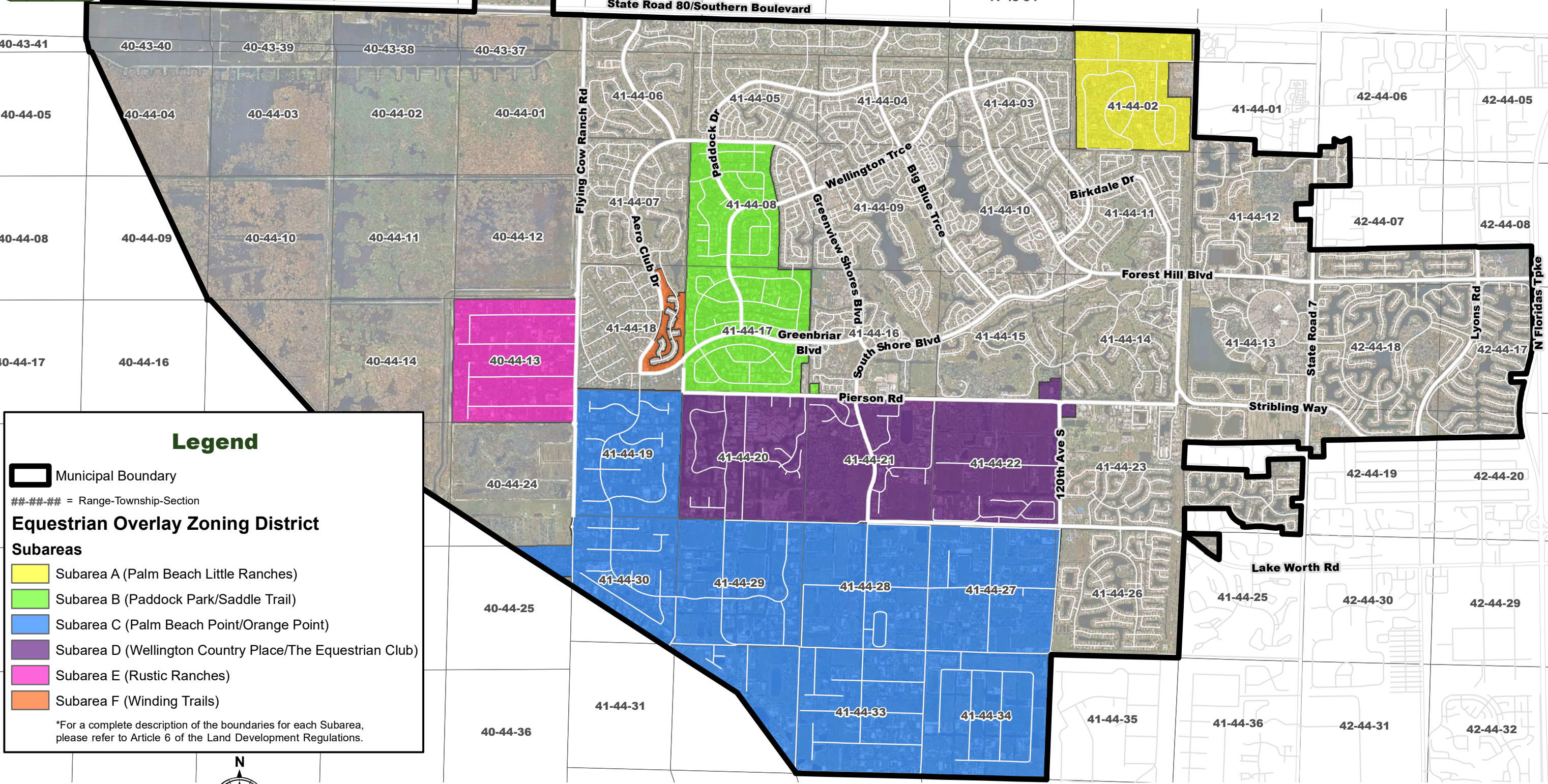
*Density ranges for properties in residential districts are determined by a property's Future Land Use Map designation.

Ordinance No. 2025-3

Adoption Date: 2/11/2025



Village of Wellington
Official Zoning Map



Legend

Municipal Boundary

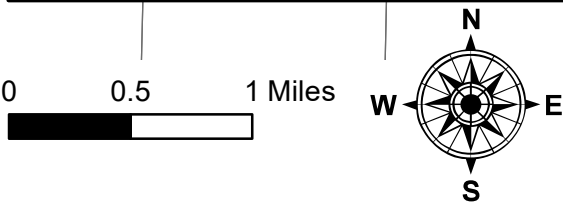
###-###-### = Range-Township-Section

Equestrian Overlay Zoning District

Subareas

- Subarea A (Palm Beach Little Ranches)
- Subarea B (Paddock Park/Saddle Trail)
- Subarea C (Palm Beach Point/Orange Point)
- Subarea D (Wellington Country Place/The Equestrian Club)
- Subarea E (Rustic Ranches)
- Subarea F (Winding Trails)

*For a complete description of the boundaries for each Subarea, please refer to Article 6 of the Land Development Regulations.



Village of Wellington Official Zoning Map



Legend

Zoning Districts

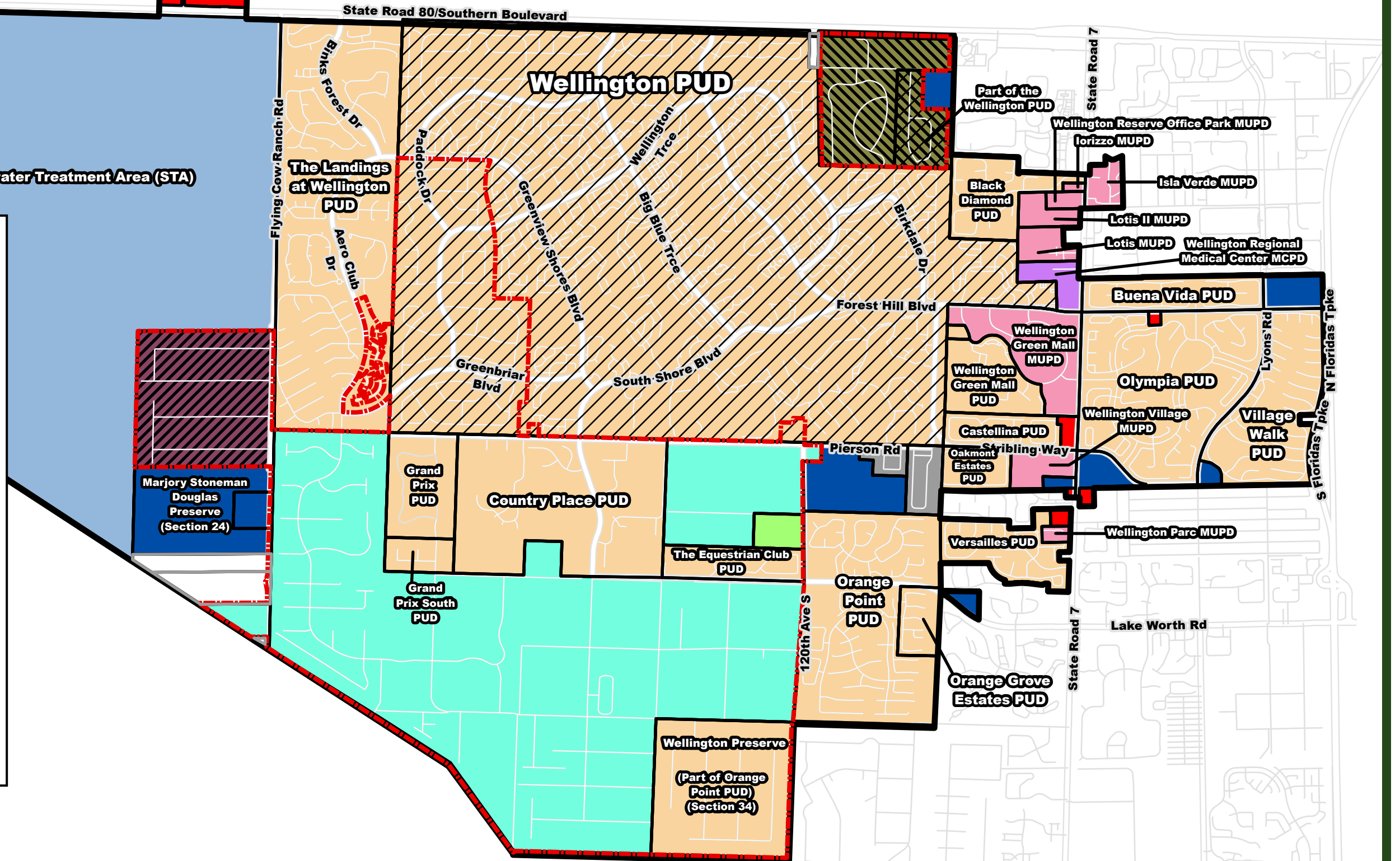
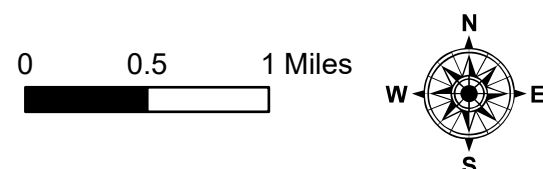
- ER - Equestrian Residential (Low Density)
- RS - Residential Single-family (Low Density)
- RM - Residential (Medium Density)
- RH - Residential (High Density)
- CC - Community Commercial
- CF - Community Facilities
- ECR - Equestrian Commercial Recreation
- PUD - Planned Unit Development
- MUPD - Multiple Use Planned Development
- MCPD - Medical Center Planned Development
- FLEX - Flex Use
- PBC/Other

Overlays

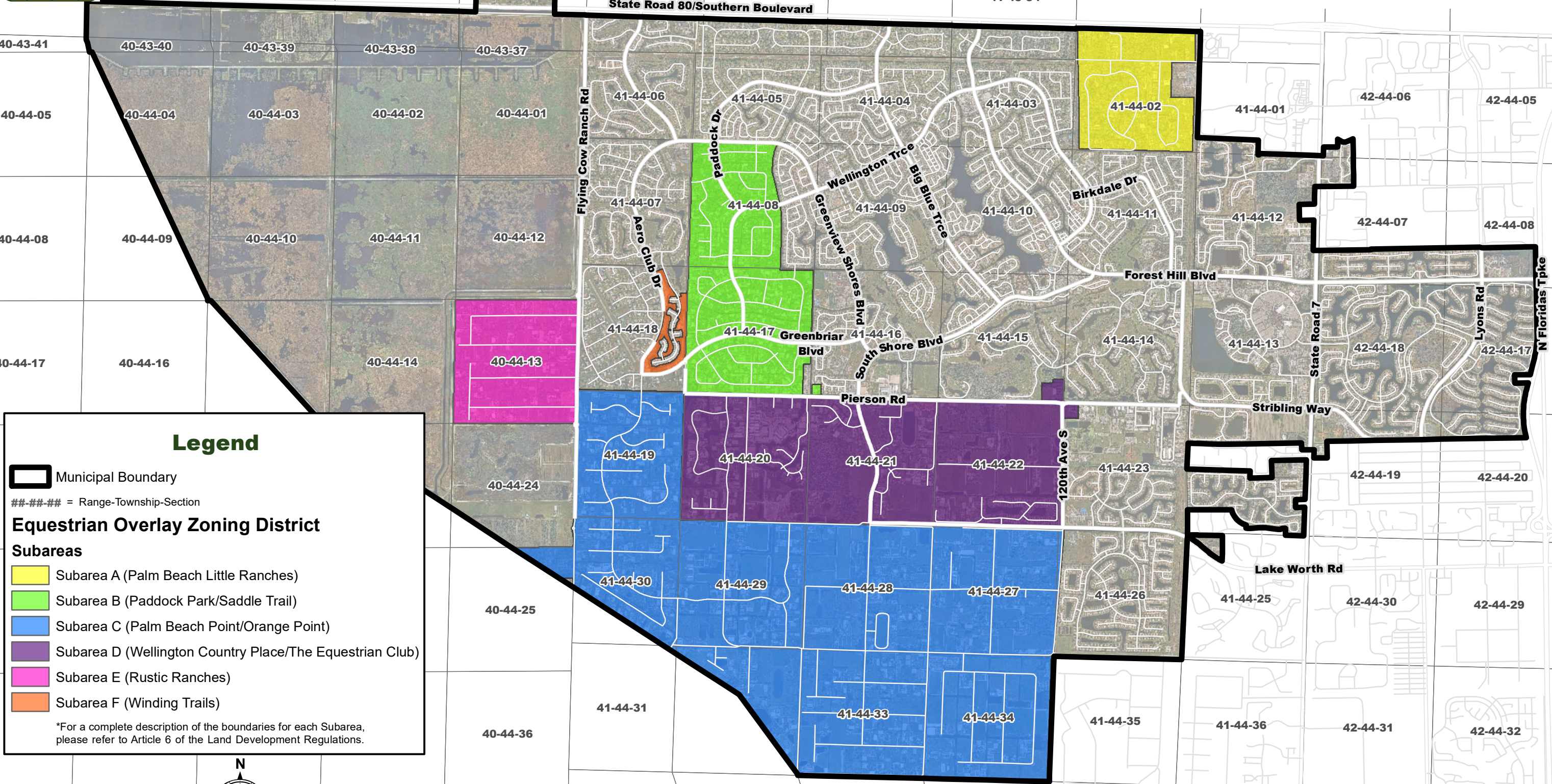
- EOZD - Equestrian Overlay Zoning District
- LROZD - Little Ranches Overlay Zoning District
- RROZD - Rustic Ranches Overlay Zoning District

Municipal Boundary

*Density ranges for properties in residential districts are determined by a property's Future Land Use Map designation.



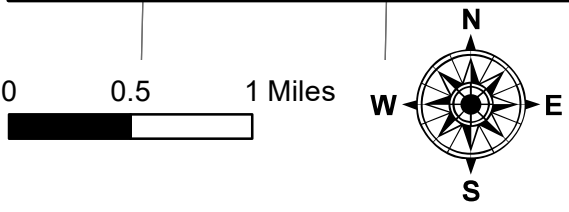
Village of Wellington
Official Zoning Map



Legend

- Municipal Boundary
- ###-###-### = Range-Township-Section
- Equestrian Overlay Zoning District**
- Subareas**
- Subarea A (Palm Beach Little Ranches)
- Subarea B (Paddock Park/Saddle Trail)
- Subarea C (Palm Beach Point/Orange Point)
- Subarea D (Wellington Country Place/The Equestrian Club)
- Subarea E (Rustic Ranches)
- Subarea F (Winding Trails)

*For a complete description of the boundaries for each Subarea, please refer to Article 6 of the Land Development Regulations.



H:\UDS\K-Park_24-080\Related Ross_001\Drawings\Master Plan\2025.12.16_MP_RdSubmittal.dwg, 12/16/2025 2:23:49 PM, MClunara, DWG To PDF-pc3, ARCH full bleed D (24.00 x 36.00 inches), 1:1

MUPD DATA

Exhibit F Wellington Village MUPD Master Plan

PETITION NO.: 2025-0001.MP

GROSS SITE AREA: 3,104,656 S.F. ± / 71.27 ACRES ±

FUTURE LAND USE: MIXED USE (MU)
EXISTING ZONING DISTRICT: C & CF
PROPOSED ZONING DISTRICT: MUPD
PCN(S): 73-42-43-27-05-026-0011
73-41-44-24-06-003-0000

EXISTING USE: AGRICULTURE & VACANT

PROPOSED USES: HOTEL (CONDITIONAL USE)
SCHOOL (CONDITIONAL USE)
MULTI-FAMILY RESIDENTIAL (PERMITTED)
OFFICE, PROFESSIONAL (PERMITTED)
COMMERCIAL MIXED-USE (PERMITTED)

GROSS LAKE AREA : 6.33 AC. (WATER SURFACE)

| PROPERTY DEVELOPMENT REGULATIONS | | | | | | | | | | | |
|----------------------------------|-----|---------------------|------------------|---------|-------------------|------------------------|-----------|-----------|-----------|-----------|-----------|
| | FLU | MIN. LOT DIMENSIONS | OVERALL | | MAX. BLDG. HEIGHT | MIN. BUILDING SETBACKS | | | | | |
| | | SIZE | WIDTH & FRONTAGE | DEPTH | FAR | BUILDING COVERAGE | FRONT | SIDE | CORNER | REAR | |
| REQUIRED | | 5 AC. | 300' | 300' | | MAX. | POD A 35' | POD A 30' | POD A 30' | POD A -- | POD A 30' |
| PROVIDED | | 71.27 AC. | 2,399' | 949.58' | .50 | 45% | POD B 35' | POD B 25' | POD B 25' | POD B 25' | POD B 30' |

** ALL BUILDINGS OVER 35' IN HEIGHT SUBJECT TO CONDITIONAL USE HEIGHT APPROVAL

| DEVELOPMENT SUMMARY | | | |
|----------------------|-----------------------------|---------------------------------------|------------|
| RESIDENTIAL | | UNITS | GFA |
| MULTI-FAMILY | PHASE 1 - 15 UNITS | 30,000 SF | |
| | PHASE 2 - 200 UNITS | 300,000 SF | |
| TOTAL | | 215 UNITS | 330,000 SF |
| NON-RESIDENTIAL | | INTENSITY / GFA | |
| HOTEL | 180 ROOMS / 175,000 SF | | |
| | OFFICE | 75,000 SF | |
| RETAIL | 210,000 SF | | |
| | RESTAURANT | 105,000 SF | |
| SCHOOL | 1,750 STUDENTS / 405,000 SF | | |
| | TOTAL | 970,000 SF | |
| GROSS FLOOR AREA: | | UP TO 1,300,000 SF | |
| PROPOSED FAR: | | 0.42 FAR | |
| RESIDENTIAL DENSITY: | | 3.02 DU / AC. (215 UNITS / 71.27 AC) | |
| NUMBER OF RESIDENTS: | | 604 RESIDENTS (2.81 RESIDENTS / UNIT) | |

RECREATION REQUIREMENT
PRIVATE AREA: 110 SF / PERSON @ 604 = 1.53 AC.
PUBLIC AREA: 5 AC / 1,000 PERSONS @ 604 = 3.02 AC.

CIVIC REQUIREMENT
AREA REQUIRED: 1 AC / 1,000 PERSONS @ 604= 0.60 AC.

RECREATION & CIVIL PROPOSED
REFER TO PROJECT STANDARDS MANUAL

| LAND USE ALLOCATION TABLE | | | |
|------------------------------|---------------|----------------------|--|
| LAND USE | | LAND USE ACREAGE (%) | PROPOSED USE MAX. INTENSITY ** / DENSITY |
| POD A | INSTITUTIONAL | 41.49 AC. * (58%) | SCHOOL: 1,750 STUDENTS / 405,000 SF |
| | COMMERCIAL | | RETAIL: 210,000 SF RESTAURANT: 105,000 SF (PLUS OUTDOOR DINING: 20,000 SF) |
| POD B | OFFICE | 22.22 AC. * (31%) | HOTEL: 180 ROOMS / 175,000 SF |
| | | | OFFICE: 75,000 SF |
| | RESIDENTIAL | | MULTI-FAMILY: 215 UNITS / 330,000 SF |
| POD O | OPEN SPACE | 3.23 AC. (5%) | POD B LAKE, PUBLIC USE LAKE TRAIL |
| MUNICIPAL POD | UTILITY | 0.19 AC. * | VILLAGE OF WELLINGTON LIFT STATION |
| BUFFER POD A | | 1.96 AC. (3%) | |
| BUFFER POD B | | 1.58 AC. (2%) | |
| BUFFER MUNICIPAL POD | | 0.03 AC. | |
| R/W DEDICATION POD A | | 0.31 AC. | |
| R/W DEDICATION POD B | | 0.25 AC. | |
| R/W DEDICATION MUNICIPAL POD | | 0.01 AC. | |
| TOTAL ACREAGE | | 71.27 AC. (100%) | |

* LAND USE ACREAGE EXCLUDES BUFFERS & R/W DEDICATION AREA
** INTENSITY IS PROVIDED AS GROSS SQUARE FEET (GSF)

MUPD LEGAL DESCRIPTION

PARCEL 1 (FEE SIMPLE)
PORTIONS OF TRACTS 1 THROUGH 8, IN BLOCK 26, OF PALM BEACH FARMS CO. PLAT NO. 3, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, AND THAT PART OF THE PALM BEACH FARMS CO. PLAT NO. 3, ROADWAY NORTH OF TRACTS 1 TO 4 AND WEST OF TRACTS 4 AND 5, SAID BLOCK 26, AS MORE FULLY DESCRIBED BELOW: TRACTS 1 THROUGH 8, BLOCK 26, OF PALM BEACH FARMS PLAT NO. 3, RECORDED IN PLAT BOOK 2, PAGE 45, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, LESS AND EXCEPT:
1. THE SOUTH 86 FEET OF SAID TRACTS 5 THROUGH 8 AND
2. THE RIGHT OF WAY FOR US 441. (STATE ROAD 7)
3. TEN ACRES, MORE OR LESS, DECEDED TO THE NEW COMMUNITY CHURCH OF THE PALM BEACHES, AS RECORDED IN OFFICIAL RECORDS BOOK 10931, PAGE 489 AND 492.
ADD 4 ACRES OF PBF CO. ROADWAY NORTH OF TRACTS 1 TO 4 AND WEST OF TRACTS 4 AND 5 ALL IN BLOCK 26, ORB 13325/1198, LESS THE SOUTH 86 FEET.
ALSO BEING DESCRIBED AS FOLLOWS: (THIS DESCRIPTION AUTHORIZED IN THE OFFICE OF DENNIS J. LEAVY & ASSOCIATES, INC.)
A PARCEL OF LAND LYING IN SECTION 24, TOWNSHIP 44 SOUTH, RANGE 41 EAST, BEING A PORTION OF TRACTS 1 THROUGH 8 INCLUSIVE, BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, AS RECORDED IN PLAT BOOK 2, PAGE 45, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:
COMMENCING AT THE NORTH ONE-QUARTER (1/4) CORNER OF SAID SECTION 24; THENCE SOUTH 01°24'43" WEST, AS A BASIS OF BEARINGS, ALONG THE WEST LINE OF THE NORTHEAST ONE-QUARTER OF SAID SECTION 24, SAID LINE ALSO BEING THE WEST LINE OF BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, A DISTANCE OF 19.57 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 88°10'28" EAST, A DISTANCE OF 178.01 FEET; THENCE SOUTH 88°54'00" EAST, A DISTANCE OF 315.08 FEET; THENCE SOUTH 88°10'28" EAST A DISTANCE OF 330.50 FEET; THENCE SOUTH 43°08'16" EAST A DISTANCE OF 57.12 FEET TO A POINT ON THE WESTERLY RIGHT-OF-WAY LINE OF STATE ROAD #7 AND U.S. HIGHWAY #441, AS SHOWN ON THE FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP, SECTION 93210-2519, SHEET 9 OF 27.
REVISION DATE 02/01/86, THE PRECEDING TWO (2) COURSES ALSO BEING CONCURRENT WITH THE NORTH LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 10931, PAGE 489 AND 492 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; THENCE SOUTH 88°52'02" WEST, ALONG SAID PARALLEL LINE, A DISTANCE OF 1270.71 FEET TO A POINT ON THE AFOREMENTIONED WEST LINE OF THE NORTHEAST ONE-QUARTER (1/4) OF SECTION 24 AND THE WEST LINE OF SAID BLOCK 26, PALM BEACH FARMS CO. PLAT NO. 3, SAID LINE ALSO BEING CONCURRENT WITH THE WEST LINE OF THOSE LANDS DESCRIBED IN OFFICIAL RECORD BOOK 13325, PAGE 1198 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; THENCE NORTH 01°24'43" EAST, ALONG SAID WEST LINE, A DISTANCE OF 1475.51 FEET TO THE POINT OF BEGINNING, SAID LANDS SITUATE, LYING AND BEING IN PALM BEACH COUNTY, FLORIDA.

PARCEL 2 (FEE SIMPLE):
TRACT C, OAKMONT ESTATES P.U.D., ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 106, PAGE 2, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA.

PARCEL 3 (EASEMENT):
TOGETHER WITH NON-EXCLUSIVE EASEMENTS) FOR THE BENEFIT OF PARCEL 1 ABOVE AS SET FORTH AND CREATED BY THAT CERTAIN ROADWAY AND MAINTENANCE EASEMENT BY AND BETWEEN NEW COMMUNITY CHURCH OF THE PALM BEACHES, INC. AND HERBERT F. KAHLERT, PERSONAL REPRESENTATIVE OF THE ESTATE OF HEINZ KAHLERT, AND KARL A. KAHLERT AND HERBERT F. KAHLERT, AS CO-TRUSTEES UNDER TRUST A OF THE WILL OF FRITZ M. KAHLERT RECORDED FEBRUARY 16, 1969 IN OFFICIAL RECORDS BOOK 10933, PAGE 1003, WHICH EASEMENT WAS ASSIGNED TO ACME IMPROVEMENT DISTRICT, A DEPENDENT SPECIAL DISTRICT EXISTING UNDER THE LAWS OF THE STATE OF FLORIDA BY ASSIGNMENT OF EASEMENTS RECORDED IN OFFICIAL RECORDS BOOK 16026, PAGE 462, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, OVER, UNDER AND ACROSS THE LANDS DESCRIBED THEREIN.

LOCATION MAP



DEVELOPMENT TEAM

| | |
|----------------------------|---|
| APPLICANT: | RELATED ROSS EDUCATION IN MOTION |
| SURVEYOR & CIVIL ENGINEER: | LANGAN 525 Okeechobee Blvd., Ste. 910 West Palm Beach, FL 33401 561.473.8350 |
| PLANNER : | URBAN DESIGN STUDIO 610 Clematis St., Ste. CU02 West Palm Beach, FL 33401 561.366.1100 |
| TRAFFIC ENGINEER: | SIMMONS & WHITE 2581 Metrocentre Blvd. West, Ste. 3 West Palm Beach, FL 33407 561.478.7848 |
| ARCHITECT: | ELKUS MANFREDI ARCHITECTS 25 Drydock Ave. Boston, Massachusetts 617.368.3395 |

MASTER PLAN LEGEND

| | |
|--------|-----------------------------|
| AC. | = ACRES |
| BLDG. | = BUILDING |
| E.O.W. | = EDGE OF WATER |
| GFA | = GROSS FLOOR AREA |
| PG. | = PAGE |
| O.R.B. | = OFFICIAL RECORD BOOK |
| L.B. | = LANDSCAPE BUFFER |
| L.M.E. | = LAKE MAINTENANCE EASEMENT |
| MIN. | = MINIMUM |
| S.B. | = SETBACK |
| S.F. | = SQUARE FEET |
| R | = RADIUS |
| RW | = RIGHT OF WAY |
| T.O.B. | = TOP OF BANK |
| T.B.A | = TO BE ABANDONED |
| T.R. | = TO REMAIN |
| TYP. | = TYPICAL |
| U.E. | = UTILITY EASEMENT |

NOTES:

- POD LINES ARE NOT LOT LINES AND DO NOT REPRESENT SUBDIVISION LINES.
- BASE INFORMATION FOR THIS PROJECT WAS OBTAINED FROM A BOUNDARY SURVEY PREPARED BY LANGAN DATED 2025.04.22

ZONING STAMPS:

urban design studio

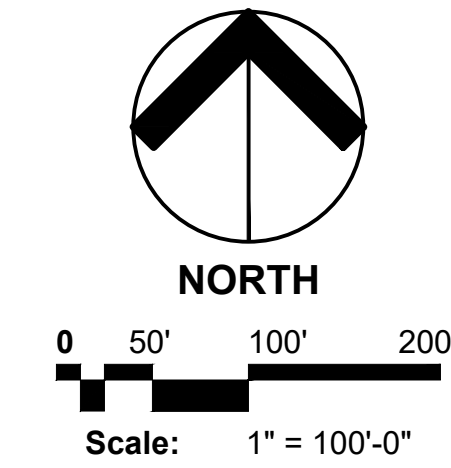
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K Park MUPD

Wellington, Florida
Master Plan



Date: May 16, 2025
Project No.: 24-080.001
Designed By:
Drawn By: MLC
Checked By:

Revision Dates:
2025.05.16: REZONING SUBMITTAL
2025.08.18: RESUBMITTAL #1
2025.09.29: RESUBMITTAL #2
2025.11.17: RESUBMITTAL #3
2025.12.16: RESUBMITTAL #4

MP-1

of 2

H:\05\K-Park_24-080\Related Res_001\Drawings\Master Plan\0251.12.16_MP_ResSubmittal.dwg, 12/16/2025 2:27:40 PM, MLCusera, DWG to PDF-pc-3, ARCH-full bleed D (A,0,0 x 36,00 inches), 1:1

MASTER SITE PLAN DATA

PETITION NO.: 2025-0001.MP
GROSS SITE AREA: 3,104,656 S.F. ± / 71.27 ACRES ±
FUTURE LAND USE: MIXED USE (MU)
EXISTING ZONING DISTRICT: C & CF
PROPOSED ZONING DISTRICT: MUPD
PCN(S): 73-42-43-27-05-026-0011
73-41-44-24-06-003-0000
EXISTING USE: AGRICULTURE & VACANT
PROPOSED USES: HOTEL (CONDITIONAL USE)
SCHOOL (CONDITIONAL USE)
MULTI-FAMILY RESIDENTIAL (PERMITTED)
OFFICE, PROFESSIONAL (PERMITTED)
COMMERCIAL MIXED-USE (PERMITTED)
GROSS LAKE AREA : 6.33 AC. (WATER SURFACE)

Exhibit G - Conceptual Site Plan

DEVELOPMENT SUMMARY

| RESIDENTIAL | UNITS | | GFA |
|-----------------|-----------------------------|------------------------|-----|
| | PHASE 1 - 15 UNITS | 30,000 SF | |
| MULTI-FAMILY | PHASE 2 - 200 UNITS | 300,000 SF | |
| TOTAL | 215 UNITS | 330,000 SF | |
| NON-RESIDENTIAL | INTENSITY / GFA | | |
| | HOTEL | 180 ROOMS / 175,000 SF | |
| OFFICE | RETAIL | 75,000 SF | |
| RESTAURANT | SCHOOL | 210,000 SF | |
| SCHOOL | | 105,000 SF | |
| TOTAL | 1,750 STUDENTS / 405,000 SF | 970,000 SF | |

GROSS FLOOR AREA: UP TO 1,300,000 SF
PROPOSED FAR: 0.42 FAR
RESIDENTIAL DENSITY: 3.02 DU / AC. (215 UNITS / 71.27 AC)
NUMBER OF RESIDENTS: 604 RESIDENTS (2.81 RESIDENTS / UNIT)

RECREATION REQUIREMENT
PRIVATE AREA: 110 SF / PERSON @ 604 = 1.53 AC.
PUBLIC AREA: 5 AC / 1,000 PERSONS @ 604 = 3.02 AC.

CIVIC REQUIREMENT
AREA REQUIRED: 1 AC / 1,000 PERSONS @ 604= 0.60 AC.

RECREATION & CIVIL PROPOSED
REFER TO PROJECT STANDARDS MANUAL

| PROPERTY DEVELOPMENT REGULATIONS | | | | | | | | | | | |
|----------------------------------|-----|---------------------|------------------|---------|---------|-------------------|----------------------|------------------------|-----------|-----------|------|
| | FLU | MIN. LOT DIMENSIONS | | | OVERALL | | MAX. BLDG. HEIGHT ** | MIN. BUILDING SETBACKS | | | |
| | | SIZE | WIDTH & FRONTAGE | DEPTH | FAR | BUILDING COVERAGE | | FRONT | SIDE | CORNER | REAR |
| REQUIRED | MU | 5 AC. | 300' | 300' | | MAX. | POD A 35' | POD A 30' | POD A 30' | POD A 30' | |
| PROVIDED | MU | 71.27 AC. | 2,399' | 949.58' | MAX. 50 | MAX. 45% | POD B 35' | POD B 25' | POD B 25' | POD B 30' | |

** ALL BUILDINGS OVER 35' IN HEIGHT SUBJECT TO CONDITIONAL USE HEIGHT APPROVAL

NOTES:

- THIS MASTER PRELIMINARY SITE PLAN IS INTENDED TO CONVEY THE INTENDED OVERALL DESIGN FOR THE PROPOSED MUPD. A DETAILED SITE PLAN FOR EACH INDIVIDUAL DEVELOPMENT POD(S) SHALL BE SUBMITTED TO THE VILLAGE FOR FORMAL REVIEW AND APPROVAL PRIOR TO PERMITTING.
- MASTER SITE PLAN PARKING SUMMARY'S ARE PRELIMINARY. DETAILED PARKING DATA SHALL BE INCLUDED WITH THE FORMAL SITE PLAN SUBMITTAL(S) FOR EACH INDIVIDUAL DEVELOPMENT PODS. REFER TO ENGINEERING REPORTS FOR PARKING STUDY IN SUPPORT OF PROPOSED PARKING DEVIATIONS.
- BASE INFORMATION FOR THIS PROJECT WAS OBTAINED FROM A BOUNDARY SURVEY PREPARED BY LANGAN DATED 2025.04.22

POD A PARKING SUMMARY

PARKING REQUIRED: PHASE 1 - 402 SPACES / PHASE 2 - 646 SPACES (REFER TO PARKING STUDY)

PARKING PROVIDED:
PHASE 1 - 577 SPACES (SURFACE PARKING)
PHASE 2 - 646 SPACES (127 SPS. SURFACE PARKING / 519 SPS. PARKING GARAGE)
ACCESSIBLE PARKING TO BE PROVIDED TO MEET APPLICABLE F.B.C. REQUIREMENTS

LOADING REQUIRED (FOR 405,000 GSF):
1 / 15,000 GSF, UP TO 100K SF = 7 SPACES & 1 / 50,000 GSF OVER 100K SF = 6 SPACES
TOTAL REQUIRED = 13 SPACES @ 15' X 55'

LOADING PROVIDED: 3 SPACES, 15' X 50' (SUBJECT TO WAIVER)

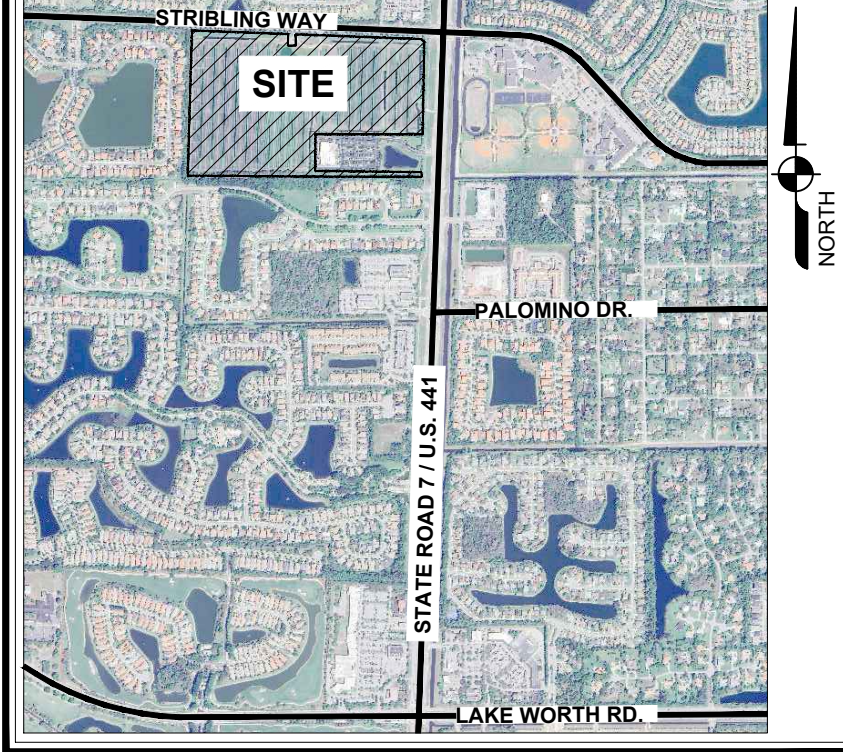
POD B PARKING SUMMARY

PARKING REQUIRED: 867 SPACES / 954 SPACES INC. 10% BUFFER (REFER TO SHARED PARKING STUDY)
PARKING PROVIDED: ON-SITE: 776 SPACES
OFF-SITE: 147 SPACES SHARED PKG (PER AGREEMENT WITH ADJ. CHURCH SITE)
VALET: 100 SPACES
TOTAL: 1,023 SPACES
ACCESSIBLE PARKING TO BE PROVIDED TO MEET APPLICABLE F.B.C. REQUIREMENTS

LOADING REQUIRED: 21 SPACES @ 15' x 55' (SUBJECT TO WAIVER)
HOTEL 9 SPACES (1/15,000 SF, UP TO 100K SF = 7 & 1/50,000 SF, OVER 100K SF = 2)
MIXED-USE 12 SPACES (1/15,000 SF, UP TO 100K SF = 7 & 1/50,000 SF, OVER 100K SF = 5)

LOADING PROVIDED: 13 SPACES (SUBJECT TO WAIVER)
HOTEL 2 SPACE, 14' X 50'
MIXED-USE 9 SPACES, 14' X 50' & 2 SPACES, 14' X 40'

LOCATION MAP

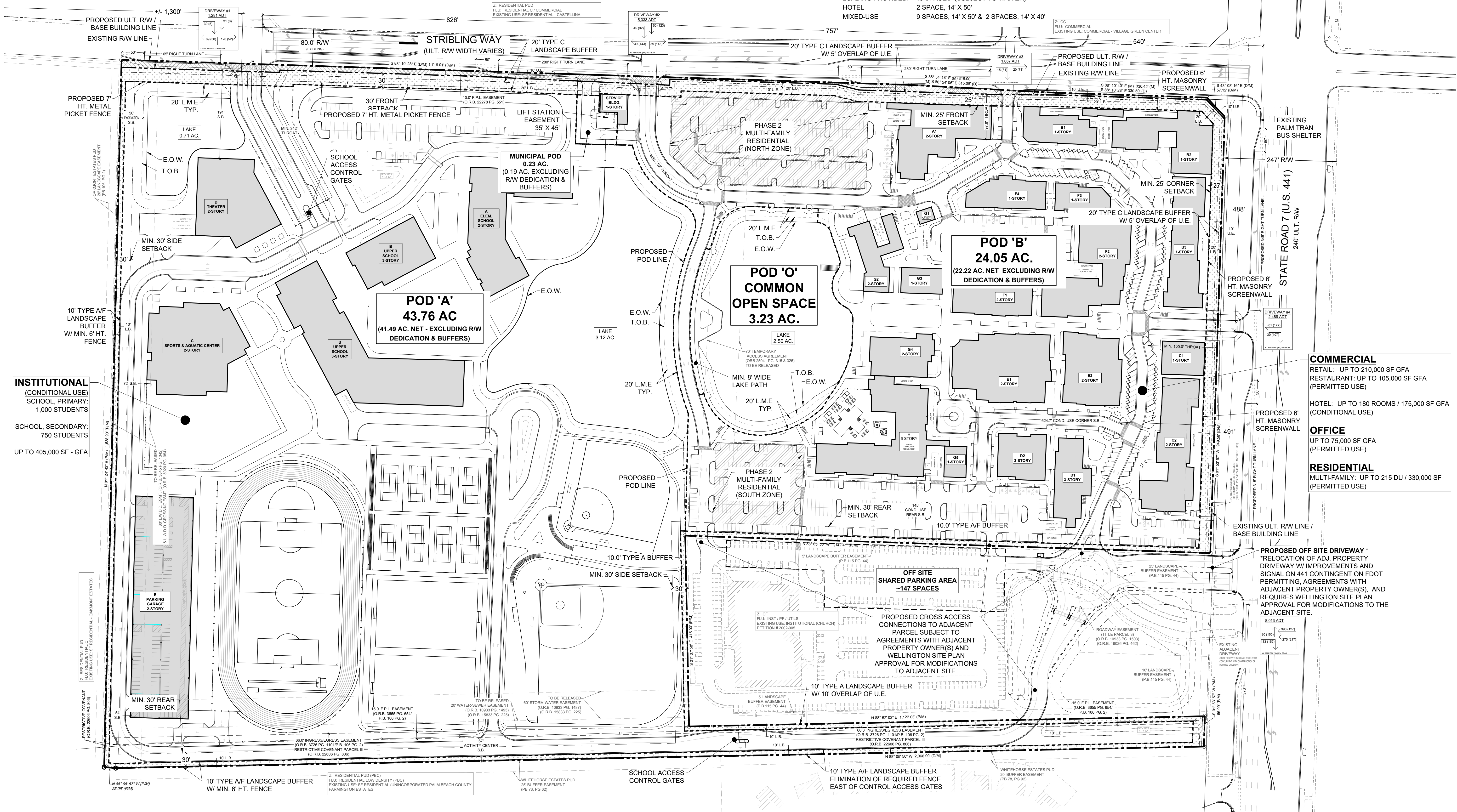


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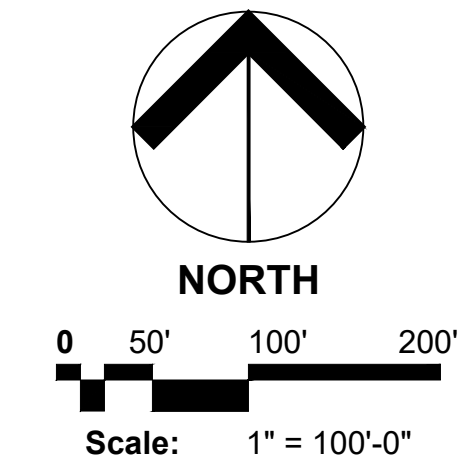
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K Park MUPD

Wellington, Florida
Master Preliminary Site Plan



Date: May 16, 2025
Project No.: 24-080.001
Designed By:
Drawn By: MLC
Checked By:

Revision Dates:
2025.05.16: REZONING SUBMITTAL
2025.08.18: RESUBMITTAL #1
2025.09.26: RESUBMITTAL #2
2025.11.17: RESUBMITTAL #3
2025.12.16: RESUBMITTAL #4

EXHIBIT

Exhibit H - Circulation Plan

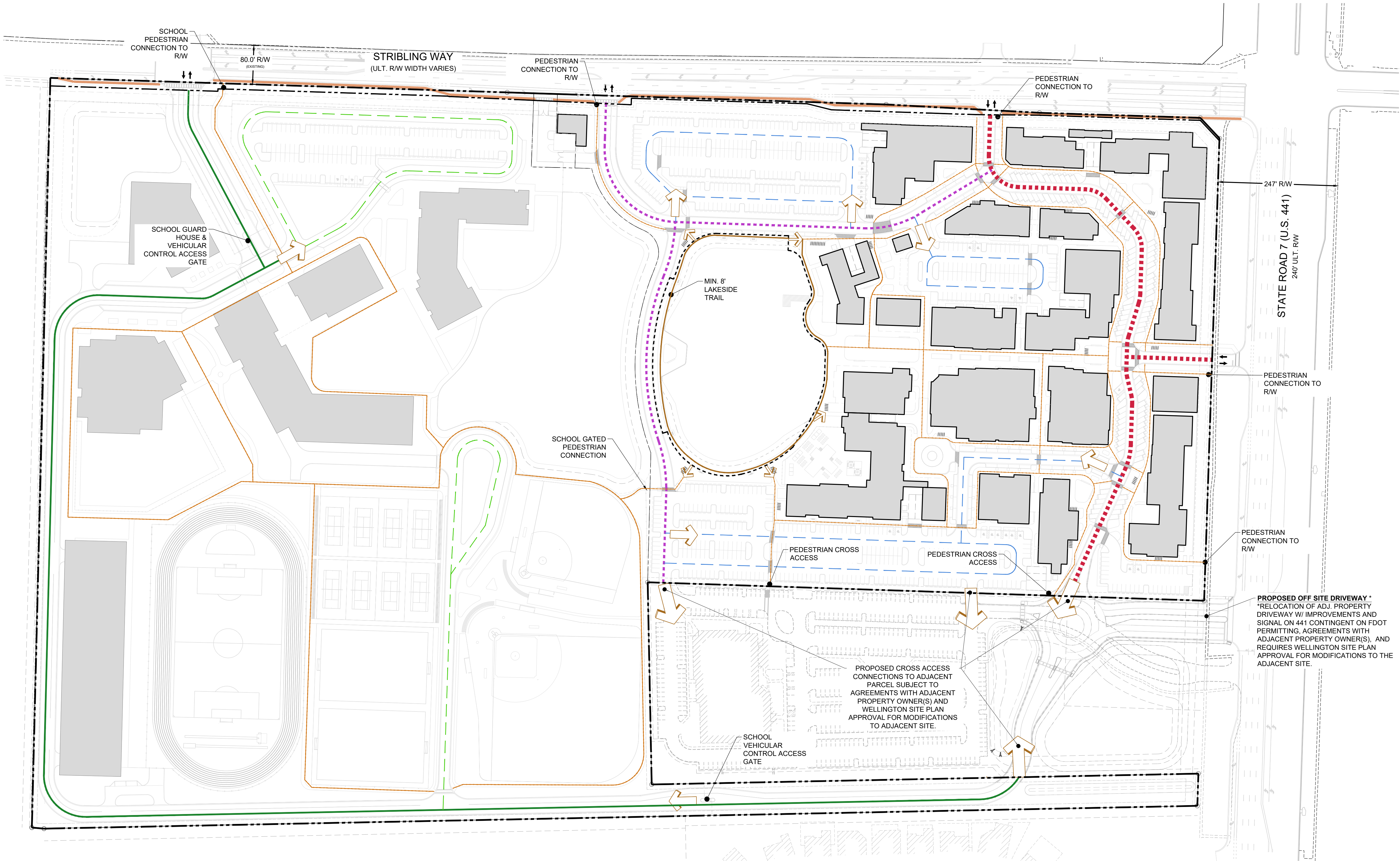
LEGEND

POD A SCHOOL CIRCULATION

- ACCESS POINT
- SCHOOL PRIMARY VEHICULAR CIRCULATION
- SCHOOL PARKING CIRCULATION
- SCHOOL PEDESTRIAN CIRCULATION
- MULTI-MODAL PERIMETER SIDEWALK

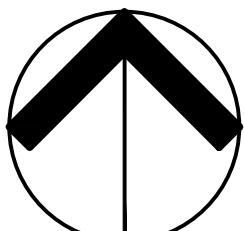
POD B MIXED USE VILLAGE CIRCULATION

- ACCESS POINT
- 'MAINSTREET' VEHICULAR CIRCULATION
- PRIMARY VEHICULAR CIRCULATION
- PARKING CIRCULATION
- PRIMARY PEDESTRIAN CIRCULATION
- MULTI-MODAL PERIMETER SIDEWALK

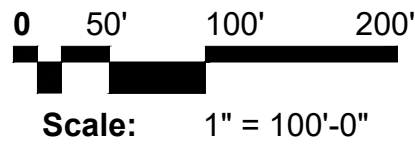


K Park MUPD

Wellington, Florida
Master Circulation Plan



NORTH



Scale: 1" = 100'-0"

Date: May 16, 2025
Project No.: 24-080.001
Designed By:
Drawn By: MLC
Checked By:

Revision Dates:
2025.05.16: REZONING SUBMITTAL
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2025.09.29: RESUBMITTAL #2
2025.11.17: RESUBMITTAL #3
2025.12.16: RESUBMITTAL #4

EXHIBIT



K-PARK MUPD

Village of Wellington, Florida

TRAFFIC IMPACT STATEMENT

PREPARED FOR:

Related Ross
360 South Rosemary Avenue
Suite 800
West Palm Beach, FL 33401

JOB NO. 25-024

DATE: 05/14/2025
Revised: 08/15/2025
Revised: 09/23/2025
Revised: 11/14/2025

Bryan G. Kelley, Professional Engineer, State of Florida, License No. 74006

This item has been digitally signed and sealed by Bryan G. Kelley, P.E., on 11/14/25.

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

Digitally signed by
Bryan Kelley
Date: 2025.11.18
10:54:44-05'00'

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- 2.0 PURPOSE OF STUDY

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- 3.0 TRIP GENERATION

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- 4.0 ROADWAY LINK ANALYSIS

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- 5.0 INTERSECTION ANALYSIS

PAGE 15

- 6.0 SITE RELATED IMPROVEMENTS

PAGE 16

- 7.0 CONCLUSION

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ROADWAY LINK ANALYSIS

APPENDIX B

INTERSECTION ANALYSIS

APPENDIX C

SIGNAL TIMING SHEETS

APPENDIX D

TPS DATABASE PRINTOUTS

APPENDIX E

PROPORTIONATE SHARE CALCULATIONS

APPENDIX F

CONCEPTUAL STATE ROAD 7 AT MAIN ENTRANCE ACCESS DESIGN

APPENDIX G

FDOT TRAFFIC DATA FOR GROWTH RATE CALCULATIONS

1.0 SITE DATA

The subject parcel is located in the southwest corner of Stribling Way at State Road 7 and consists of approximately +/- 71 acres in the Village of Wellington, Florida. The Property Control Numbers (PCN) for the subject parcel are 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000. The proposed mixed-use development plan is to consist of the following land uses and intensities:

- 215 Multi-Family Residential DU
- 180 Room Hotel
- 1,750 Student K-12 Private School
- 75,000 SF Office
- 335,000 SF Shopping Plaza (Including +/- 105,000 SF of Restaurant and up to 20,000 SF of outdoor seating area)

The project is estimated to have a build out of 2030 for purposes of the traffic study. Site access is proposed via five total driveway connections on Stribling Way and on State Road 7 and may be summarized as follows:

1. Full access driveway on Stribling Way approximately 2,300 feet west of State Road 7 (school traffic signal or roundabout proposed)
2. Full access driveway connection on Stribling Way approximately 1/4 mile west of State Road 7
3. Right in, right out driveway connection to Stribling Way approximately 500 feet west of State Road 7
4. Right in, right out driveway connection to State Road 7 approximately 500 feet south of Stribling Way
5. Full access signalized driveway connection to State Road 7 approximately 1,100 feet south of Stribling Way

For additional information on site layout, please refer to the Master Plan prepared by Urban Design Studio.

2.0 PURPOSE OF STUDY

This study will analyze the proposed development's impact on the surrounding major thoroughfares within the project's radius of development influence in accordance with the Village of Wellington Traffic Performance Standards.

3.0 TRIP GENERATION

Since access is proposed to be shared with the existing Life Church on State Road 7, trips associated with the existing church are included in the traffic study. The traffic generated by the existing church are calculated in accordance with the trip generation rates listed in the *ITE Trip Generation Manual, 11th Edition* and rates published by the Palm Beach County Engineering Traffic Division. Table 1 shows the daily traffic generation associated with the existing development in trips per day (tpd). Tables 2 and 3 show the AM and PM peak hour traffic generation, respectively, in peak hour trips (pht). The traffic generated by the existing development may be summarized as follows:

Existing Use

| | | |
|--|---|---------------------|
| Daily Traffic Generation | = | 227 tpd |
| AM Peak Hour Traffic Generation (In/Out) | = | 9 pht (6 In/3 Out) |
| PM Peak Hour Traffic Generation (In/Out) | = | 14 pht (7 In/7 Out) |

The traffic generated by the proposed site has been calculated in accordance with the trip generation rates listed in the *ITE Trip Generation Manual, 11th Edition* and rates published by the Palm Beach County Engineering Traffic Division. ITE Land Use Code #820 (Shopping Center >150k SF) incorporates restaurant use within the calculated trip generation rate. Therefore, the combined retail and restaurant uses were combined into 335,000 SF of shopping center. The K-12 private school will have multiple staggered periods that will disperse peak traffic over a 2-hour time period. For the AM peak hour trip generation, it is conservatively estimated that a maximum of 1,250 students would arrive during the highest peak hour. Therefore, the AM peak hour trip generation rate was based on 1,250 students. The internal capture rates were taken from the *ITE Trip Generation Handbook, 3rd Edition* and the NCHRP Report 684. Since no internal capture rates are provided in the ITE/NCHRP for schools, the school internalization was estimated based on engineering judgement.

Table 4 shows the daily traffic generation associated with the proposed development in trips per day (tpd). Tables 5 and 6 show the AM and PM peak hour traffic generation, respectively, in peak hour trips (pht). The traffic to be generated by the proposed development may be summarized as follows:

Proposed Use

| | | |
|--|---|----------------------------|
| Daily Traffic Generation | = | 15,291 tpd |
| AM Peak Hour Traffic Generation (In/Out) | = | 1,378 pht (866 In/512 Out) |
| PM Peak Hour Traffic Generation (In/Out) | = | 1,309 pht (596 In/713 Out) |

Table 7 attached to the report shows the net tips associated with the difference between the proposed and existing use and may be summarized as follows:

Net Trips (Proposed – Existing)

| | | |
|--|---|----------------------------|
| Daily Traffic Generation | = | 15,064 tpd |
| AM Peak Hour Traffic Generation (In/Out) | = | 1,369 pht (860 In/509 Out) |
| PM Peak Hour Traffic Generation (In/Out) | = | 1,295 pht (589 In/706 Out) |

4.0 ROADWAY LINK ANALYSIS

The distribution of project trips was based upon the existing and proposed geometry of the roadway network, a review of the existing and historical travel patterns, and a review of the proposed development and improvements in the area. The distributed traffic for the project at full build-out of the development was assigned to the links until the project traffic was insignificant within the Village of Wellington and a 4-mile radius for roadways outside of the Village.

Area Wide Growth Rate Calculations

The area wide historical growth rates are provided in Table 8 and were calculated based on AADT data from the FDOT from 2019 to 2024. Year 2019 was chosen instead of 2020 or 2021 to avoid traffic volumes influenced by Covid-19. The FDOT printouts for the area wide growth rate calculations are provided in Appendix "G". The area wide growth rate was calculated below 1.0%. Therefore, the overall background growth was determined based on a 1.0% nominal growth plus committed project trips. The committed project trips included approved but unbuilt projects within the TPS database. Some of the notable projects included were Wellington North, Wellington South, Wellington Marketplace, Wellington Sports Academy, Wellington Aquatic Center, Lotis, and many others.

Tables 9-10 (in Appendix A) show the project assignment as well as the applicable Level of Service Standard for each of the roadway links until the project assignment is no longer significant (within Wellington). Note the Village of Wellington Level of Service thresholds were used on all Wellington roadways. All roadway segments in which the project's traffic represented 1.0% or more of the LOS D volume threshold were further analyzed.

Due to the extensive scale of the roadway analysis, traffic counts were derived from several different methods including Palm Beach County roadway link volumes, FDOT segment counts, intersection turning movement counts, and new roadway tube counts. All traffic counts were collected between 2023 and 2025 and consistent with the Village of Wellington Traffic Performance Standards. The majority of traffic counts were collected during peak season conditions and any counts collected outside of the peak season were adjusted based on the FDOT peak season correction factor (PSCF).

As shown in Tables 11-12 (in Appendix A), all significantly impacted links meet the applicable Level of Service standards with the exception of the following roadway segments.

- South Shore Boulevard from Pierson Road to Lake Worth Road as a 2-lane section
- Stribling Way from Forest Hill Boulevard to Fairlane Farms as a 2-lane section
- Stribling Way from Fairlane Farms to Castellina Way as a 2-lane section
- Stribling Way from Castellina Way to State Road 7 as a 2-lane section
- Southern Boulevard from State Road 7 to Lyons Road as an 8-lane section
- Southern Boulevard from Lyons Road to Florida Turnpike as an 8-lane section
- Southern Boulevard from Florida Turnpike to Jog Road as an 8-lane section
- State Road 7 from Lake Worth Road to Lantana Road as a 6-lane section
- State Road 7 from Lantana Road to Hypoluxo Road as a 6-lane section
- Sansbury's Way from Belvedere Road to Southern Boulevard as a 2-lane section
- Lyons Road from Southern Boulevard to Forest Hill Boulevard as a 2-lane section
- Big Blue Trace from Wellington Trace to South Shore Boulevard as a 2-lane section

Many of the above roadways are background failures and do not meet LOS requirements based on existing and background traffic requirements and not a result of the proposed project. Per Florida Statutes Chapter 163, Section 3180, improvements needed to address existing/background failures are not the developer's responsibility. However, the property owner will be required to make proportionate share payments for the following new roadway deficiencies from the proposed project:

- Stribling Way from Forest Hill Boulevard to Fairlane Farms Road – PM Peak Hour
- Stribling Way from Fairlane Farms Road to Castellina Way – PM Peak Hour
- Stribling Way from Castellina Way to State Road 7 – PM Peak Hour
- Big Blue Trace from Wellington Tr to South Shore Blvd – PM Peak Hour
- Southern Boulevard from State Road 7 to Lyons Road – PM Peak Hour

The proportionate share calculations are provided in Appendix "E". With the proportionate share payments, the project meets the applicable requirements of the Wellington Traffic Performance Standards.

5.0 INTERSECTION ANALYSIS

Intersection operational analysis is required at each intersection nearest the project's access point and on any roadway link end in which the roadway has an 80% v/c ratio. Based on these criteria, the following intersections were analyzed.

1. State Road 7 at Stribling Way (signalized)
2. State Road 7 at Lake Worth Road (signalized)
3. Forest Hill Boulevard at State Road 7 (signalized)
4. Stribling Way at Fairlane Farms Road (roundabout)
5. Forest Hill Boulevard at Stribling Way (signalized)
6. South Shore Boulevard at Forest Hill Boulevard (signalized)
7. Wellington Trace S. at Forest Hill Boulevard (signalized)
8. Big Blue Trace at Wellington Trace (signalized)
9. Greenview Shores Boulevard at Wellington Trace (signalized)
10. Big Blue Trace at South Shore Boulevard (signalized)
11. South Shore Boulevard at Greenview Shores Boulevard (signalized)
12. Paddock Drive at Greenview Shores Boulevard (signalized)
13. South Shore Boulevard at Lake Worth Road (signalized)
14. Lake Worth Road at 120th Avenue (TWSC)
15. Stribling Way at Castellina Way (TWSC existing, Roundabout or School Signal proposed)
20. Pierson Road at 120th Avenue (roundabout)
25. Pierson Road at South Shore Boulevard (signalized)

The above intersections have been analyzed using Synchro software with HCM 7th edition results and the printouts are included in Appendix B. Existing signal timing sheets from Palm Beach County Traffic (included in Appendix C) were used in the analysis and signal timing splits were optimized as applicable. The intersections of State Road 7 at the main driveway entrance and Stribling Way at the mixed-use full access entrance were also analyzed in the total traffic conditions. Note the intersection of State Road 7 at Lake Worth Road is not within the Village of Wellington boundary and therefore was analyzed using the Critical Movement Analysis (CMA) methodology per Palm Beach County Traffic requirements. The intersection of Stribling Way at Castellina Way/school entrance is currently being considered for a roundabout or a school traffic signal. A roundabout is the preferred option but would require right-of-way from the Castellina HOA that has not been agreed to at this time. Therefore, both the roundabout and school traffic signal were analyzed in this report. The results of the analysis are summarized as follows:

5.0 INTERSECTION ANALYSIS (CONTINUED)

Table 13
Intersection Analysis – Existing Lane Geometry

| Intersection | | Peak Hour | Background Conditions | | Total Traffic Conditions | |
|--------------|--|-----------|-----------------------|------|--------------------------|------|
| | | | Average Delay (s/veh) | LOS | Average Delay (s/veh) | LOS |
| 1 | Stribling Way at State Road 7 | AM | 78.4 | E | 119.4 | F |
| | | PM | 84.8 | F | 87.8 | F |
| 2 | Lake Worth Road at State Road 7* | AM | 1,570 – CMA | Over | 1,582 – CMA | Over |
| | | PM | 1,541 – CMA | Over | 1,572 – CMA | Over |
| 3 | Forest Hill Boulevard at State Road 7 | AM | 87.7 | F | 93.3 | F |
| | | PM | 90.7 | F | 101.3 | F |
| 4 | Stribling Way at Fairlane Farms Road | AM | 13.3 | B | 23.1 | C |
| | | PM | 25.5 | D | 48.4 | E |
| 5 | Forest Hill Boulevard at Stribling Way | AM | 19.1 | B | 22.1 | C |
| | | PM | 39.0 | D | 45.7 | D |
| 6 | South Shore Boulevard at Forest Hill Boulevard | AM | 49.5 | D | 55.2 | E |
| | | PM | 64.9 | E | 78.5 | E |
| 7 | Wellington Trace S. at Forest Hill Boulevard | AM | 28.4 | C | 30.0 | C |
| | | PM | 26.8 | C | 28.6 | C |
| 8 | Big Blue Trace at Wellington Trace | AM | 33.5 | C | 35.1 | D |
| | | PM | 45.2 | D | 47.7 | D |
| 9 | Greenview Shores Boulevard at Wellington Trace | AM | 52.5 | D | 54.9 | D |
| | | PM | 39.5 | D | 43.0 | D |
| 10 | Big Blue Trace at South Shore Boulevard | AM | 41.2 | D | 48.9 | D |
| | | PM | 56.7 | E | 63.2 | E |
| 11 | South Shore Boulevard at Greenview Shores Boulevard | AM | 48.9 | D | 52.8 | D |
| | | PM | 37.0 | D | 37.9 | D |
| 12 | Paddock Drive at Greenview Shores Boulevard | AM | 8.3 | A | 8.4 | A |
| | | PM | 9.1 | A | 9.4 | A |
| 13 | South Shore Boulevard at Lake Worth Road | AM | 24.8 | C | 25.5 | C |
| | | PM | 49.3 | D | 51.4 | D |
| 14 | Lake Worth Road at 120 th Avenue (Southbound) | AM | 261.8 | F | 414.4 | F |
| | | PM | - | F | - | F |
| 15 | Stribling Way at Castellina Way (SB Approach for Background) | AM | 35.7 | E | n/a | n/a |
| | | PM | 96.3 | F | n/a | n/a |
| 16 | SR 7 at Main Driveway Entrance | AM | n/a | n/a | 7.0 | A |
| | | PM | n/a | n/a | 7.6 | A |
| 20 | Pierson Road at 120 th Avenue | AM | 5.3 | A | 5.6 | A |
| | | PM | 7.0 | A | 7.4 | A |
| 25 | South Shore Boulevard at Pierson Road | AM | 25.3 | C | 26.0 | C |
| | | PM | - | - | - | - |

*Intersection of Lake Worth Road at State Road 7 analyzed using the Critical Movement Analysis (CMA)

5.0 INTERSECTION ANALYSIS (CONTINUED)

As shown above, several intersections have background failures. Per Florida Statutes Chapter 163, Section 3180, improvements needed to address existing/background failures are not the developer's responsibility.

Table 14
Intersection Analysis – With Improvements

| | Intersection | Peak Hour | Background Conditions | | Total Traffic Conditions | |
|----|--|-----------|-----------------------|-----|--------------------------|-----|
| | | | Average Delay (s/veh) | LOS | Average Delay (s/veh) | LOS |
| 1 | Stribling Way at State Road 7 (2 nd NBL, 2 nd EBR, 2 nd EBL)* | AM | 51.7 | D | 54.5 | D |
| | | PM | 51.0 | D | 47.3 | D |
| 2 | Lake Worth Road at State Road 7 (3 rd NBL, 3 rd EBT, 3 rd WBL, 3 rd WBT)** | AM | 1,358 - CMA | D | 1,395 - CMA | D |
| | | PM | 1,291 - CMA | D | 1,322 - CMA | D |
| 3 | Forest Hill Boulevard at State Road 7 (Several Improvements)*** | AM | 47.1 | D | 54.3 | D |
| | | PM | 52.0 | D | 54.3 | D |
| 4 | Stribling Way at Fairlane Farms Road (2 NB Lanes: 1 NBL, 1 NBT) | AM | - | - | - | - |
| | | PM | 12.5 | C | 16.5 | C |
| 6 | South Shore Boulevard at Forest Hill Boulevard (Convert EBR turn lane into shared EBT/EBR lane)**** | AM | 39.3 | D | 43.3 | D |
| | | PM | 49.0 | D | 52.9 | D |
| 10 | Big Blue Trace at South Shore Boulevard (Add 2 nd SBL turn lane) | AM | - | - | - | - |
| | | PM | 29.1 | C | 30.9 | C |
| 14 | Lake Worth Road at 120 th Avenue (Signalized) | AM | 19.4 | A | 20.2 | C |
| | | PM | 32.9 | C | 39.7 | D |
| 15 | Stribling Way at Castellina Way (Traffic Signal) | AM | n/a | n/a | 26.1 | C |
| | | PM | n/a | n/a | 19.7 | B |
| 15 | Stribling Way at Castellina Way (Roundabout) | AM | n/a | n/a | 9.9 | A |
| | | PM | n/a | n/a | 13.6 | B |
| 16 | SR 7 at Main Driveway Entrance (Proposed Geometry) | AM | n/a | n/a | 7.0 | A |
| | | PM | n/a | n/a | 7.6 | A |

*Stribling Way at State Road 7: Background improvements consist of 2nd NBL, 2nd EBR and 2nd WBL. For the Total traffic conditions, the 2nd WBL was changed to a 2nd EBL.

**Intersection of Lake Worth Road at SR 7 analyzed using the Critical Movement Analysis

***Intersection of Forest Hill Boulevard at State Road 7 requires an additional intersection improvement in the total traffic conditions for the PM peak hour which will require proportionate share

****Improvement at the intersection of South Shore Boulevard at Forest Hill Boulevard requires a proportionate share during the AM peak hour but is a background failure for the PM peak hour.

5.0 INTERSECTION ANALYSIS (CONTINUED)

The improvements shown above for the background conditions are the minimum required to meet LOS D requirements with the exception of the following intersections:

1. Stribling Way at Fairlane Farms in the PM Peak Hour
2. Forest Hill Boulevard at State Road 7 in the PM Peak Hour
3. Forest Hill Boulevard at South Shore Boulevard in the AM Peak Hour

The above three intersections will require a proportionate share payment. With the background and mitigation improvements shown above, the proposed development meets LOS D requirements. See Appendix E for proportionate share calculations.

The intersection of Stribling Way at State Road 7 shows an improvement in overall delay and Level of Service for the total traffic conditions during the PM Peak Hour. This is due to the signal coordination and progression caused by adding the proposed traffic signal at the main driveway entrance. A separate analysis is also provided in Appendix B which includes both a second eastbound left turn lane and second westbound left turn lane (along with the required improvements) for the total traffic conditions on Stribling Way at State Road 7. This is provided for information purposes and not proposed as the second westbound left turn lane is not required to meet LOS D thresholds in the total traffic conditions.

The improvements required for Forest Hill Boulevard and State Road 7 match the improvements identified in the approved Wellington South traffic study for the AM peak hour. For the PM peak hour analysis, an additional northbound left turn lane was also required for the background conditions and an additional westbound left turn lane was required for the total traffic conditions. Due to high existing traffic volumes and long cycle length of this intersection, it was necessary to reduce the cycle length for traffic analysis purposes.

A back of queue analysis was also conducted for the intersections of State Road 7 at Stribling Way and several of the site driveways. The results of the analysis are provided below.

Table 15
Stribling Way at State Road 7 – 95th Percentile Queues

| Turn Lane | Peak Hour | Total Traffic with Improvements - 95 th Percentile Queue (ft) | Proposed Storage Length* (ft) |
|-------------------|-----------|--|-------------------------------|
| Eastbound Left | AM | 288 | 400 |
| | PM | 360 | |
| Eastbound Through | AM | 275 | N/A |
| | PM | 515 | |
| Eastbound Right | AM | 543 | 490 (double)/ 840 (single) |
| | PM | 305 | |
| Northbound Left | AM | 278 | 585 |
| | PM | 413 | |

*Proposed storage length to be finalized during final design and permitting with Village of Wellington, Palm Beach County, and FDOT. The storage lengths provided are based on preliminary design.

Table 16
Signalized Main Entrance at State Road 7 – 95th Percentile Queues

| Turn Lane | Peak Hour | 95 th Percentile Queue (ft) | Proposed Storage Length* (ft) |
|------------------|-----------|--|-------------------------------|
| Eastbound Left | AM | 130 | 240 |
| | PM | 190 | |
| Eastbound Right | AM | 213 | 240 |
| | PM | 213 | |
| Southbound Right | AM | 25 | 305 |
| | PM | 25 | |
| Northbound Left | AM | 295 | 325 |
| | PM | 205 | |

*Proposed storage length to be finalized during final design and permitting with Village of Wellington and FDOT. The storage lengths provided are based on preliminary design.

Table 17
Internal Roundabout West of State Road 7– 95th Percentile Queues

| Approach | Peak Hour | Total Traffic with Improvements - 95 th Percentile Queue (ft) | Proposed Storage Length (ft) |
|------------|-----------|--|------------------------------|
| Eastbound | AM | 25 | N/A |
| | PM | 25 | |
| Westbound | AM | 50 | 250 |
| | PM | 25 | |
| Northbound | AM | 25 | N/A |
| | PM | 25 | |
| Southbound | AM | 25 | N/A |
| | PM | 25 | |

Table 18
Stribling Way at Retail Entrance – 95th Percentile Queues

| Turn Lane | Peak Hour | 95 th Percentile Queue (ft) | Proposed Storage Length (ft) |
|------------------|-----------|--|------------------------------|
| Westbound Left | AM | 25 | 280 |
| | PM | 25 | |
| Northbound Left | AM | 25 | N/A |
| | PM | 195 | |
| Northbound Right | AM | 25 | 200 |
| | PM | 85 | |

Table 19
Stribling Way at Castellina Way/School Entrance – 95th Percentile Queues
Roundabout Alternative

| Approach | Peak Hour | Total Traffic with Improvements - 95 th Percentile Queue (ft) | Proposed Storage Length (ft) |
|------------|-----------|--|------------------------------|
| Eastbound | AM | 75 | N/A |
| | PM | 75 | |
| Westbound | AM | 100 | N/A |
| | PM | 300 | |
| Northbound | AM | 75 | N/A |
| | PM | 25 | |
| Southbound | AM | 25 | N/A |
| | PM | 25 | |

Table 20
Stribling Way at Castellina Way/School Entrance – 95th Percentile Queues
School Traffic Signal Alternative

| Turn Lane | Peak Hour | 95 th Percentile Queue (ft) | Proposed Storage Length (ft) |
|-------------------------|-----------|--|------------------------------|
| Eastbound Right | AM | 25 | 160 |
| | PM | 25 | |
| Westbound Left | AM | 33 | 280 |
| | PM | 25 | |
| Northbound Left/Through | AM | 118 | 300 |
| | PM | 40 | |
| Northbound Right | AM | 100 | 300 |
| | PM | 78 | |

6.0 SITE RELATED IMPROVEMENTS

The AM and PM peak hour turning movement volumes and directional distributions at the project entrance(s) for the overall development are shown in Tables 5 and 6 attached with this report and may be summarized as follows:

**DIRECTIONAL
DISTRIBUTION
(TRIPS IN/OUT)**

AM = 915 / 540
PM = 723 / 863

Figures 2-5 present the AM and PM peak turning movement volume assignments at the project driveways based on the directional distributions. Site access is proposed via a total of five driveway connections to Stribling Way and State Road 7. The proposed driveways may be summarized as follows:

1. Full access driveway on Stribling Way approximately 2,300 feet west of State Road 7 (school traffic signal or roundabout proposed)
2. Full access driveway connection on Stribling Way approximately 1/4 mile west of State Road 7
3. Right in, right out driveway connection to Stribling Way approximately 500 feet west of State Road 7
4. Right in, right out driveway connection to State Road 7 approximately 500 feet south of Stribling Way
5. Full access signalized driveway connection to State Road 7 approximately 1,100 feet south of Stribling Way

7.0 CONCLUSION

The proposed project will result in 15,064 net daily trips, 1,369 net AM peak hour trips, and 1,295 net PM peak hour trips. The traffic analysis demonstrates the proposed development meets the requirements of both the Palm Beach County Traffic Performance Standards and the Village of Wellington Traffic Performance Standards with the following conditions:

1. Proportionate Share Payment for the widening of Stribling Way from State Road 7 to Forest Hill Boulevard to four lanes.
2. Proportionate Share Payment for the widening of Big Blue Trace from Wellington Trace to South Shore Boulevard to four lanes.
3. Proportionate Share Payment for widening of Southern Boulevard from State Road 7 to Lyons Road to 8+ lanes.
4. Proportionate Share Payment for an additional northbound lane (1 left turn lane and one through lane) at the roundabout of Stribling Way at Fairlane Farms Road.
5. Proportionate Share Payment for an additional intersection improvement at Forest Hill Boulevard at State Road 7.
6. Proportionate Share Payment for an additional eastbound through lane at the intersection of Forest Hill Boulevard at South Shore Boulevard.
7. Proposed roundabout or school traffic signal at the school entrance/Castellina Way and Stribling Way.
8. Proposed traffic signal at State Road 7 main entrance including two northbound left turn lanes and two eastbound left turn lanes.
9. Turn lanes at project entrances documented above.

The applicant will further coordinate the above traffic mitigation requirements and timing with the Village.

K-PARK MUPD

EXISTING DEVELOPMENT

TABLE 1 - Daily Traffic Generation

| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|-------------------------------|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Church/Synagogue ^b | 560 | 31,396 | 7.6 | | 239 | 0 | 239 | 5% | 227 |
| | | S.F. | | | 239 | 0.0% | 239 | 5% | 227 |
| Grand Totals: | | | | | | | | | |

TABLE 2 - AM Peak Hour Traffic Generation

| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|-------------------------------|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Church/Synagogue ^b | 560 | 31,396 | 0.32 | 0.62 0.38 | 6 4 | 0.0% | 6 4 | 5% | 6 3 9 |
| | | S.F. | | | 6 4 | 0.0% | 6 4 | 10% | 6 3 9 |
| Grand Totals: | | | | | | | | | |

TABLE 3 - PM Peak Hour Traffic Generation

| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|-------------------------------|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Church/Synagogue ^b | 560 | 31,396 | 0.49 | 0.44 0.56 | 7 8 | 0.0% | 7 8 | 5% | 7 7 14 |
| | | S.F. | | | 7 8 | 0.0% | 7 8 | 7% | 7 7 14 |
| Grand Totals: | | | | | | | | | |

Notes:
 Church Square Footage taken from PBC Property Appraiser Website

K-PARK MUPD

PROPOSED DEVELOPMENT

TABLE 4 - Daily Traffic Generation

| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|--|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Multifamily Mid-Rise Housing 4-10 story (Apartment/Condo/TH) | 221 | 215 | 4.54 | | 976 | 13.9% | 841 | 0% | 841 |
| Hotel | 310 | 180 | 7.99 | | 1,438 | 13.3% | 1,247 | 10% | 1,122 |
| Private School (K-12) | 532 | 1,750 | 2.48 | | 4,340 | 8.5% | 3,971 | 0% | 3,971 |
| Church/Synagogue ^b | 560 | 31,396 | 7.6 | | 239 | 10.0% | 215 | 5% | 204 |
| General Office (10k-250k SF GFA) ^b | 710 | 75,000 | 10.84 | | 813 | 17.2% | 674 | 10% | 607 |
| Shop Center (>150ksf) | 820 | 335,000 | 37.01 | | 12,398 | 9.3% | 11,245 | 24% | 8,546 |
| Grand Totals: | | | | | 20,204 | 10.0% | 18,193 | 16% | 15,291 |

TABLE 5 - AM Peak Hour Traffic Generation

| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|--|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Multifamily Mid-Rise Housing 4-10 story (Apartment/Condo/TH) | 221 | 215 | 0.37 | 0.23 0.77 | 18 62 | 7.5% | 17 57 | 0% | 17 57 |
| Hotel | 310 | 180 | 0.46 | 0.56 0.44 | 46 37 | 9.6% | 46 29 | 10% | 41 26 |
| Private School (K-12) | 532 | 1,250 | 0.79 | 0.63 0.37 | 622 366 | 9.2% | 613 343 | 0% | 613 343 |
| Church/Synagogue ^b | 560 | 31,396 | 0.32 | 0.62 0.38 | 6 4 | 10.0% | 5 4 | 5% | 0 5 |
| General Office (10k-250k SF GFA) ^b | 710 | 75,000 | 1.52 | 0.88 0.12 | 100 14 | 15.8% | 87 9 | 10% | 78 8 |
| Shop Center (>150ksf) | 820 | 335,000 | 0.84 | 0.62 0.38 | 174 107 | 12.8% | 147 98 | 24% | 112 74 |
| Grand Totals: | | | | | 966 590 | 6.5% | 915 540 | 5% | 866 512 |

TABLE 6 - PM Peak Hour Traffic Generation

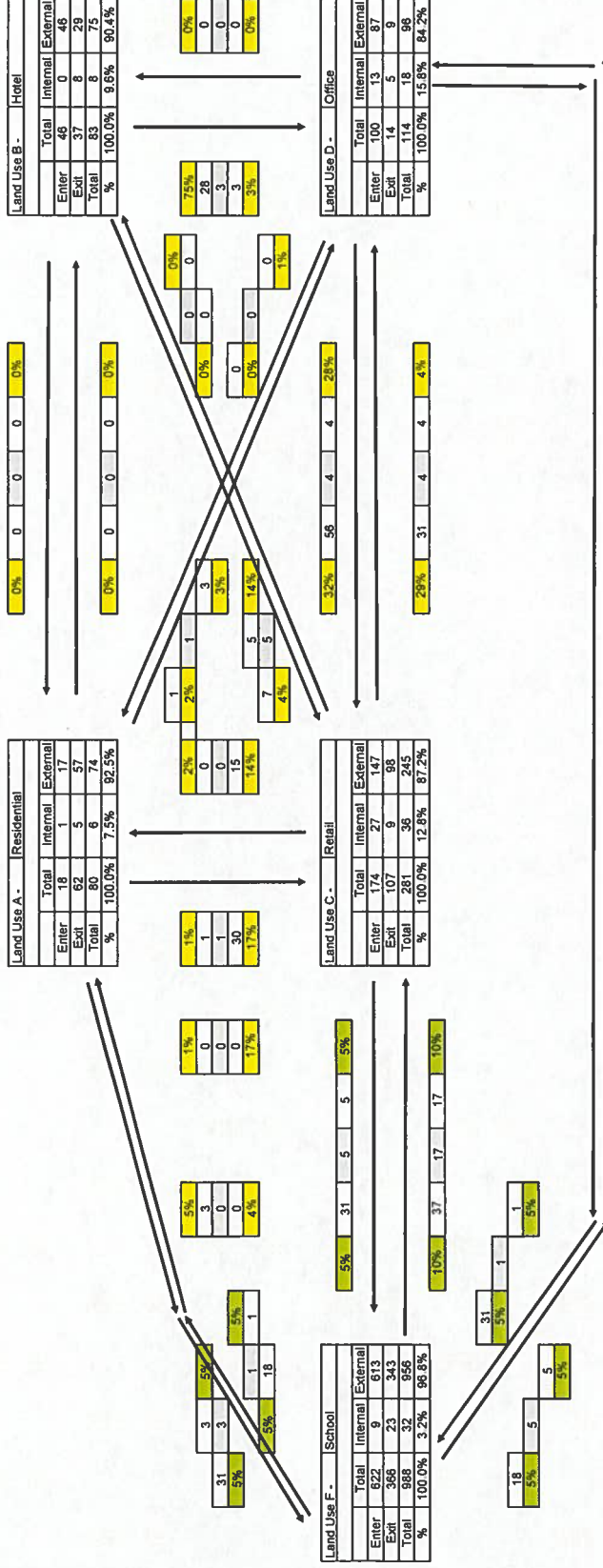
| Landuse | ITE Code | Intensity | Rate/Equation | Dir Split In Out | Gross Trips | Internalization % Total | External Trips | Pass-by % Trips | Net Trips |
|--|----------|-----------|---------------|---------------------|-------------|----------------------------|----------------|--------------------|-----------|
| Multifamily Mid-Rise Housing 4-10 story (Apartment/Condo/TH) | 221 | 215 | 0.39 | 0.61 0.39 | 51 33 | 20.2% | 41 26 | 0% | 41 26 |
| Hotel | 310 | 180 | 0.59 | 0.51 0.49 | 54 52 | 17.0% | 44 44 | 10% | 40 39 |
| Private School (K-12) | 532 | 1,750 | 0.17 | 0.43 0.57 | 128 170 | 13.8% | 108 149 | 0% | 108 149 |
| Church/Synagogue ^b | 560 | 31,396 | 0.49 | 0.44 0.56 | 7 8 | 10.0% | 6 7 | 5% | 1 6 |
| General Office (10k-250k SF GFA) ^b | 710 | 75,000 | 1.44 | 0.17 0.83 | 18 90 | 18.5% | 14 74 | 10% | 13 66 |
| Shop Center (>150ksf) | 820 | 335,000 | 3.4 | 0.48 0.52 | 547 592 | 5.8% | 510 563 | 24% | 388 427 |
| Grand Totals: | | | | | 805 945 | 9.4% | 723 863 | 17% | 596 713 |

Notes:

Due to multiple staggered start times, it is estimated that a maximum of 1,250 students would arrive within the one-hour peak. Therefore, the AM peak hour trip generation was adjusted to 1,250 students. Shopping Center rates include restaurant use per ITE. Approximately 105,000 SF of the 335,000 SF is anticipated to be restaurant. Additionally up to 20,000 SF of outdoor seating area is included in the total.

K-PARK MUPD

AM PEAK





04/30/2025
 Revised: 08/15/2025
 Revised: 09/23/2025
 Revised: 11/14/2025

K-PARK MUPD

TABLE 7

TRAFFIC GENERATION DIFFERENCE - NET TRIPS

| | DAILY | AM PEAK HOUR | | | PM PEAK HOUR | | |
|---------------------------------|--------|--------------|-----|-----|--------------|-----|-----|
| | | TOTAL | IN | OUT | TOTAL | IN | OUT |
| PREVIOUSLY VESTED DEVELOPMENT = | 227 | 9 | 6 | 3 | 14 | 7 | 7 |
| PROPOSED DEVELOPMENT = | 15,291 | 1,378 | 866 | 512 | 1,309 | 596 | 713 |
| DIFFERENCE = | 15,064 | 1,369 | 860 | 509 | 1,295 | 589 | 706 |

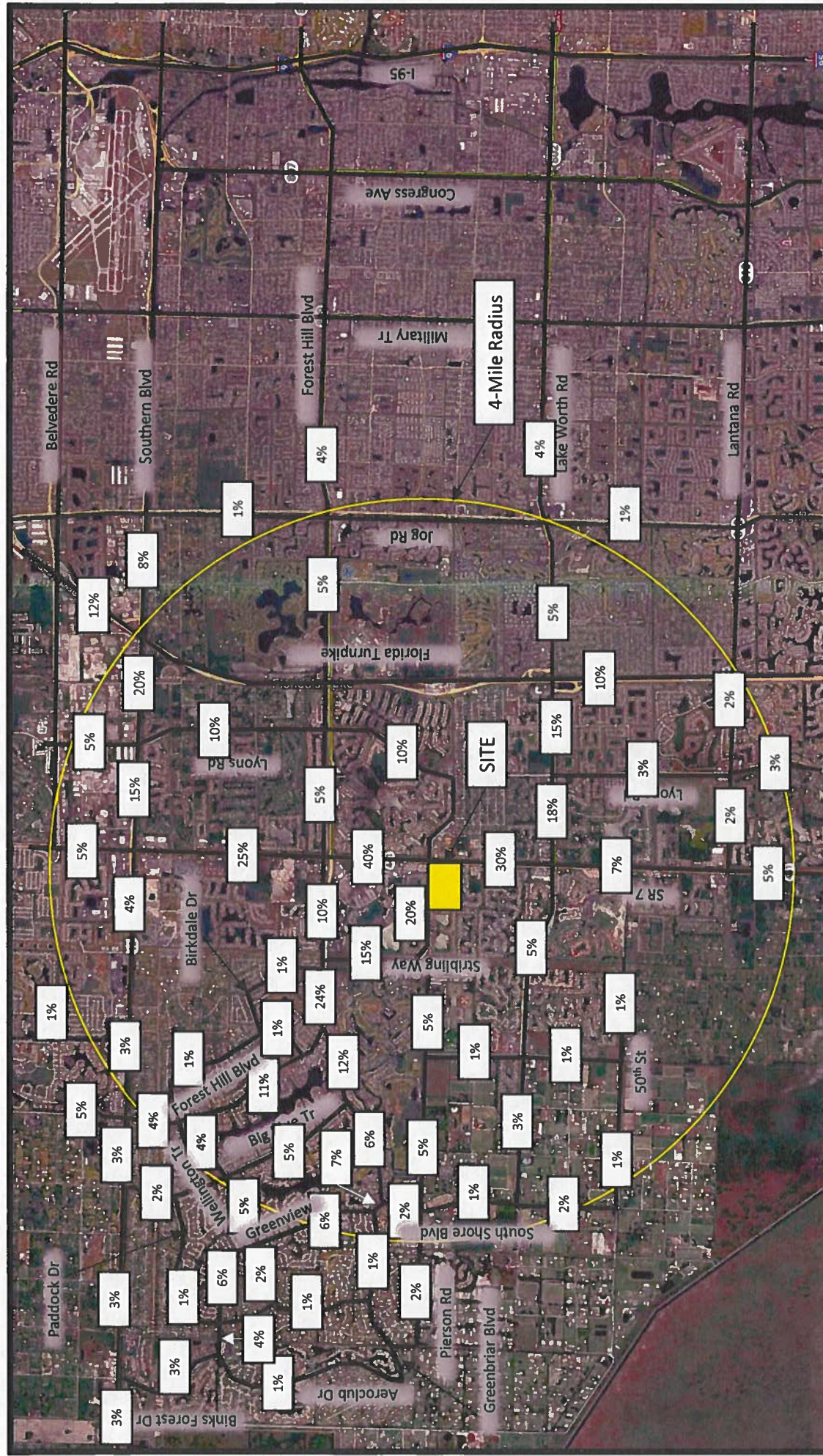


figure 1 - Trip Distribution

-Park

project # 25-024



**SIMMONS
& WHITE**
CIVIL & TRAFFIC ENGINEER 145

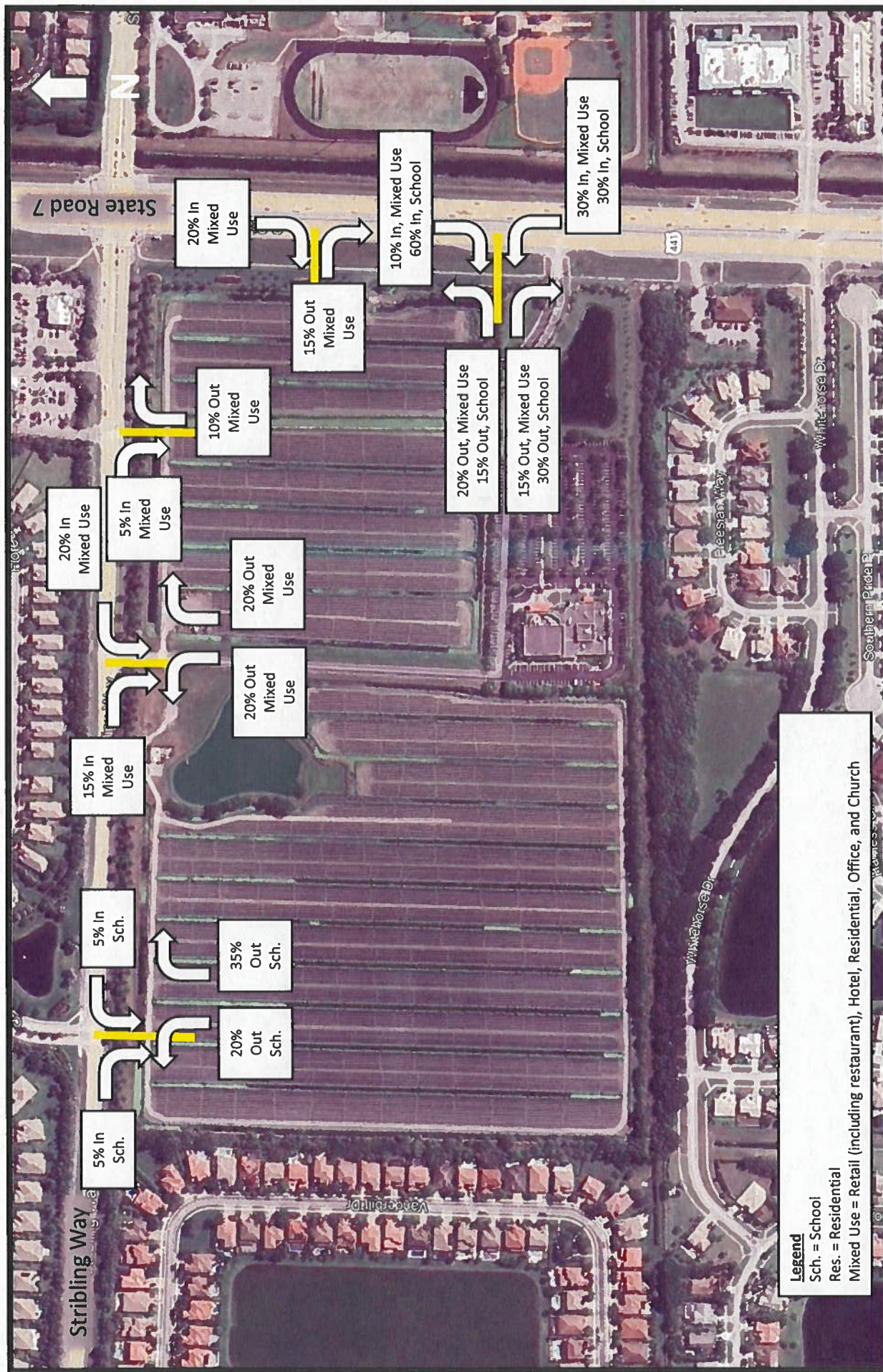


Figure 2 - Driveway Volumes

K-Park

Project # 25-024

December 4, 2025

Wellington Planning and Zoning
12300 Forest Hill Boulevard
Wellington, Florida 33414

Attention: Ms. Kelly Ferraiola, AICP

Reference: K-Park Traffic Analysis Supplemental Analysis
Village of Wellington, Florida

Dear Ms. Ferraiola:

This letter serves as a supplemental traffic analysis to the K-Park MUPD Traffic Impact Statement dated November 14, 2025.

An additional roadway segment analysis has been completed for the AM Peak Hour for the following roadway segments:

1. Wellington Trace between Forest Hill Boulevard N. and Birkdale Drive
2. Wellington Trace between Birkdale Drive and Forest Hill Boulevard S.
3. Aero Club Rd from Binks Forest Drive to Greenbriar Boulevard
4. Birkdale Road from Forest Hill Boulevard and Wellington Trace

The above roadways are owned and maintained by the Village of Wellington. The K-Park development project trips for the above intersections may be summarized below:

Table 1
AM Peak Hour Project Significance

| Roadway | Dir. | In/Out | K-Park Trips | LOS D Capacity | Project % Impact | Sign. (Yes/No) |
|---|------|--------|--------------|----------------|------------------|----------------|
| Wellington Trace between Forest Hill Blvd N. and Birkdale Drive | EB | Out | 5 | 880 | 0.6% | No |
| | WB | In | 9 | 880 | 1.0% | Yes |
| Wellington Trace between Birkdale Drive and Forest Hill Blvd S. | EB | Out | 5 | 880 | 0.6% | No |
| | WB | In | 9 | 880 | 1.0% | Yes |
| Aero Club Rd from Binks Forest Dr to Greenbriar Blvd | EB | In | 9 | 880 | 1.0% | Yes |
| | WB | Out | 5 | 880 | 0.6% | No |
| Birkdale Rd from Forest Hill Blvd to Wellington Tr | NB | Out | 5 | 880 | 0.6% | No |
| | SB | In | 9 | 880 | 1.0% | Yes |

As shown above in Table 1, the proposed development results in a 1% impact to the each of the roadway segments for the AM inbound direction. The existing volumes for the significantly impacted roadway segments are shown below in Table 2.

Table 2
AM Peak Hour Existing Volumes

| Roadway | Dir. | Existing Volume | LOS D Capacity | v/c ratio | Meets LOS? (Yes/No) |
|---|------|-----------------|----------------|-----------|---------------------|
| Wellington Trace between Forest Hill Blvd N. and Birkdale Drive | WB | 446 | 880 | 0.51 | Yes |
| Wellington Trace between Birkdale Drive and Forest Hill Blvd S. | WB | 331 | 880 | 0.38 | Yes |
| Aero Club Rd from Binks Forest Dr to Greenbriar Blvd | EB | 220 | 880 | 0.25 | Yes |
| Birkdale Rd from Forest Hill Blvd to Wellington Tr | SB | 148 | 880 | 0.17 | Yes |

As shown above, the existing AM peak hour traffic on the subject roadways are substantially below the LOS D volume thresholds. Therefore, the roadways will continue to meet LOS D thresholds with the inclusion of the proposed development project and with background traffic. It should be noted the majority of the counts above were derived from intersection counts and are likely conservative. For example, Wellington Trace just east of Forest Hil Boulevard N. is likely overstated due to the old Wellington Mall being located at the corner. Historical traffic counts collected by the Village of Wellington midsegment show significantly lower counts on Wellington Trace. The count data utilized in Table 2 is attached to this letter for reference. Additionally, the 2022 Village of Wellington traffic counts are also provided for informational purposes. However, more recent data was utilized for the analysis.

In addition to the roadway segment analysis, the intersection of State Road 7 at Stribling Way has been analyzed. The results of the overall intersection analysis and the projected 95th percentile queues are provided below.

Table 3
Intersection Analysis – With Improvements

| Intersection | | Peak Hour | Total Traffic Conditions | |
|--------------|---|-----------|--------------------------|-----|
| | | | Average Delay (s/veh) | LOS |
| 1 | Stribling Way at State Road 7 (2 nd NBL, 2 nd EBR, 2 nd EBL) | AM | 54.7 | D |
| | | PM | 47.4 | D |

Table 4
Stribling Way at State Road 7 – 95th Percentile Queues

| Turn Lane | Peak Hour | Total Traffic with Improvements – 95 th Percentile Queue (ft) | Proposed Storage Length* (ft) |
|-------------------|-----------|--|-------------------------------|
| Eastbound Left | AM | 270 | 400 |
| | PM | 360 | |
| Eastbound Through | AM | 283 | N/A |
| | PM | 515 | |
| Eastbound Right | AM | 620 | 490 (double)/ 840 (single) |
| | PM | 305 | |
| Northbound Left | AM | 300 | 585 |
| | PM | 413 | |

*Proposed storage length to be finalized during final design and permitting with Village of Wellington, Palm Beach County, and FDOT. The storage lengths provided are based on preliminary design.

Based on this supplemental traffic analysis, the results and conclusions documented in the K-Park MUPD Traffic Impact Statement dated November 14, 2025, remain the same. Please let us know if you have any questions or require additional information.

Sincerely,

SIMMONS & WHITE, INC.



Bryan G. Kelley, P.E.
Vice President

ENCLOSURES

BGK x:/docs/miscltr/Kelley/25024.kelley

CMA INTERSECTION ANALYSIS
K-PARK MUPD
17 - WELLINGTON TRACE AT FOREST HILL BOULEVARD

INPUT DATA

Comments: **USED FOR FOREST HILL COUNTS N. OF WELLINGTON TRACE**

Growth Rate = 1.00% Peak Season = 1.00 Current Year = 2024 Buildout Year = 2030

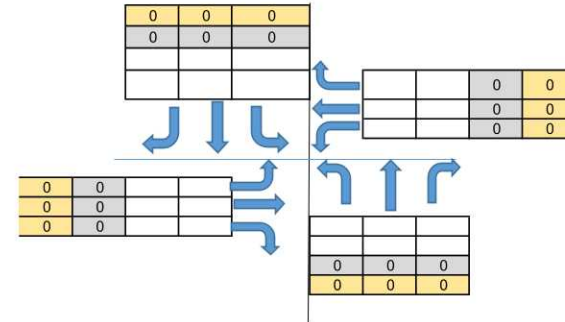
AM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | | |
|------------------------|------------|------|-------|------------|------|-------|-----------|------|-------|-----------|------|-------|---------------------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | |
| Existing Volume (2024) | 188 | 787 | 14 | 305 | 1221 | 390 | 858 | 669 | 124 | 48 | 96 | 302 | 5002 Existing Total |

PROJECT TRIPS

| | IN | OUT |
|----|-----|-----|
| AM | 860 | 509 |
| PM | 589 | 706 |



EXISTING TRAFFIC CALCS FOR ROADWAY SEGMENTS

| | | |
|--------------------------------------|----|------|
| Forest Hill Blvd N. of Wellington Tr | | |
| AM | NB | 1947 |
| | SB | 1916 |

| | | |
|--------------------------------------|----|-----|
| Wellington Tr E. of Forest Hill Blvd | | |
| AM | | |
| | WB | 446 |

CMA INTERSECTION ANALYSIS
K-PARK MUPD
7 - WELLINGTON TRACE S. AT FOREST HILL BOULEVARD

INPUT DATA

Comments:

Growth Rate = 1.00% Peak Season = 1.11 Current Year = 2024 Buildout Year = 2030

AM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | |
|---------------------------|------------|------|-------|------------|------|-------|-----------|------|-------|-----------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volume (2024) | 28 | 808 | 181 | 41 | 999 | 14 | 18 | 11 | 3 | 250 | 6 | 42 |
| Peak Season Adjustment | 3 | 89 | 20 | 5 | 110 | 2 | 2 | 1 | 0 | 28 | 1 | 5 |
| Background Traffic Growth | 2 | 55 | 12 | 3 | 68 | 1 | 1 | 1 | 0 | 17 | 0 | 3 |
| 1.0% Background Growth | 2 | 55 | 12 | 3 | 68 | 1 | 1 | 1 | 0 | 17 | 0 | 3 |
| Major Projects Traffic | 0 | 92 | 13 | 0 | 58 | 0 | 0 | 0 | 0 | 12 | 0 | 0 |
| Background Traffic Used | 2 | 147 | 25 | 3 | 126 | 1 | 1 | 1 | 0 | 29 | 0 | 3 |
| 2030 Background Traffic | 33 | 1044 | 226 | 48 | 1235 | 16 | 21 | 13 | 4 | 307 | 7 | 49 |
| Project Traffic | 0 | 56 | 5 | 0 | 95 | 0 | 0 | 0 | 0 | 9 | 0 | 0 |

2401 Existing Total

164 BG Total

339 1% + MP Total

| | | | | | | | | | | | | |
|-----------------------|--------------|-------------|------------|--------------|-------------|-----------|-----------|-----------|----------|------------|----------|-----------|
| Total | 33 | 1100 | 231 | 48 | 1330 | 16 | 21 | 13 | 4 | 316 | 7 | 49 |
| Approach Total | 1,364 | | | 1,395 | | | 38 | | | 372 | | |

CRITICAL VOLUME ANALYSIS

| | 1 | 2 | 1 | 1 | 2 | < | 1 | 1 | < | 1 | 1 | < |
|-------------------------------|----|-----|-----|-----|-----|----|----|-----|---|-----|----|---|
| No. of Lanes | 33 | 550 | 231 | 48 | 668 | 0 | 21 | 16 | 0 | 316 | 47 | 0 |
| Per Lane Volume | | | | | | | | | | | | |
| Right on Red | | | 60 | | | 0 | | | 0 | | | 0 |
| Overlaps Left | | | 316 | | | 21 | | | 0 | | | 0 |
| Adj. Per Lane Volume | 33 | 550 | 0 | 48 | 668 | 0 | 21 | 16 | 0 | 316 | 47 | 0 |
| Through/Right Volume | | 550 | | | 668 | | | 16 | | | 47 | |
| Opposing Left Turns | | 48 | | | 33 | | | 316 | | | 21 | |
| Critical Volume for Approach | | 598 | | | 701 | | | 332 | | | 68 | |
| Critical Volume for Direction | | | | 701 | | | | 332 | | | | |

| | | | | | | | | | | | | |
|-------------------------------------|--------------|--|--|--|--|--|--|--|--|--|--|--|
| Intersection Critical Volume | 1,033 | | | | | | | | | | | |
| STATUS? | UNDER | | | | | | | | | | | |

PM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | |
|---------------------------|------------|------|-------|------------|------|-------|-----------|------|-------|-----------|------|-------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volume (2024) | 27 | 1089 | 161 | 46 | 1037 | 22 | 73 | 45 | 21 | 182 | 8 | 29 |
| Peak Season Adjustment | 3 | 120 | 18 | 5 | 114 | 2 | 8 | 5 | 2 | 20 | 1 | 3 |
| Background Traffic Growth | 2 | 74 | 11 | 3 | 71 | 2 | 5 | 3 | 1 | 12 | 1 | 2 |
| 1.0% Background Growth | 2 | 74 | 11 | 3 | 71 | 2 | 5 | 3 | 1 | 12 | 1 | 2 |
| Major Projects Traffic | 0 | 53 | 11 | 1 | 83 | 0 | 0 | 0 | 0 | 13 | 0 | 1 |
| Background Traffic Used | 2 | 127 | 22 | 4 | 154 | 2 | 5 | 3 | 1 | 25 | 1 | 3 |
| 2030 Background Traffic | 32 | 1336 | 201 | 55 | 1305 | 26 | 86 | 53 | 25 | 227 | 9 | 35 |
| Project Traffic | 0 | 78 | 7 | 0 | 65 | 0 | 0 | 0 | 0 | 6 | 0 | 0 |

2740 Existing Total

187 BG Total

349 1% + MP Total

| | | | | | | | | | | | | |
|-----------------------|--------------|-------------|------------|--------------|-------------|-----------|------------|-----------|-----------|------------|----------|-----------|
| Total | 32 | 1414 | 208 | 55 | 1370 | 26 | 86 | 53 | 25 | 233 | 9 | 35 |
| Approach Total | 1,654 | | | 1,451 | | | 164 | | | 278 | | |

Critical Volume Analysis

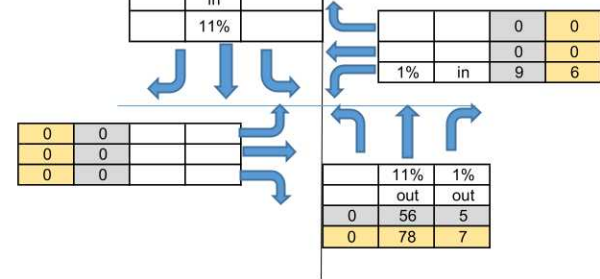
| | 1 | 2 | 1 | 1 | 2 | < | 1 | 1 | < | 1 | 1 | < |
|-------------------------------|----|-----|-----|-----|-----|----|----|-----|---|-----|-----|---|
| No. of Lanes | 32 | 707 | 208 | 55 | 693 | 0 | 86 | 78 | 0 | 233 | 35 | 0 |
| Per Lane Volume | | | | | | | | | | | | |
| Right on Red | | | 60 | | | 0 | | | 0 | | | 0 |
| Overlaps Left | | | 233 | | | 86 | | | 0 | | | 0 |
| Adj. Per Lane Volume | 32 | 707 | 0 | 55 | 693 | 0 | 86 | 78 | 0 | 233 | 35 | 0 |
| Through/Right Volume | | 707 | | | 693 | | | 78 | | | 35 | |
| Opposing Left Turns | | 55 | | | 32 | | | 233 | | | 86 | |
| Critical Volume for Approach | | 762 | | | 725 | | | 311 | | | 121 | |
| Critical Volume for Direction | | | | 762 | | | | 311 | | | | |

| | | | | | | | | | | | | |
|-------------------------------------|--------------|--|--|--|--|--|--|--|--|--|--|--|
| Intersection Critical Volume | 1,073 | | | | | | | | | | | |
| STATUS? | UNDER | | | | | | | | | | | |

PROJECT TRIPS

| | IN | OUT |
|----|-----|-----|
| AM | 860 | 509 |
| PM | 589 | 706 |

| | | |
|---|-----|---|
| 0 | 65 | 0 |
| 0 | 95 | 0 |
| | in | |
| | 11% | |



Wellington Trace E. of Forest Hill Blvd
 AM
 WB 331

CMA INTERSECTION ANALYSIS
K-PARK MUPD
18 - BINKS FOREST DRIVE AT GREENVIEW SHORES BLVD

INPUT DATA

Comments: **USED FOR BINKS FOREST AND GREENVIEW SHORES COUNTS**

Growth Rate = 1.00% Peak Season = 1.09 Current Year = 2024 Buildout Year = 2030

AM Peak Hour

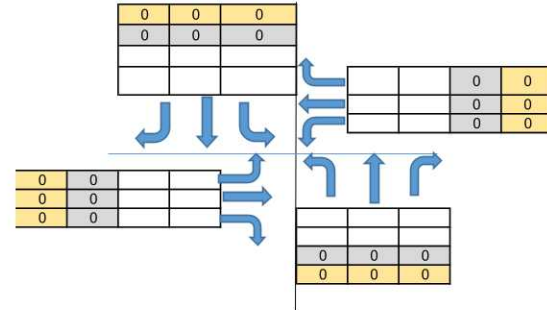
INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | | |
|------------------------|------------|------|-------|------------|------|-------|-----------|------|-------|-----------|------|-------|------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | |
| Existing Volume (2024) | 0 | 0 | 0 | 422 | 0 | 78 | 145 | 57 | 0 | 0 | 45 | 402 | 1149 |
| Peak Season Adjustment | 0 | 0 | 0 | 38 | 0 | 7 | 13 | 5 | 0 | 0 | 4 | 36 | |

Existing Total

PROJECT TRIPS

| | IN | OUT |
|----|-----|-----|
| AM | 860 | 509 |
| PM | 589 | 706 |



EXISTING TRAFFIC CALCS FOR ROADWAY SEGMENTS

| | | |
|---|----|-----|
| Binks Forest Dr N. of Greenview Shores Blvd | | |
| AM | NB | 596 |
| | SB | 545 |

| | | |
|---|----|-----|
| Aero Club Road W. of Binks Forest Drive | | |
| AM | EB | 220 |

| | | |
|---|----|-----|
| Greenview Shores Blvd E. of Binks Forest Dr | | |
| AM | EB | 522 |
| | WB | 487 |

PM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | | |
|------------------------|------------|------|-------|------------|------|-------|-----------|------|-------|-----------|------|-------|------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | |
| Existing Volume (2024) | 0 | 0 | 0 | 391 | 0 | 164 | 82 | 73 | 0 | 0 | 83 | 430 | 1223 |
| Peak Season Adjustment | 0 | 0 | 0 | 35 | 0 | 15 | 7 | 7 | 0 | 0 | 7 | 39 | |

Existing Total

| | | |
|---|----|-----|
| Binks Forest Dr N. of Greenview Shores Blvd | | |
| PM | NB | 558 |
| | SB | 605 |

| | | |
|---|----|-----|
| Greenview Shores Blvd E. of Binks Forest Dr | | |
| PM | EB | 506 |
| | WB | 559 |

Traffic Survey Specialists, Inc.

85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Phone (561) 272-3255

BIRKDALE DRIVE & WELLINGTON TRACE
WELLINGTON, FLORIDA
VIDEO COUNT
NOT SIGNALIZED

File Name : WT & BD 12th
Site Code : 220144
Start Date : 9/12/2022
Page No : 1

**NOT
COUNTED**

Groups Printed- LIGHT VEHICLES - HEAVY VEHICLES

| | WELLINGTON TRACE From North | | | | | BIRKDALE DRIVE From East | | | | | WELLINGTON TRACE From South | | | | | BIRKDALE DRIVE From West | | | | | |
|----------------|--------------------------------|------|------|-------|------------|-----------------------------|------|------|-------|------------|--------------------------------|------|------|-------|------------|-----------------------------|------|------|-------|------------|------------|
| Start Time | UTurn | Left | Thru | Right | App. Total | UTurn | Left | Thru | Right | App. Total | UTurn | Left | Thru | Right | App. Total | UTurn | Left | Thru | Right | App. Total | Int. Total |
| 07:00 AM | 0 | 15 | 17 | 0 | 32 | 0 | 6 | 1 | 38 | 45 | 0 | 4 | 44 | 11 | 59 | 0 | 3 | 1 | 5 | 9 | 145 |
| 07:15 AM | 0 | 19 | 27 | 0 | 46 | 0 | 11 | 0 | 34 | 45 | 0 | 0 | 48 | 23 | 71 | 0 | 1 | 1 | 0 | 2 | 164 |
| 07:30 AM | 0 | 14 | 50 | 0 | 64 | 0 | 19 | 0 | 38 | 57 | 0 | 0 | 69 | 28 | 97 | 0 | 1 | 2 | 0 | 3 | 221 |
| 07:45 AM | 0 | 6 | 68 | 0 | 74 | 0 | 14 | 1 | 29 | 44 | 0 | 1 | 75 | 11 | 87 | 0 | 3 | 1 | 3 | 7 | 212 |
| Total | 0 | 54 | 162 | 0 | 216 | 0 | 50 | 2 | 139 | 191 | 0 | 5 | 236 | 73 | 314 | 0 | 8 | 5 | 8 | 21 | 742 |
| | | | | | | | | | | | | | | | | | | | | | |
| 02:00 PM | 0 | 17 | 19 | 0 | 36 | 0 | 12 | 1 | 18 | 31 | 0 | 0 | 24 | 11 | 35 | 0 | 1 | 1 | 1 | 3 | 105 |
| 02:15 PM | 0 | 11 | 25 | 0 | 36 | 0 | 11 | 3 | 32 | 46 | 0 | 0 | 31 | 10 | 41 | 0 | 0 | 3 | 2 | 5 | 128 |
| 02:30 PM | 0 | 18 | 15 | 0 | 33 | 0 | 5 | 4 | 25 | 34 | 0 | 2 | 37 | 6 | 45 | 0 | 0 | 0 | 4 | 4 | 116 |
| 02:45 PM | 0 | 11 | 25 | 0 | 36 | 0 | 6 | 4 | 29 | 39 | 0 | 2 | 27 | 4 | 33 | 0 | 1 | 1 | 0 | 2 | 110 |
| Total | 0 | 57 | 84 | 0 | 141 | 0 | 34 | 12 | 104 | 150 | 0 | 4 | 119 | 31 | 154 | 0 | 2 | 5 | 7 | 14 | 459 |
| Grand Total | 0 | 111 | 246 | 0 | 357 | 0 | 84 | 14 | 243 | 341 | 0 | 9 | 355 | 104 | 468 | 0 | 10 | 10 | 15 | 35 | 1201 |
| Apprch % | 0 | 31.1 | 68.9 | 0 | | 0 | 24.6 | 4.1 | 71.3 | | 0 | 1.9 | 75.9 | 22.2 | | 0 | 28.6 | 28.6 | 42.9 | | |
| Total % | 0 | 9.2 | 20.5 | 0 | 29.7 | 0 | 7 | 1.2 | 20.2 | 28.4 | 0 | 0.7 | 29.6 | 8.7 | 39 | 0 | 0.8 | 0.8 | 1.2 | 2.9 | |
| LIGHT VEHICLES | 0 | 98.2 | 97.6 | 0 | 97.8 | 0 | 98.8 | 100 | 97.5 | 97.9 | 0 | 100 | 98.3 | 99 | 98.5 | 0 | 100 | 80 | 100 | 94.3 | 98 |
| HEAVY VEHICLES | 0 | 1.8 | 2.4 | 0 | 2.2 | 0 | 1.2 | 0 | 2.5 | 2.1 | 0 | 0 | 1.7 | 1 | 1.5 | 0 | 0 | 20 | 0 | 5.7 | 2 |

Volume not counted because camera position had been moved.

AM EB = 132 x 1.12 = 148
AM WB = 191 x 1.12 = 214

Exhibit 3A
Wellington Speed and Count Study
Traffic Volume and Growth - Weekday

**FOR INFORMATIONAL PURPOSES
ONLY - NOT USED FOR ANALYSIS**

| Loc # | Road | From | To | Lanes | Daily Traffic Volumes | | | 2022 AM Peak Hour ² | | 2022 PM Peak Hour ² | |
|-------|------------------------------------|-------------------------------|-------------------------------|---------|-----------------------|-------------------|------------------|--------------------------------|-------|--------------------------------|-------|
| | | | | | 2018 ¹ | 2022 ² | 4-Yr Growth Rate | NB/EB | SB/WB | NB/EB | SB/WB |
| 1 | Flying Cow Ranch Road | Southern Boulevard | 1 Mile South | 2L | 1,708 | 1,782 | 1.07% /Year | 51 | 84 | 83 | 87 |
| 2 | Flying Cow Ranch Road | 1 Mile South | Rustic Road | 2L | N/A | 1,784 | /Year | 50 | 84 | 77 | 75 |
| 3 | Binks Forest Drive | Southern Boulevard | Greenview Shores Boulevard | 4LD | 13,181 | 13,373 | 0.36% /Year | 749 | 575 | 589 | 600 |
| 4 | Aero Club Drive | Binks Forest Drive | Greenbriar Boulevard | 2L | 5,817 | 4,098 | -8.38% /Year | 115 | 213 | 194 | 150 |
| 5 | Greenbriar Boulevard | Aero Club Drive | Greenview Shores Boulevard | 2L | 6,301 | 2,999 | -16.94% /Year | 192 | 167 | 216 | 168 |
| 6 | Greenview Shores Boulevard | Binks Forest Drive | Wellington Trace | 4LD | 13,212 | 13,082 | -0.25% /Year | 484 | 430 | 651 | 608 |
| 7 | Greenview Shores Boulevard | Wellington Trace | South Shore Boulevard | 4LD | 19,343 | 16,708 | -3.59% /Year | 641 | 824 | 722 | 731 |
| 8 | Wellington Trace | Greenview Shores Boulevard | Big Blue Trace | 4LD | 24,104 | 23,493 | -0.64% /Year | 875 | 788 | 963 | 996 |
| 9 | Wellington Trace | Big Blue Trace | Forest Hill Boulevard (North) | 4LD | 21,732 | 22,600 | 0.98% /Year | 963 | 783 | 885 | 1,027 |
| 10 | Wellington Trace | Forest Hill Boulevard (North) | Forest Hill Boulevard (South) | 2L | 6,033 | 5,900 | -0.56% /Year | 343 | 224 | 306 | 271 |
| 11 | Paddock Drive | Greenview Shores Boulevard | Big Blue Trace | 2L | 2,438 | 2,667 | 2.27% /Year | 120 | 110 | 187 | 131 |
| 12 | Big Blue Trace | Southern Boulevard | Wellington Trace | 2L/4L | 11,465 | 8,443 | -7.36% /Year | 436 | 390 | 336 | 394 |
| 13 | Big Blue Trace | Wellington Trace | South Shore Boulevard | 2L | 11,760 | 11,565 | -0.42% /Year | 271 | 506 | 481 | 480 |
| 14 | Forest Hill Boulevard | Southern Boulevard | Wellington Trace | 6LD | 39,502 | 47,545 | 4.74% /Year | 1,441 | 2,368 | 1,768 | 2,220 |
| 15 | Forest Hill Boulevard ³ | Wellington Trace | South Shore Boulevard | 4LD/6LD | 30,258 | 28,664 | -1.34% /Year | 930 | 1,215 | 1,248 | 1,275 |
| 16 | Forest Hill Boulevard | South Shore Boulevard | SR 7 | 6LD | 49,836 | 53,987 | 2.02% /Year | 2,546 | 1,622 | 2,330 | 2,202 |
| 17 | Birkdale Drive | Forest Hill Boulevard | Wellington Trace | 2L | 4,229 | 3,303 | -5.99% /Year | 113 | 211 | 239 | 98 |
| 18 | Stribling Way | Forest Hill Boulevard | Pierson Road | 2L | 13,259 | 13,303 | 0.08% /Year | 265 | 799 | 610 | 651 |
| 19 | Stribling Way | Pierson Road | SR 7 | 2L | 16,078 | 14,618 | -2.35% /Year | 737 | 443 | 743 | 670 |
| 20 | Stribling Way | SR 7 | Lyons Road | 2L | 5,613 | 6,315 | 2.99% /Year | 467 | 437 | 408 | 250 |
| 21 | South Shore Boulevard ³ | Forest Hill Boulevard | Greenview Shores Boulevard | 4LD | 26,302 | 14,057 | -14.50% /Year | 639 | 716 | 627 | 501 |
| 22 | South Shore Boulevard | Greenview Shores Boulevard | Pierson Road | 4LD | 23,417 | 19,837 | -4.06% /Year | 528 | 875 | 986 | 688 |
| 23 | South Shore Boulevard | Pierson Road | Lake Worth Road | 2LD | 18,764 | 16,444 | -3.25% /Year | 486 | 733 | 816 | 598 |
| 24 | 40th Street South | Palm Beach Point Boulevard | Lake Worth Road | 2L | N/A | 2,187 | /Year | 39 | 94 | 131 | 78 |
| 25 | Lake Worth Road | South Shore Boulevard | 120th Avenue South | 2L | 12,936 | 11,164 | -3.62% /Year | 469 | 398 | 457 | 557 |
| 26 | Pierson Road | South Shore Boulevard | Stribling Way | 2L | 4,743 | 4,238 | -2.78% /Year | 132 | 141 | 209 | 214 |
| 27 | Pierson Road | Ousley Farms Road | South Shore Boulevard | 2L | 10,154 | 4,796 | -17.10% /Year | 166 | 245 | 214 | 165 |
| 28 | South Shore Boulevard | Lake Worth Road | 50th Street South | 2L | 5,202 | 4,600 | -3.03% /Year | 106 | 230 | 242 | 138 |
| 29 | 120th Avenue South | Pierson Road | Lake Worth Road | 2L | 1,056 | 4,001 | 39.52% /Year | 149 | 114 | 274 | 168 |
| 30 | 120th Avenue South | Lake Worth Road | 50th Street South | 2L | 3,461 | 1,800 | -15.08% /Year | 53 | 75 | 75 | 79 |
| 31 | 50th Street South | 130th Avenue South | 120th Avenue South | 2L | 3,523 | 4,029 | 3.41% /Year | 146 | 159 | 199 | 146 |
| 32 | Little Ranches Trail | Southern Boulevard | Acme Road | 2L | 2,381 | 2,304 | -0.82% /Year | 92 | 76 | 88 | 87 |

¹ Source: Wellington Traffic Counts and Analysis, April 11, 2018.

² See Appendix A for count data.

³ Locations 15 and 21 were recounted in June and adjusted based on peak factors from control Location #9. See Appendix A. Use with caution.

CMA INTERSECTION ANALYSIS
K-PARK MUPD
1 - STRIBLING WAY AT STATE ROAD 7

INPUT DATA

Comments:

Growth Rate = 1.00% Peak Season = 1.00 Current Year = 2025 Buildout Year = 2030

AM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | |
|---------------------------|--------------|-------------|------------|--------------|-------------|------------|--------------|------------|------------|------------|------------|------------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volume (2025) | 381 | 2570 | 228 | 120 | 2225 | 93 | 85 | 85 | 663 | 303 | 86 | 133 |
| Peak Season Adjustment | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 19 | 131 | 12 | 6 | 113 | 5 | 4 | 4 | 34 | 15 | 4 | 7 |
| 1.0% Background Growth | 19 | 131 | 12 | 6 | 113 | 5 | 4 | 4 | 34 | 15 | 4 | 7 |
| Major Projects Traffic | 14 | 76 | 3 | 0 | 154 | 27 | 21 | 12 | 9 | 1 | 15 | 0 |
| Background Traffic Used | 33 | 207 | 15 | 6 | 267 | 32 | 25 | 16 | 43 | 16 | 19 | 7 |
| Project Traffic - Related | 0 | 25 | 8 | 0 | 63 | 38 | 42 | 8 | 0 | 13 | 13 | 0 |
| Project Traffic - School | 0 | 34 | 17 | 0 | 245 | 0 | 103 | 17 | 92 | 31 | 31 | 0 |
| 2030 Background Traffic | 414 | 2777 | 243 | 126 | 2492 | 125 | 110 | 101 | 706 | 319 | 105 | 140 |
| Total | 414 | 2836 | 268 | 126 | 2800 | 163 | 255 | 126 | 798 | 363 | 149 | 140 |
| Approach Total | 3,518 | | | 3,089 | | | 1,179 | | | 653 | | |

CRITICAL VOLUME ANALYSIS

| | | | | | | | | | | | | |
|-------------------------------|------|-----|------|-----|------|-----|-------|-----|-----|-----|-----|-----|
| No. of Lanes | 1 | 4 | 1 | 1 | 4 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |
| Per Lane Volume | 414 | 709 | 268 | 126 | 700 | 163 | 255 | 126 | 798 | 363 | 149 | 140 |
| Right on Red | | | 60 | | | 60 | | | 60 | | | 60 |
| Overlaps Left | | | 363 | | | 255 | | | 414 | | | 126 |
| Adj. Per Lane Volume | 414 | 649 | | 126 | 700 | 0 | 255 | 126 | 323 | 363 | 149 | 0 |
| Through/Right Volume | | 649 | | | 700 | | | 323 | | | 149 | |
| Opposing Left Turns | | 126 | | | 414 | | | 363 | | | 255 | |
| Critical Volume for Approach | | 775 | | | 1115 | | | 687 | | | 405 | |
| Critical Volume for Direction | | | 1115 | | | | | | 687 | | | |
| Intersection Critical Volume | | | | | | | 1,801 | | | | | |
| STATUS? | OVER | | | | | | | | | | | |

PM Peak Hour

INTERSECTION VOLUME DEVELOPMENT

| | Northbound | | | Southbound | | | Eastbound | | | Westbound | | |
|---------------------------|--------------|-------------|------------|--------------|-------------|------------|--------------|------------|------------|------------|------------|-----------|
| | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right | Left | Thru | Right |
| Existing Volume (2025) | 450 | 2266 | 330 | 103 | 2022 | 92 | 127 | 160 | 548 | 259 | 155 | 70 |
| Peak Season Adjustment | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Background Traffic Growth | 23 | 116 | 17 | 5 | 103 | 5 | 6 | 8 | 28 | 13 | 8 | 4 |
| 1.0% Background Growth | 23 | 116 | 17 | 5 | 103 | 5 | 6 | 8 | 28 | 13 | 8 | 4 |
| Major Projects Traffic | 16 | 260 | 4 | 0 | 182 | 35 | 38 | 19 | 16 | 4 | 20 | 0 |
| Background Traffic Used | 39 | 376 | 21 | 5 | 285 | 40 | 44 | 27 | 44 | 17 | 28 | 4 |
| Project Traffic - Related | 0 | 85 | 28 | 0 | 122 | 73 | 141 | 28 | 0 | 24 | 24 | 0 |
| Project Traffic - School | 0 | 15 | 7 | 0 | 43 | 0 | 45 | 7 | 16 | 5 | 5 | 0 |
| 2030 Background Traffic | 489 | 2642 | 351 | 108 | 2307 | 132 | 171 | 187 | 592 | 276 | 183 | 74 |
| Total | 489 | 2742 | 386 | 108 | 2472 | 205 | 357 | 222 | 608 | 305 | 212 | 74 |
| Approach Total | 3,616 | | | 2,785 | | | 1,188 | | | 591 | | |

Critical Volume Analysis

| | | | | | | | | | | | | |
|-------------------------------|------|-----|-----|------|------|-------|-----|-----|-----|-----|-----|-----|
| No. of Lanes | 1 | 4 | 1 | 1 | 4 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |
| Per Lane Volume | 489 | 685 | 386 | 108 | 618 | 205 | 357 | 222 | 608 | 305 | 212 | 74 |
| Right on Red | | | 60 | | | 60 | | | 60 | | | 60 |
| Overlaps Left | | | 305 | | | 357 | | | 489 | | | 108 |
| Adj. Per Lane Volume | 489 | 625 | | 108 | 618 | 0 | 357 | 222 | 59 | 305 | 212 | 0 |
| Through/Right Volume | | 625 | | | 618 | | | 222 | | | 212 | |
| Opposing Left Turns | | 108 | | | 489 | | | 305 | | | 357 | |
| Critical Volume for Approach | | 734 | | | 1107 | | | 527 | | | 569 | |
| Critical Volume for Direction | | | | 1107 | | | | | 569 | | | |
| Intersection Critical Volume | | | | | | 1,676 | | | | | | |
| STATUS? | OVER | | | | | | | | | | | |

Note:

MIXED USE NET TRIPS

| | IN | OUT |
|----|-----|-----|
| AM | 253 | 169 |
| PM | 488 | 564 |

| | | |
|-----|-----|---|
| 73 | 122 | 0 |
| 38 | 63 | 0 |
| IN | IN | |
| 15% | 25% | |

| | | | |
|----|----|----|----|
| | | 0 | 0 |
| 5% | IN | 13 | 24 |
| 5% | IN | 13 | 24 |

| | | | |
|-----|----|-----|-----|
| 141 | 42 | OUT | 25% |
| 28 | 8 | OUT | 5% |
| 0 | 0 | | 0% |

| | | |
|-----|-----|-----|
| 0% | 15% | 5% |
| OUT | OUT | OUT |
| 0 | 25 | 8 |
| 0 | 85 | 28 |

SCHOOL NET TRIPS

| | IN | OUT |
|----|-----|-----|
| AM | 613 | 343 |
| PM | 108 | 149 |

| | | |
|---|-----|---|
| 0 | 43 | 0 |
| 0 | 245 | 0 |
| | IN | |
| | 40% | |

| | | | |
|----|----|----|---|
| | | 0 | 0 |
| 5% | IN | 31 | 5 |
| 5% | IN | 31 | 5 |

| | | | |
|----|-----|-----|-----|
| 45 | 103 | OUT | 30% |
| 7 | 17 | OUT | 5% |
| 16 | 92 | IN | 15% |

























| | | |
|-----|-----|-----|
| 0% | 10% | 5% |
| OUT | OUT | OUT |
| 0 | 34 | 17 |
| 0 | 15 | 7 |

EXISTING TRAFFIC CALCS FOR ROADWAY SEGMENTS

| | | | | | | | | | |
|--------------------------|----|------|--|----|----|------|--|--|--|
| SR 7 N. of Stribling Way | | | | | | | | | |
| AM | NB | 2788 | | PM | NB | 2463 | | | |
| | SB | 2438 | | | SB | 2217 | | | |
| SR 7 S. of Stribling Way | | | | | | | | | |
| AM | NB | 3179 | | PM | NB | 3046 | | | |
| | SB | 3191 | | | SB | 2829 | | | |
| Stribling Way E. of SR 7 | | | | | | | | | |
| AM | EB | 433 | | PM | EB | 593 | | | |
| | WB | 522 | | | WB | 484 | | | |
| Stribling Way W. of SR 7 | | | | | | | | | |
| AM | EB | 833 | | PM | EB | 835 | | | |
| | WB | 560 | | | WB | 697 | | | |

Lanes, Volumes, Timings
1: State Road 7 & Stribling Way

12/04/2025













| |  |  |  |  |  |  |  |  |  |  |  |  |
|----------------------------|---|---|---|---|---|---|---|---|---|---|---|---|
| Lane Group | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations |  |  |  |  |  |  |  |  |  |  |  |  |
| Traffic Volume (vph) | 255 | 126 | 798 | 363 | 149 | 140 | 414 | 2836 | 268 | 126 | 2800 | 163 |
| Future Volume (vph) | 255 | 126 | 798 | 363 | 149 | 140 | 414 | 2836 | 268 | 126 | 2800 | 163 |
| Ideal Flow (vphpl) | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 |
| Storage Length (ft) | 400 | | 500 | 280 | | 260 | 700 | | 220 | 0 | | 0 |
| Storage Lanes | 2 | | 2 | 1 | | 1 | 2 | | 0 | 1 | | 1 |
| Taper Length (ft) | 25 | | | 50 | | | 100 | | | 25 | | |
| Lane Util. Factor | 0.97 | 1.00 | 0.88 | 1.00 | 1.00 | 1.00 | 0.97 | 0.86 | 1.00 | 1.00 | 0.86 | 1.00 |
| Frt | | | 0.850 | | | 0.850 | | | 0.850 | | | 0.850 |
| Flt Protected | 0.950 | | | 0.950 | | | 0.950 | | | 0.950 | | |
| Satd. Flow (prot) | 3433 | 1863 | 2787 | 1770 | 1863 | 1583 | 3433 | 6408 | 1583 | 1770 | 6408 | 1583 |
| Flt Permitted | 0.950 | | | 0.345 | | | 0.950 | | | 0.950 | | |
| Satd. Flow (perm) | 3433 | 1863 | 2787 | 643 | 1863 | 1583 | 3433 | 6408 | 1583 | 1770 | 6408 | 1583 |
| Right Turn on Red | | | Yes | | | Yes | | | Yes | | | Yes |
| Satd. Flow (RTOR) | | | 76 | | | 121 | | | 160 | | | 121 |
| Link Speed (mph) | | 30 | | | 30 | | | 50 | | | 50 | |
| Link Distance (ft) | | 1218 | | | 3652 | | | 1113 | | | 5373 | |
| Travel Time (s) | | 27.7 | | | 83.0 | | | 15.2 | | | 73.3 | |
| Peak Hour Factor | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 |
| Adj. Flow (vph) | 268 | 133 | 840 | 382 | 157 | 147 | 436 | 2985 | 282 | 133 | 2947 | 172 |
| Shared Lane Traffic (%) | | | | | | | | | | | | |
| Lane Group Flow (vph) | 268 | 133 | 840 | 382 | 157 | 147 | 436 | 2985 | 282 | 133 | 2947 | 172 |
| Enter Blocked Intersection | No | No | No | No | No | No | No | No | No | No | No | No |
| Lane Alignment | Left | Left | Right | Left | Left | Right | Left | Left | Right | Left | Left | Right |
| Median Width(ft) | | 24 | | | 24 | | | 48 | | | 48 | |
| Link Offset(ft) | | 0 | | | 0 | | | 0 | | | 0 | |
| Crosswalk Width(ft) | | 16 | | | 16 | | | 16 | | | 16 | |
| Two way Left Turn Lane | | | | | | | | | | | | |
| Headway Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Turning Speed (mph) | 15 | | 9 | 15 | | 9 | 15 | | 9 | 15 | | 9 |
| Turn Type | Prot | NA | pm+ov | pm+pt | NA | pm+ov | Prot | NA | pm+ov | Prot | NA | pm+ov |
| Protected Phases | 3 | 8 | 1 | 7 | 4 | 5 | 1 | 6 | 7 | 5 | 2 | 3 |
| Permitted Phases | | | 8 | 4 | | 4 | | | 6 | | | 2 |
| Detector Phase | 3 | 8 | 1 | 7 | 4 | 5 | 1 | 6 | 7 | 5 | 2 | 3 |
| Switch Phase | | | | | | | | | | | | |
| Minimum Initial (s) | 4.0 | 6.0 | 4.0 | 4.0 | 6.0 | 4.0 | 4.0 | 20.0 | 4.0 | 4.0 | 20.0 | 4.0 |
| Minimum Split (s) | 12.0 | 14.0 | 11.5 | 12.0 | 14.0 | 11.5 | 11.5 | 27.5 | 12.0 | 11.5 | 27.5 | 12.0 |
| Total Split (s) | 33.0 | 26.0 | 34.0 | 33.0 | 26.0 | 22.0 | 34.0 | 99.0 | 33.0 | 22.0 | 87.0 | 33.0 |
| Total Split (%) | 18.3% | 14.4% | 18.9% | 18.3% | 14.4% | 12.2% | 18.9% | 55.0% | 18.3% | 12.2% | 48.3% | 18.3% |
| Maximum Green (s) | 25.0 | 18.0 | 26.5 | 25.0 | 18.0 | 14.5 | 26.5 | 91.5 | 25.0 | 14.5 | 79.5 | 25.0 |
| Yellow Time (s) | 5.0 | 5.0 | 5.5 | 5.0 | 5.0 | 5.5 | 5.5 | 5.5 | 5.0 | 5.5 | 5.5 | 5.0 |
| All-Red Time (s) | 3.0 | 3.0 | 2.0 | 3.0 | 3.0 | 2.0 | 2.0 | 2.0 | 3.0 | 2.0 | 2.0 | 3.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 8.0 | 8.0 | 7.5 | 8.0 | 8.0 | 7.5 | 7.5 | 7.5 | 8.0 | 7.5 | 7.5 | 8.0 |
| Lead/Lag | Lead | Lag | Lag | Lead | Lag | Lag | Lag | Lead | Lead | Lag | Lead | Lead |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Vehicle Extension (s) | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 |
| Recall Mode | None | None | None | None | None | None | None | C-Max | None | None | C-Max | None |
| Act Effct Green (s) | 19.3 | 16.4 | 43.4 | 46.6 | 22.1 | 37.1 | 26.5 | 93.1 | 117.6 | 14.5 | 81.1 | 99.9 |

Total Traffic - With Improvements
Timing Plan: AM Peak

Synchro 12 Light Report
Page 1

Lanes, Volumes, Timings
1: State Road 7 & Stribling Way

12/04/2025

| |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Lane Group | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Actuated g/C Ratio | 0.11 | 0.09 | 0.24 | 0.26 | 0.12 | 0.21 | 0.15 | 0.52 | 0.65 | 0.08 | 0.45 | 0.56 |
| v/c Ratio | 0.73 | 0.79 | 1.15 | 1.19 | 0.69 | 0.35 | 0.86 | 0.90 | 0.26 | 0.94 | 1.02 | 0.18 |
| Control Delay (s/veh) | 89.4 | 109.4 | 120.2 | 159.9 | 91.1 | 11.2 | 82.6 | 36.9 | 1.8 | 139.6 | 70.1 | 3.4 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 7.3 | 0.0 |
| Total Delay (s/veh) | 89.4 | 109.4 | 120.2 | 159.9 | 91.1 | 11.2 | 82.6 | 36.9 | 1.8 | 139.6 | 77.4 | 3.4 |
| LOS | F | F | F | F | F | B | F | D | A | F | E | A |
| Approach Delay (s/veh) | 112.4 | | | 112.3 | | | 39.6 | | | 76.0 | | |
| Approach LOS | F | | | F | | | D | | | E | | |

Intersection Summary

Area Type: Other

Cycle Length: 180

Actuated Cycle Length: 180

Offset: 23 (13%), Referenced to phase 2:SBT and 6:NBT, Start of Green

Natural Cycle: 150

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 1.19

Intersection Signal Delay (s/veh): 68.7









Intersection LOS: E

Intersection Capacity Utilization 107.8%

ICU Level of Service G

Analysis Period (min) 15
























Splits and Phases: 1: State Road 7 & Stribling Way

| | | | | | | | |
|---|--------|---|----|---|----|---|----|
|  | Ø2 (R) |  | Ø1 |  | Ø3 |  | Ø4 |
| 87 s | | 34 s | | 33 s | | 26 s | |
|  | Ø6 (R) |  | Ø5 |  | Ø7 |  | Ø8 |
| 99 s | | 22 s | | 33 s | | 26 s | |

HCM 7th Signalized Intersection Summary

1: State Road 7 & Stribling Way

12/04/2025

























| |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|---|---|---|---|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations |  |  |  |  |  |  |  |  |  |  |  | |
| Traffic Volume (veh/h) | 255 | 126 | 798 | 363 | 149 | 140 | 414 | 2836 | 268 | 126 | 2800 | 163 |
| Future Volume (veh/h) | 255 | 126 | 798 | 363 | 149 | 140 | 414 | 2836 | 268 | 126 | 2800 | 163 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | No | | | No | | | No | | |
| Adj Sat Flow, veh/h/ln | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 |
| Adj Flow Rate, veh/h | 268 | 133 | 777 | 382 | 157 | 84 | 436 | 2985 | 219 | 133 | 2947 | 109 |
| Peak Hour Factor | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 |
| Percent Heavy Veh, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Cap, veh/h | 314 | 167 | 690 | 299 | 256 | 362 | 546 | 3271 | 1026 | 163 | 2842 | 844 |
| Arrive On Green | 0.09 | 0.09 | 0.09 | 0.14 | 0.14 | 0.14 | 0.32 | 1.00 | 1.00 | 0.09 | 0.44 | 0.44 |
| Sat Flow, veh/h | 3456 | 1870 | 2790 | 1781 | 1870 | 1585 | 3456 | 6434 | 1585 | 1781 | 6434 | 1585 |
| Grp Volume(v), veh/h | 268 | 133 | 777 | 382 | 157 | 84 | 436 | 2985 | 219 | 133 | 2947 | 109 |
| Grp Sat Flow(s),veh/h/ln | 1728 | 1870 | 1395 | 1781 | 1870 | 1585 | 1728 | 1609 | 1585 | 1781 | 1609 | 1585 |
| Q Serve(g_s), s | 13.8 | 12.6 | 10.9 | 25.0 | 14.2 | 1.4 | 20.8 | 0.0 | 0.0 | 13.2 | 79.5 | 2.4 |
| Cycle Q Clear(g_c), s | 13.8 | 12.6 | 10.9 | 25.0 | 14.2 | 1.4 | 20.8 | 0.0 | 0.0 | 13.2 | 79.5 | 2.4 |
| Prop In Lane | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 314 | 167 | 690 | 299 | 256 | 362 | 546 | 3271 | 1026 | 163 | 2842 | 844 |
| V/C Ratio(X) | 0.85 | 0.80 | 1.13 | 1.28 | 0.61 | 0.23 | 0.80 | 0.91 | 0.21 | 0.82 | 1.04 | 0.13 |
| Avail Cap(c_a), veh/h | 480 | 187 | 720 | 299 | 256 | 362 | 546 | 3271 | 1026 | 163 | 2842 | 844 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.67 | 0.67 | 0.67 | 0.17 | 0.17 | 0.17 |
| Uniform Delay (d), s/veh | 80.6 | 80.4 | 32.0 | 64.7 | 73.2 | 30.5 | 58.9 | 0.0 | 0.0 | 80.3 | 50.3 | 7.0 |
| Incr Delay (d2), s/veh | 9.0 | 19.3 | 74.7 | 147.8 | 4.3 | 0.3 | 5.6 | 3.5 | 0.3 | 5.6 | 19.3 | 0.1 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 10.8 | 11.3 | 24.8 | 22.3 | 11.6 | 4.0 | 12.0 | 1.4 | 0.2 | 7.9 | 39.7 | 1.9 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 89.6 | 99.7 | 106.7 | 212.5 | 77.4 | 30.8 | 64.5 | 3.5 | 0.3 | 85.9 | 69.5 | 7.1 |
| LnGrp LOS | F | F | F | F | E | C | E | A | A | F | F | A |
| Approach Vol, veh/h | 1178 | | | 623 | | | 3640 | | | 3189 | | |
| Approach Delay, s/veh | 102.0 | | | 153.9 | | | 10.6 | | | 68.1 | | |
| Approach LOS | F | | | F | | | B | | | E | | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 36.0 | 87.0 | 24.4 | 32.7 | 24.0 | 99.0 | 33.0 | 24.0 | | | | |
| Change Period (Y+Rc), s | 7.5 | 7.5 | 8.0 | 8.0 | 7.5 | 7.5 | 8.0 | 8.0 | | | | |
| Max Green Setting (Gmax), s | 26.5 | 79.5 | 25.0 | 18.0 | 14.5 | 91.5 | 25.0 | 18.0 | | | | |
| Max Q Clear Time (g_c+I1), s | 22.8 | 81.5 | 15.8 | 16.2 | 15.2 | 2.0 | 27.0 | 14.6 | | | | |
| Green Ext Time (p_c), s | 0.6 | 0.0 | 0.6 | 0.2 | 0.0 | 67.1 | 0.0 | 1.5 | | | | |
| Intersection Summary | | | | | | | | | | | | |
| HCM 7th Control Delay, s/veh | 54.7 | | | | | | | | | | | |
| HCM 7th LOS | D | | | | | | | | | | | |

Total Traffic - With Improvements
Timing Plan: AM Peak

Synchro 12 Light Report
Page 3

Lanes, Volumes, Timings
1: State Road 7 & Stribling Way

12/04/2025













| |  |  |  |  |  |  |  |  |  |  |  |  |
|----------------------------|---|---|---|---|---|---|---|---|---|---|---|---|
| Lane Group | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations |  |  |  |  |  |  |  |  |  |  |  |  |
| Traffic Volume (vph) | 357 | 222 | 608 | 305 | 212 | 74 | 489 | 2742 | 386 | 108 | 2472 | 205 |
| Future Volume (vph) | 357 | 222 | 608 | 305 | 212 | 74 | 489 | 2742 | 386 | 108 | 2472 | 205 |
| Ideal Flow (vphpl) | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 | 1900 |
| Storage Length (ft) | 400 | | 500 | 280 | | 260 | 700 | | 220 | 0 | | 0 |
| Storage Lanes | 2 | | 2 | 1 | | 1 | 2 | | 0 | 1 | | 1 |
| Taper Length (ft) | 25 | | | 50 | | | 100 | | | 25 | | |
| Lane Util. Factor | 0.97 | 1.00 | 0.88 | 1.00 | 1.00 | 1.00 | 0.97 | 0.86 | 1.00 | 1.00 | 0.86 | 1.00 |
| Frt | | | 0.850 | | | 0.850 | | | 0.850 | | | 0.850 |
| Flt Protected | 0.950 | | | 0.950 | | | 0.950 | | | 0.950 | | |
| Satd. Flow (prot) | 3433 | 1863 | 2787 | 1770 | 1863 | 1583 | 3433 | 6408 | 1583 | 1770 | 6408 | 1583 |
| Flt Permitted | 0.950 | | | 0.232 | | | 0.950 | | | 0.950 | | |
| Satd. Flow (perm) | 3433 | 1863 | 2787 | 432 | 1863 | 1583 | 3433 | 6408 | 1583 | 1770 | 6408 | 1583 |
| Right Turn on Red | | | Yes | | | Yes | | | Yes | | | Yes |
| Satd. Flow (RTOR) | | | 121 | | | 121 | | | 145 | | | 76 |
| Link Speed (mph) | | 30 | | | 30 | | | 50 | | | 50 | |
| Link Distance (ft) | | 1178 | | | 3652 | | | 1113 | | | 5373 | |
| Travel Time (s) | | 26.8 | | | 83.0 | | | 15.2 | | | 73.3 | |
| Peak Hour Factor | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 |
| Adj. Flow (vph) | 376 | 234 | 640 | 321 | 223 | 78 | 515 | 2886 | 406 | 114 | 2602 | 216 |
| Shared Lane Traffic (%) | | | | | | | | | | | | |
| Lane Group Flow (vph) | 376 | 234 | 640 | 321 | 223 | 78 | 515 | 2886 | 406 | 114 | 2602 | 216 |
| Enter Blocked Intersection | No | No | No | No | No | No | No | No | No | No | No | No |
| Lane Alignment | Left | Left | Right | Left | Left | Right | Left | Left | Right | Left | Left | Right |
| Median Width(ft) | | 24 | | | 24 | | | 60 | | | 60 | |
| Link Offset(ft) | | 0 | | | 0 | | | 0 | | | 0 | |
| Crosswalk Width(ft) | | 16 | | | 16 | | | 16 | | | 16 | |
| Two way Left Turn Lane | | | | | | | | | | | | |
| Headway Factor | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Turning Speed (mph) | 15 | | 9 | 15 | | 9 | 15 | | 9 | 15 | | 9 |
| Turn Type | Prot | NA | pm+ov | pm+pt | NA | pm+ov | Prot | NA | pm+ov | Prot | NA | pm+ov |
| Protected Phases | 3 | 8 | 1 | 7 | 4 | 5 | 1 | 6 | 7 | 5 | 2 | 3 |
| Permitted Phases | | | 8 | 4 | | 4 | | | 6 | | | 2 |
| Detector Phase | 3 | 8 | 1 | 7 | 4 | 5 | 1 | 6 | 7 | 5 | 2 | 3 |
| Switch Phase | | | | | | | | | | | | |
| Minimum Initial (s) | 4.0 | 6.0 | 4.0 | 4.0 | 6.0 | 4.0 | 4.0 | 20.0 | 4.0 | 4.0 | 20.0 | 4.0 |
| Minimum Split (s) | 12.0 | 14.0 | 11.5 | 12.0 | 14.0 | 11.5 | 11.5 | 27.5 | 12.0 | 11.5 | 27.5 | 12.0 |
| Total Split (s) | 28.0 | 31.0 | 35.0 | 32.0 | 35.0 | 24.0 | 35.0 | 93.0 | 32.0 | 24.0 | 82.0 | 28.0 |
| Total Split (%) | 15.6% | 17.2% | 19.4% | 17.8% | 19.4% | 13.3% | 19.4% | 51.7% | 17.8% | 13.3% | 45.6% | 15.6% |
| Maximum Green (s) | 20.0 | 23.0 | 27.5 | 24.0 | 27.0 | 16.5 | 27.5 | 85.5 | 24.0 | 16.5 | 74.5 | 20.0 |
| Yellow Time (s) | 5.0 | 5.0 | 5.5 | 5.0 | 5.0 | 5.5 | 5.5 | 5.5 | 5.0 | 5.5 | 5.5 | 5.0 |
| All-Red Time (s) | 3.0 | 3.0 | 2.0 | 3.0 | 3.0 | 2.0 | 2.0 | 2.0 | 3.0 | 2.0 | 2.0 | 3.0 |
| Lost Time Adjust (s) | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Lost Time (s) | 8.0 | 8.0 | 7.5 | 8.0 | 8.0 | 7.5 | 7.5 | 7.5 | 8.0 | 7.5 | 7.5 | 8.0 |
| Lead/Lag | Lag | Lead | Lead | Lag | Lead | Lag | Lead | Lead | Lag | Lag | Lag | Lag |
| Lead-Lag Optimize? | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes |
| Vehicle Extension (s) | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 |
| Recall Mode | None | None | None | None | None | None | None | C-Max | None | None | C-Max | None |
| Act Effct Green (s) | 22.1 | 23.0 | 51.0 | 48.9 | 24.9 | 49.4 | 27.5 | 85.5 | 117.0 | 16.5 | 74.5 | 96.1 |

Total Traffic - With Improvements
Timing Plan: PM Peak

Synchro 12 Light Report
Page 1

Lanes, Volumes, Timings
1: State Road 7 & Stribling Way

12/04/2025

| |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Lane Group | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Actuated g/C Ratio | 0.12 | 0.13 | 0.28 | 0.27 | 0.14 | 0.27 | 0.15 | 0.48 | 0.65 | 0.09 | 0.41 | 0.53 |
| v/c Ratio | 0.89 | 0.98 | 0.73 | 1.09 | 0.87 | 0.15 | 0.98 | 0.95 | 0.38 | 0.70 | 0.98 | 0.25 |
| Control Delay (s/veh) | 99.8 | 129.9 | 33.8 | 141.9 | 105.7 | 1.6 | 117.6 | 45.3 | 8.8 | 102.0 | 65.1 | 8.0 |
| Queue Delay | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Total Delay (s/veh) | 99.8 | 129.9 | 33.8 | 141.9 | 105.7 | 1.6 | 117.6 | 45.3 | 8.8 | 102.0 | 65.1 | 8.0 |
| LOS | F | F | C | F | F | A | F | D | A | F | E | A |
| Approach Delay (s/veh) | 71.6 | | | 111.3 | | | 51.2 | | | 62.3 | | |
| Approach LOS | E | | | F | | | D | | | E | | |

Intersection Summary

Area Type: Other

Cycle Length: 180

Actuated Cycle Length: 180

Offset: 61 (34%), Referenced to phase 2:SBT and 6:NBT, Start of Green

Natural Cycle: 150

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 1.09

Intersection Signal Delay (s/veh): 62.3

Intersection LOS: E

Intersection Capacity Utilization 104.2%

ICU Level of Service G

Analysis Period (min) 15
























Splits and Phases: 1: State Road 7 & Stribling Way



HCM 7th Signalized Intersection Summary

1: State Road 7 & Stribling Way

12/04/2025

| |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|---|---|---|---|---|---|
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations |  |  |  |  |  |  |  |  |  |  |  | |
| Traffic Volume (veh/h) | 357 | 222 | 608 | 305 | 212 | 74 | 489 | 2742 | 386 | 108 | 2472 | 205 |
| Future Volume (veh/h) | 357 | 222 | 608 | 305 | 212 | 74 | 489 | 2742 | 386 | 108 | 2472 | 205 |
| Initial Q (Qb), veh | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Lane Width Adj. | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Ped-Bike Adj(A_pbT) | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 |
| Parking Bus, Adj | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach | No | | | No | | | No | | | No | | |
| Adj Sat Flow, veh/h/ln | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 | 1870 |
| Adj Flow Rate, veh/h | 376 | 234 | 577 | 321 | 223 | 15 | 515 | 2886 | 343 | 114 | 2602 | 153 |
| Peak Hour Factor | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 | 0.95 |
| Percent Heavy Veh, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Cap, veh/h | 449 | 239 | 783 | 280 | 245 | 353 | 528 | 3056 | 964 | 163 | 2663 | 862 |
| Arrive On Green | 0.13 | 0.13 | 0.13 | 0.13 | 0.13 | 0.13 | 0.31 | 0.95 | 0.95 | 0.09 | 0.41 | 0.41 |
| Sat Flow, veh/h | 3456 | 1870 | 2790 | 1781 | 1870 | 1585 | 3456 | 6434 | 1585 | 1781 | 6434 | 1585 |
| Grp Volume(v), veh/h | 376 | 234 | 577 | 321 | 223 | 15 | 515 | 2886 | 343 | 114 | 2602 | 153 |
| Grp Sat Flow(s),veh/h/ln | 1728 | 1870 | 1395 | 1781 | 1870 | 1585 | 1728 | 1609 | 1585 | 1781 | 1609 | 1585 |
| Q Serve(g_s), s | 19.1 | 22.5 | 21.4 | 24.0 | 21.2 | 0.0 | 26.5 | 39.2 | 0.0 | 11.2 | 71.6 | 3.7 |
| Cycle Q Clear(g_c), s | 19.1 | 22.5 | 21.4 | 24.0 | 21.2 | 0.0 | 26.5 | 39.2 | 0.0 | 11.2 | 71.6 | 3.7 |
| Prop In Lane | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 | 1.00 | | 1.00 |
| Lane Grp Cap(c), veh/h | 449 | 239 | 783 | 280 | 245 | 353 | 528 | 3056 | 964 | 163 | 2663 | 862 |
| V/C Ratio(X) | 0.84 | 0.98 | 0.74 | 1.15 | 0.91 | 0.04 | 0.98 | 0.94 | 0.36 | 0.70 | 0.98 | 0.18 |
| Avail Cap(c_a), veh/h | 449 | 239 | 783 | 280 | 281 | 383 | 528 | 3056 | 964 | 163 | 2663 | 862 |
| HCM Platoon Ratio | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 2.00 | 2.00 | 2.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I) | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.68 | 0.68 | 0.68 | 0.34 | 0.34 | 0.34 |
| Uniform Delay (d), s/veh | 76.4 | 78.3 | 27.1 | 76.2 | 77.2 | 54.9 | 62.2 | 3.3 | 1.3 | 79.3 | 51.9 | 6.8 |
| Incr Delay (d2), s/veh | 13.0 | 52.2 | 3.7 | 100.0 | 29.2 | 0.0 | 26.2 | 5.6 | 0.7 | 4.4 | 6.1 | 0.2 |
| Initial Q Delay(d3), s/veh | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| %ile BackOfQ(95%),veh/ln | 14.4 | 20.6 | 12.2 | 30.1 | 18.0 | 1.0 | 16.5 | 5.0 | 1.3 | 7.5 | 34.2 | 3.2 |
| Unsig. Movement Delay, s/veh | | | | | | | | | | | | |
| LnGrp Delay(d), s/veh | 89.4 | 130.4 | 30.8 | 176.2 | 106.3 | 54.9 | 88.4 | 8.9 | 2.0 | 83.8 | 58.1 | 7.0 |
| LnGrp LOS | F | F | C | F | F | D | F | A | A | F | E | A |
| Approach Vol, veh/h | 1187 | | | 559 | | | 3744 | | | 2869 | | |
| Approach Delay, s/veh | 69.0 | | | 145.1 | | | 19.2 | | | 56.4 | | |
| Approach LOS | E | | | F | | | B | | | E | | |
| Timer - Assigned Phs | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | | |
| Phs Duration (G+Y+Rc), s | 35.0 | 82.0 | 31.4 | 31.6 | 24.0 | 93.0 | 32.0 | 31.0 | | | | |
| Change Period (Y+Rc), s | 7.5 | 7.5 | 8.0 | 8.0 | 7.5 | 7.5 | 8.0 | 8.0 | | | | |
| Max Green Setting (Gmax), s | 27.5 | 74.5 | 20.0 | 27.0 | 16.5 | 85.5 | 24.0 | 23.0 | | | | |
| Max Q Clear Time (g_c+I1), s | 28.5 | 73.6 | 21.1 | 23.2 | 13.2 | 41.2 | 26.0 | 24.5 | | | | |
| Green Ext Time (p_c), s | 0.0 | 0.8 | 0.0 | 0.4 | 0.1 | 37.5 | 0.0 | 0.0 | | | | |
| Intersection Summary | | | | | | | | | | | | |
| HCM 7th Control Delay, s/veh | 47.4 | | | | | | | | | | | |
| HCM 7th LOS | D | | | | | | | | | | | |

Total Traffic - With Improvements
Timing Plan: PM Peak

Synchro 12 Light Report
Page 3



Florida Department of Transportation

RON DESANTIS
GOVERNOR

3400 West Commercial Boulevard
Fort Lauderdale, FL 33309

JARED W. PERDUE, P.E.
SECRETARY

September 24, 2025

Bryan Kelley, P.E.
Simmons & White
2581 Metrocentre Boulevard, WPB, FL, 33407

Dear Bryan,

RE: Variance Committee Review to allow for **Category F Driveway**

Applicant/Property Owner: ACME Improvement District

Palm Beach County City of Wellington State Road: 7 Section: 93210000 MP: 21.1

Access Class: 03 Posted Speed: 50 mph SIS: N/A

Site Acreage: 70 Acres Development Size: 15 Multifamily Residential DU (Low-Rise); 180 Room Hotel; 1,750 Student Private School (K-12); 80,000 SF Office; 285,000 SF Shopping Plaza (Including +/- 80,000 SF Restaurant; 31,396 SF Church (existing))

Project Name & Address: K-Park - 10400 Stribling Way

AMRC Meeting Date: 9/4/2025, 11:15 AM

Request:

- **Driveway 1:** Proposed right-in/right-out driveway with right turn lane on the west side of SR 7, located approximately 514 feet south of Stribling Way.
- **Driveway 2:** Proposed signalized intersection on SR 7, located approximately 1,029 feet south of Stribling Way.
- Close the existing right-in/right-out/left-in driveway on the west side of SR 7, located approximately 1,153 feet south of Stribling Way.
- Remove the existing directional median opening on SR 7, located approximately 1,029 feet south of Stribling Way.

This request is: **Approved with Conditions**

Conditions:

- ☐ Driveway 1 shall have a minimum driveway length of 100 feet, measured from the ultimate right-of-way line to the first conflict point in the driveway.
- ☐ The proposed signalized intersection (Driveway 2) shall have a minimum driveway length determined by a traffic study conducted by the applicant.
- ☐ The proposed multilane roundabout shall follow nationwide design guidelines (FDOT FDM Chapter 213). Queuing from the multilane roundabout's eastern approach must not encroach onto SR 7, as determined by a traffic study conducted by the applicant at permit.
- ☐ Right turn lanes are required for Driveways 1 & 2 and shall meet the minimum requirements in the Florida Design Manual (FDM) and shall provide space for a buffered bicycle lane.

Bryan Kelley – K-Park Access Management Review Committee Letter

- ☐ The queue length for the proposed northbound dual left turn lanes at Driveway 2 and the proposed dual northbound left turn lanes at the intersection of SR 7 and Stribling Way, must be determined by a traffic study conducted by the applicant at permit.
- ☐ An Intersection Control Evaluation (ICE) Analysis conducted by the applicant at permit, is required for Driveway 2.
- ☐ The proposed traffic signal support system shall have the structural capacity to implement a minimum of one signal head per lane with five section heads for all three approaches, as needed.
- ☐ The following off-site improvements shall be implemented at the intersection of SR 7 & Stribling Way, by the applicant:
 - An additional northbound left turn lane. The intersection shall be served by dual northbound lefts.
 - An additional eastbound right turn lane.
 - An additional eastbound left turn lane.
 - Extend the southbound right turn lane to meet minimum length as determined by a traffic study conducted by the applicant at permit.
- All existing driveways not approved in this letter must be fully removed and the area restored.
- Additional right-of-way must be donated to the Department to implement the proposed improvements and mitigation, as needed.
- Drainage mitigation is required for any impacts within FDOT right-of-way (i.e. increased runoff or reduction of existing storage).
- A Storm Water Pollution Prevention Plan must be submitted with the application if there will be more than one acre of "disturbed area" (as defined by the Florida Department of Environmental Protection (FDEP)).

Comments:

Please note that the dimensions between driveways are measured from the near edge of pavement to near edge of pavement and dimensions between median openings are measured from centerline to centerline unless otherwise indicated.

The purpose of this letter is to document the conceptual review of the approximate location of driveway(s) to the State Highway system and to note any required improvements. Earlier Department decisions on this request shall be voided unless expressly approved herein. If the above concept is approved, the applicant may submit engineering plans to the Department for permitting. The Department's personnel shall review these plans for compliance with this letter as well as current Department standards and/or specifications. Final design must consider the existing roadway profile and any impacts to the existing drainage system. **Please note that this letter does not guarantee permit approval.** The permit may be denied based on the review of the submitted engineering plans. Be aware that any approved median openings may be modified (or closed) in the future, at the sole discretion of the Department.

Committee approvals and conditions which are at variance with Department rules or standards are not binding in the permitting process for more than **12 months**.

Please contact the District Traffic Operations Access Manager - Tel. # 954-777-4363 or e-mail: D4AccessManagement@dot.state.fl.us with any questions regarding the Pre-Approval Letter.

For right-of-way dedication requirements go to: <https://osp.fdot.gov>. Click on Statewide Permit News. Scroll down to District 4. Scroll down to Additional Information and Examples and choose Right-of-way Donations/Dedications.

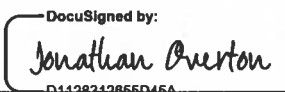
cc: Patricia Moore

File S:\Transportation Operations\Traffic Operations\Access Management\1. Pre-Apps and Variance\2025-09-04 & AMRC\AMRC 03 93210000 MP 21.20 SR 7 K-Park\93210000 MP 21.20 SR 7 K-Park-Rev2.docx

THE DISTRICT ACCESS MANAGEMENT REVIEW COMMITTEE

With the above ruling I

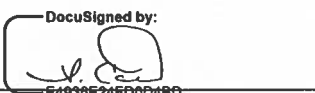
Agree Disagree

Jonathan Overton, P.E. 
District Traffic Operations Engineer

X _____ September 24, 2025

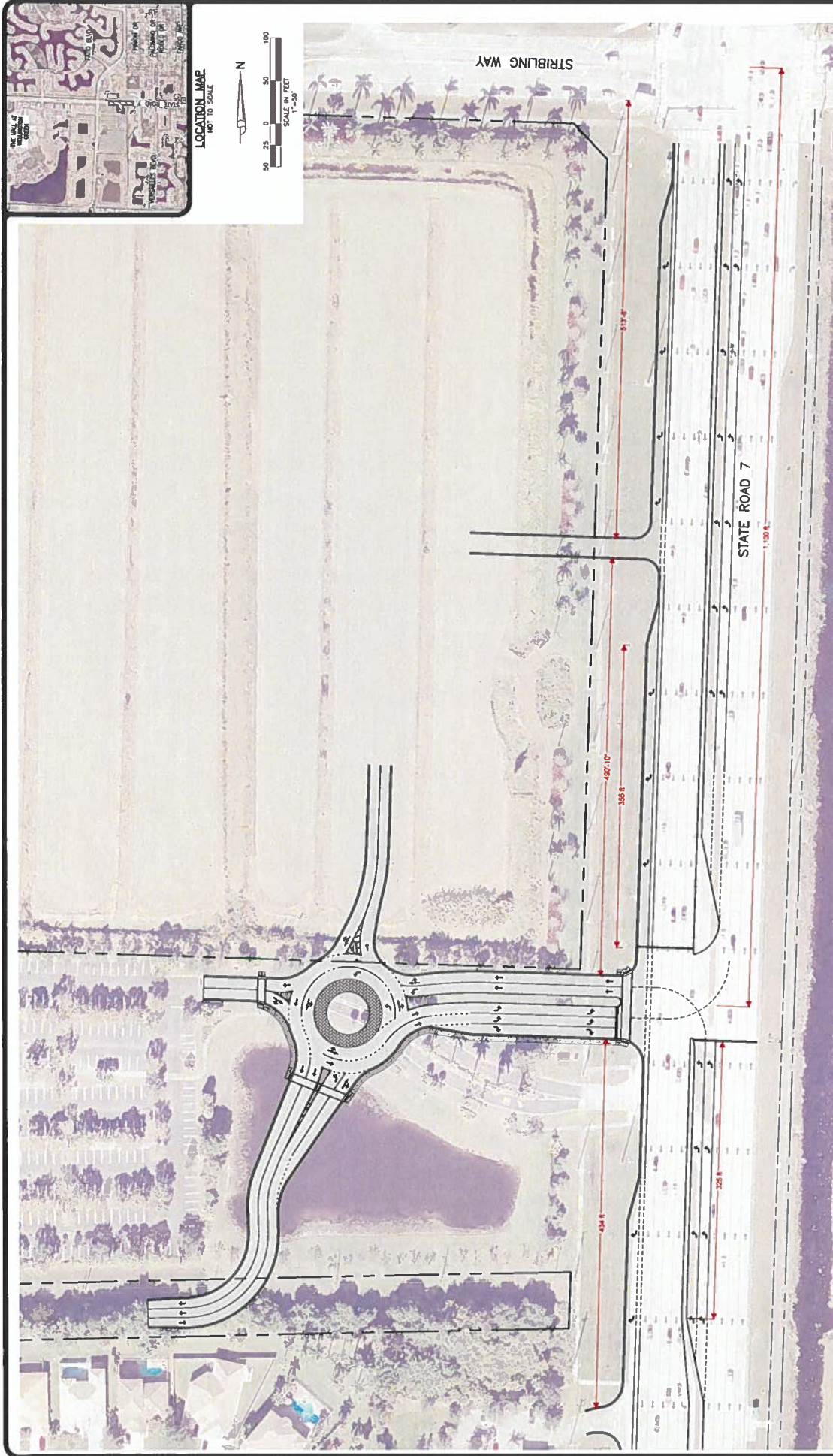
Anson Sonnett, P.E.¹ 
District IV Consultant Project Management Engineer

X _____ September 24, 2025

Antonio Castro, P.E. 
District Maintenance Engineer

X _____ September 24, 2025

¹ Granted signature authority by John Olson, P.E., District Design Engineer



8/7/2025



K PARK
SECTION 24, TOWNSHIP 44S, RANGE 41E.
VILLAGE OF WELLINGTON, FLORIDA
DRIVEWAY ENTRANCE MODIFICATION EXHIBIT

DATE: 8/7/2025
DRAWN BY: 250242025
CHECKED BY: 1
DATE: 1



To: Florida Department of Transportation

RE: Support for Signalized Intersection on SR-7 / 441 at Life.Church Wellington K-Park Intersection

06/4/2025

Dear Roya Edwards, AICP, District Traffic Operations Access Manager,

On behalf of Life.Church, I am writing to express our support for the proposed signalized intersection on SR-7/441 near the entrance to Life.Church Wellington and the planned Related Ross / ElevateEd mixed-use development (K-Park MUPD) in Wellington, Florida.

We believe this signalized intersection, conceptually shown on the following page, is a vital component of our shared efforts to enhance traffic conditions and overall accessibility for our property and the surrounding community. As our church continues to grow and serve our community, the safety and efficiency of access to our location remain a high priority. The planned intersection, along with our continued unrestricted access to Stribling Way, will significantly improve the flow of traffic in and out of our parking lot and benefit the broader community as the adjacent development progresses.

We want to thank you for your continued commitment to improving transportation infrastructure in our region. Projects like this reflect the kind of thoughtful planning and community collaboration that make a lasting difference. We are excited about the positive impact this project will have on traffic flow, safety, and future development.

Should you need any additional information or assistance from us regarding this project or any other transportation-related matters, please do not hesitate to reach out. We are eager to be a helpful partner in this important work.

Sincerely,

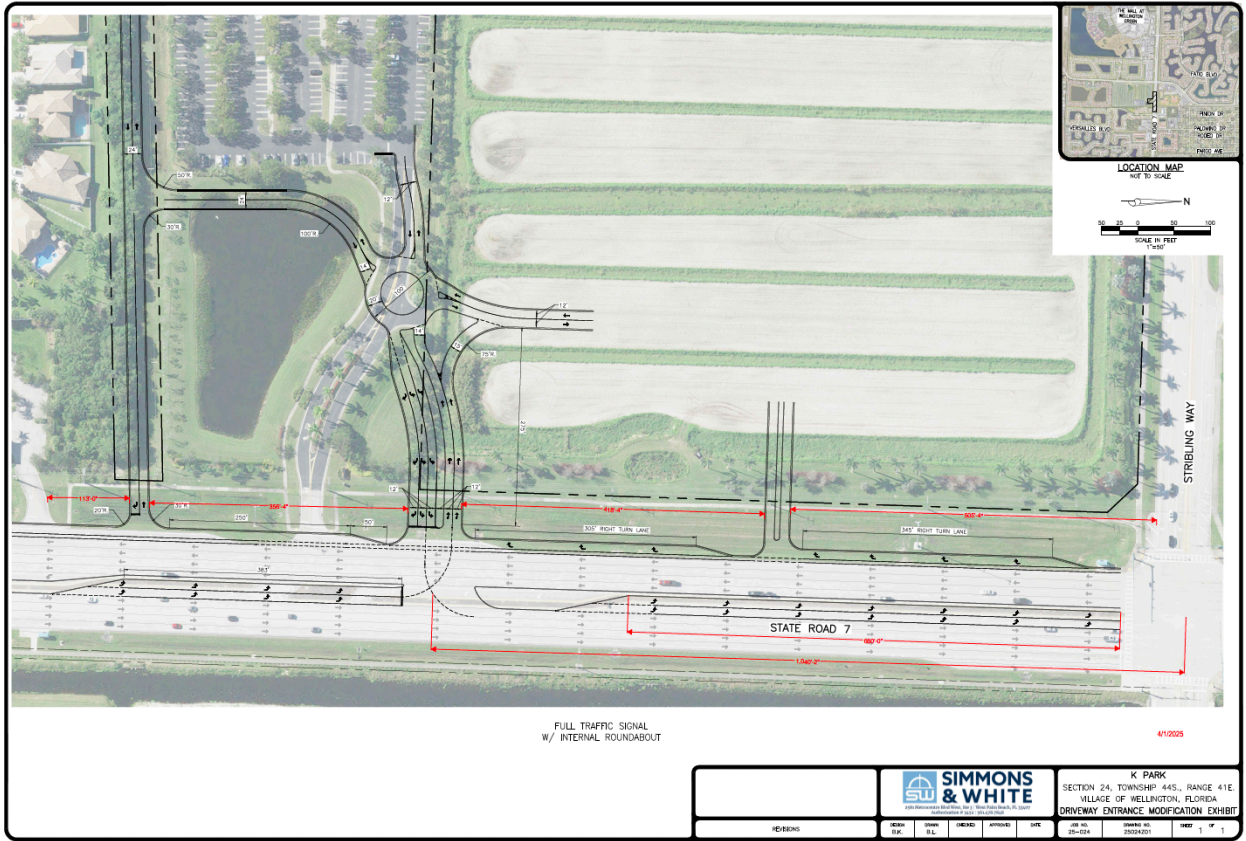
Brett Harrington

Life.Church

Real Estate Development - Team Leader

405.513.4620

brett.harrington@life.church



December 18, 2025

Ms. Kelly Ferraiolo
Wellington
Planning, Zoning & Building Department
12300 W. Forest Hill Boulevard
Wellington, FL 33414

**Re: K Park MP - #PTC24-001Q
2025-0001-MP**

Dear Ms. Ferraiolo:

Pinder Troutman Consulting, Inc. (PTC) has completed our review of the resubmitted Master Plan Application for the above referenced project. The Traffic Impact Statement dated November 14, 2025, Supplemental Traffic Analysis dated December 4, 2025, Shared Parking Study dated November 14, 2025, School Traffic Operational Management Plan dated November 14, 2025, all completed by Simmons & White, were reviewed. The resubmittals addressed our comments. We have no new comments. The project is summarized below:

| | |
|-------------------------|---|
| Proposed Uses: | 215 Multi-Family Mid-Rise Residential Units |
| | 180 Room Hotel |
| | 1,750 Student Private School |
| | 31,396 SF Church/Synagogue |
| | 75,000 SF General Office |
| | 335,000 SF Shopping Center |
| Daily Trips: | 15,291 |
| Peak Hour Trips: | AM: 866 In, 512 Out, 1,378 Total |
| | PM: 596 In, 713 Out, 1,309 Total |

It has been demonstrated that the proposed development meets the Traffic Performance Standards of Wellington. We recommend the following conditions of approval.

1. No building permits are to be issued after December 31, 2030, unless a time extension has been approved.
2. Prior to Village Council, a Palm Beach County TPS approval letter is required.
3. The County traffic concurrency approval is subject to the Project Aggregation Rules as set forth in the Traffic Performance Standards Ordinance.

4. The Property Owner is required to make proportionate share payments as follows. Total costs of improvements shall be prepared by the developers EOR and accepted by the Village Engineer:
 - a. 3.56% of the total cost of the four lane widening of Stribling Way from Forest Hill Boulevard to Fairlane Farms Road prior to the first building permit.
 - b. 2.80% of the total cost of the four lane widening of Stribling Way from Fairlane Farms Road to Castellina Way prior to the first building permit.
 - c. 0.15% of the total cost of the four lane widening of Big Blue Trace from Wellington Trace to South Shore Boulevard prior to the first building permit.
 - d. 0.19% of the total cost of the ten lane widening of Southern Boulevard from SR 7 to Lyons Road prior to the first building permit.
 - e. 47.6% of the total cost to construct an additional westbound left turn lane at the Forest Hill Boulevard and SR 7 intersection prior to the first building permit.
 - f. 40.8% of the total cost to construct an additional northbound lane (one left turn lane and one thru lane) at the Stribling Way and Fairlane Farms Road roundabout prior to the first building permit.
 - g. 0.6% of the total cost to construct an additional eastbound thru lane at the South Shore Boulevard and Forest Hill Boulevard intersection prior to the first building permit.
5. The property owner shall construct the following improvements at their site driveways. Construction shall begin prior to the first building permit and shall be complete prior to the first certificate of occupancy:
 - a. Signalized intersection or roundabout at full access driveway on Stribling Way approximately 2,300 feet west of SR 7 shall be constructed.
 - i. If a signalized intersection is constructed, then an eastbound right turn lane with 160 feet of storage and a 50 foot taper and a westbound left turn lane with 280 feet of storage and a 50 foot taper shall also be constructed or as approved by the Village Engineer. Two eastbound departure lanes with additional pavement for westbound U-turns shall be included in the construction.
 - ii. If a roundabout is constructed, then it should be multi-lane in the east/west direction and single lane in the north/south direction. This roundabout would include two eastbound lanes into the roundabout and two departure eastbound lanes. Two westbound lanes into the roundabout shall be included with westbound departure lanes including one right lane into Castellina and one through lane.
 - b. An eastbound right turn lane with 280 feet of storage and a 50 foot taper and a westbound left turn lane with 280 feet of storage and a 50 foot taper shall be constructed at full access driveway on Stribling Way approximately 1,320 feet west of SR 7 or as approved by Village Engineer.
 - c. An eastbound right turn lane with 280 feet of storage and a 50 foot taper shall be constructed at the right in/right out driveway on Stribling Way approximately 500 feet west of SR 7 or as approved by Village Engineer.

- d. A southbound right turn lane meeting the minimum requirement in the Florida Design Manual (FDM) with provided space for a buffered bike lane shall be constructed at the right in/right out driveway on SR 7 approximately 514 feet south of Stribling Way.
 - e. Signalized intersection at full access driveway on SR 7 approximately 1,029 feet south of Stribling Way shall be constructed. A southbound right turn lane meeting the minimum requirement in the Florida Design Manual (FDM) with provided space for a buffered bike lane as well as dual northbound left turn lanes, dual eastbound left turns lanes and one eastbound right turn lane shall be constructed. The queue length for the northbound dual left turn lanes must be determined by a traffic study conducted by the applicant at permit. The proposed traffic signal support system shall have the structural capacity to implement a minimum of one signal head per lane with five section heads for all approaches, as needed.
 - f. The proposed multilane roundabout shall follow nationwide design guidelines (FDOT FDM Chapter 213). Queueing from the multilane roundabout's eastern approach must not encroach onto SR 7, as determined by a traffic study conducted by the applicant at permit.
 - g. Close the existing right-in/right-out/left-in driveway on the west side of SR 7, located approximately 1,153 feet south of Stribling Way and provide cross-access with existing church.
 - h. Remove the existing directional median opening on SR 7, located approximately 1,029 feet south of Stribling Way.
6. The following improvements shall be constructed at the intersection of SR 7 and Stribling Way including any signal modifications. Storage lengths to be determined during final design. The queue length for the northbound dual left turn lanes must be determined by a traffic study conducted by the applicant at permit. Construction shall begin prior to the first building permit and shall be complete prior to the first certificate of occupancy
 - a. An additional northbound left turn lane.
 - b. An additional eastbound left turn lane
 - c. An additional eastbound right turn lane
 - d. Extend the southbound right turn lane to meet minimum length as determined by traffic study conducted by the applicant at permit.
7. The property owner shall complete the construction of Stribling Way from Castellina Way to SR 7 to be widened to four lanes. Construction shall begin prior to the first building permit and shall be complete prior to the first certificate of occupancy
8. No building permits shall be issued for the project until the property owner provides acceptable surety to the Village of Wellington in an amount as prepared by the developers EOR and accepted by the Village Engineer for the modifications described in Conditions 6 and 7 above. The surety shall be released upon completion of the modifications.

9. No building permits shall be issued for the project until the property owner provides acceptable surety to the Village of Wellington in an amount as prepared by the developers EOR and accepted by the Village Engineer for the two signals and/or roundabout as described in Conditions 5a and 5f above. The surety shall be released upon completion of the intersection improvements.
10. An annual parking utilization monitoring study shall be conducted for the site and shall be based on a minimum of one week during peak season. This monitoring study shall begin one year after issuance of the certificate of occupancies for more than 150,000 square feet, including residential. The Village shall be provided with the study within 30 days of the monitoring. The annual monitoring study shall be required for a period of 10 years. If a parking deficiency is identified, the Village will notify the property owner within 30 days of the finding and direct the construction of the contingency parking. The property owner shall be required to design, fund, and build the contingency parking within 18 months of the notice. Any appeal of this condition shall be made to the Village Council.
11. Staggered start and stop times for elementary, middle, and high school students shall be staggered a minimum of 30 minutes apart.
12. School circulation shall be per operational plan dated November 14, 2025, including required loading/unloading personnel, traffic control personnel and crossing guards, unless modifications are warranted and approved by the Village Engineer.

Please contact me by phone or at atroutman@pindertroutman.com if you need any additional information or have any questions.

Sincerely,



Andrea M. Troutman, P.E.
President

Enclosures



K-PARK MUPD

Wellington, Florida

SHARED PARKING STUDY

PREPARED FOR:

Related Ross
360 South Rosemary Avenue
Suite 800
West Palm Beach, FL 33401

JOB NO. 25-024

DATE: 05/14/2025
Revised: 08/15/2025
Revised: 09/26/2025
Revised: 11/14/2025

Bryan G. Kelley, Professional Engineer, State of Florida, License No. 74006

This item has been digitally signed and sealed by Bryan G. Kelley, P.E., on 11/14/2025.

Printed Copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

Digitally signed by
Bryan Kelley
Date: 2025.11.14
14:40:37-05'00'

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- 1.0 SITE DATA
- 2.0 PURPOSE OF STUDY
- 3.0 PARKING DATA

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- 4.0 PARKING CONTIGENCY

PAGE 5

- 5.0 CONCLUSION

1.0 SITE DATA

The subject parcel is located in the southwest corner of Stribling Way at State Road 7 and consists of approximately +/- 71 acres in the Village of Wellington, Florida. The proposed mixed-use development plan is to consist of the following land uses and intensities:

- 215 Multi-Family Residential DU (15 DU associated with Phase 1)
- 180 Room Hotel
- 1,750 Student K-12 Private School
- 75,000 SF Office
- 335,000 SF Shopping Plaza (Including +/- 105,000 SF of Restaurant and up to 20,000 SF of outdoor seating area)

The above intensity shown is the proposed gross building square footage. Site access is proposed via multiple driveway connections to both Stribling Way and State Road 7. For additional information on site layout, please refer to the Master Plan prepared by Urban Design Studio.

2.0 PURPOSE OF STUDY

The purpose of this study is to describe the results of the parking analyses for the proposed development project. Providing the appropriate level and location of parking is important to any successful development and community. While insufficient parking can result in negative impacts on properties, providing too much parking can also have a negative economic impact. The private school will be parked separately and therefore not included in these shared parking calculations. For this parking study, only the Phase 1 development is considered. Parking for Phase 2 which will consist of an additional 200 residential dwelling units for a total of 215 dwelling units will be considered at a later time.

3.0 PARKING DATA

Per the Village Code, both the retail and office parking rates are based on net square feet and not gross square feet. The shopping plaza is proposed to have 290,000 net square feet and the office is proposed to have 70,000 net square feet. Up to an additional 20,000 SF of outdoor seating area is also proposed. Therefore, a total of 380,000 net square feet is proposed for the retail and office uses (290,000 net SF retail, 70,000 net SF office, and 20,000 SF of outdoor seating area). Village Code allows for parking to be calculated at 1 space per 500 SF for all area above 80,000 SF within a MUPD.

The proposed uses, sizes and code required parking for the approved development may be summarized as follows:

| <u>USE</u> | <u>SIZE</u> | <u>REQUIRED PARKING</u> |
|---------------|--|-------------------------|
| Retail/Office | 80,000 Net S.F. (1 Space / 250 SF) | 320 Spaces |
| Retail/Office | 300,000 S.F. (1 Space / 500 SF) | 600 Spaces |
| Hotel | 180 Rooms (1.25 Spaces / Room + 1 per employee) | 250 Spaces |
| Residential | 15 DU (2.25 Spaces / DU) | 34 Spaces |

TOTAL = 1,204 Spaces

In order to more accurately estimate parking demand for the mixed-use project, the latest *ITE Parking Generation, 6th Edition* rates were utilized for the shared parking analysis. ITE Land Use #820 (Shopping Plaza) was utilized for both retail and restaurant since it is common practice for shopping centers of this size to contain a mixture of retail and restaurant uses. This is verified by the ITE Land Use #820 description of use. The principle behind shared parking reductions is that the pattern of activity for land uses in a mixed-use project are sufficiently different so that the corresponding required parking demands of each activity would not occur simultaneously. The parking analysis was based on the percent utilization rates from the Urban Land Institute's (ULI) *Shared Parking, 3rd Edition* publication as well as the ratio of customer parking to employee parking outlined in the ULI *Shared Parking, 3rd Edition* publication. As previously mentioned, there are a mix of uses currently proposed for the site. These uses have different peak hours of demand in addition to different hours of operation. As shown on the attached table, this report prepared a shared parking analysis for both the weekday and weekend demand from 6:00 A.M. to 11:00 P.M. The ULI *Shared Parking, 3rd Edition* publication allows for parking reductions based on the following factors:

- Time of Day
- Monthly
- Non-Captive
- Mode Adjustment

The time-of-day adjustment factors were taken directly from the ULI *Shared Parking, 3rd Edition* publication. However, the ULI *Shared Parking, 3rd Edition* publication does not produce standard rates for non-captive and mode adjustment factors. Local and site-specific factors as well as professional judgment are to be utilized to determine these adjustment factors.

Captive patrons refer to people who are already present in the immediate vicinity and likely patrons of a second use. The proposed plan of development is an ideal scenario to utilize non-captive rates due to the mixture of land uses and the design principles applied to the site. To be conservative, the non-captive adjustment was not used.

The results of the analysis demonstrated that the proposed plan of development will have a maximum parking demand of 719 parking spaces during the weekday, 867 parking spaces on Saturday, and 680 parking spaces on Sunday. The peak parking period occurs between 2:00 to 3:00 P.M. during the weekday and between 3:00 to 4:00 P.M. during the weekend. It should be noted that during the majority of the day, the parking demand will be less than the peak parking demand of 867 parking spaces. The shared parking calculations based on the ITE *Parking Generation* publication and the Urban Land Institute's *Shared Parking* publication and consistent with standard industry practices are attached to this report. To account for parking turnover and to be conservative, a 10% buffer can be added to the peak demand of 867 parking spaces for a total of 954 spaces.

As shown on the Site Plan, a total of 776 spaces are proposed not including the Life Church, the School, or additional spaces created with valet parking. The developer plans to utilize valet parking and cone off certain areas within the parking lots to allow for double stacked vehicles to create additional parking. Additionally, the developer is entering into a shared parking agreement with the church to obtain 147 additional spaces. With the additional valet spaces and shared Life Church parking, a total of 1023 spaces can be accommodated onsite. The development parking plan including the valet zones and shared parking is attached to this report.

4.0 CONTINGENCY PARKING

Contingency Plan 1: Shared Parking with Wingrove Academy

The first option is to establish an additional shared parking agreement with Wingrove Academy. In the school's initial phase, the adjacent baseball fields will not yet be constructed, freeing up additional space. Through an agreement with Wingrove Academy, an extra 181 parking spaces would be available. Because peak activity periods for the mixed-use site differ from those of the school, this shared approach is feasible. With these agreements in place, the total parking supply would increase to 1,204 spaces.

Contingency Plan 2: Parking Garage Construction

The second option is to construct a parking garage on either the north or south lot of the mixed-use site. The parking garage would provide at least 1,204 parking spaces, with potential for additional capacity through valet zones, construction of garages on both lots, and/or further shared parking with Life Church and Wingrove Academy. Under this scenario, parking supply could significantly exceed 1,204 spaces if needed.

Both contingency plans are included as attachments to this report for reference.

5.0 CONCLUSION

As demonstrated in this study, the anticipated maximum parking demand for the proposed plan of development is 867 parking spaces without the 10% buffer and 954 parking spaces with the 10% buffer. The proposed Site Plan will accommodate 1,023 parking spaces for the mixed-use site inclusive of dedicated valet zones and a shared parking agreement with Life Church. Parking contingency plans allow for additional parking from a shared parking agreement with Wingrove Academy along with the construction of a parking garage on the north or south lots to allow for a minimum of 1,204 spaces. The parking analysis considered parking rates from the *ITE Parking Generation, 6th Edition* and the Urban Land Institute's (ULI) *Shared Parking, 3rd Edition* publication.

bk: x:/docs/trafficdrainage/sps.25024.rev3

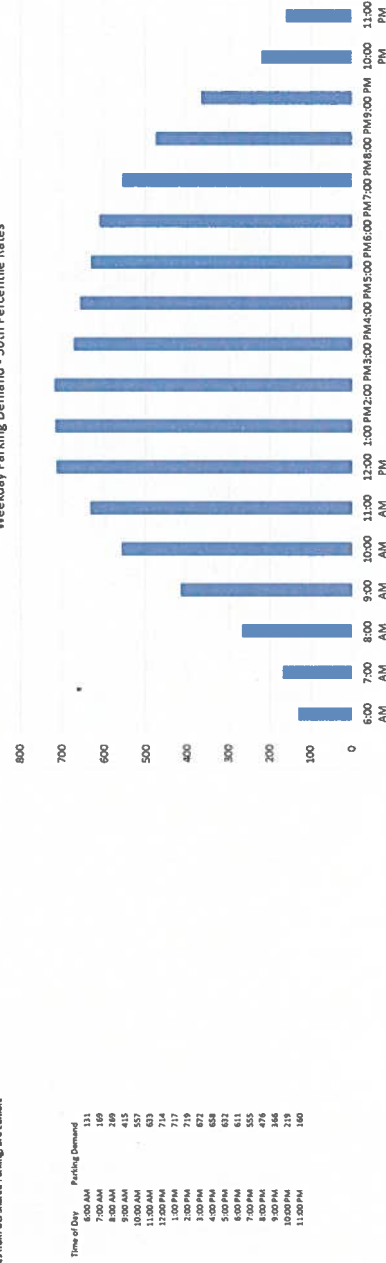
SHARED PARKING ANALYSIS - WEEKDAY
BASED ON ITE 50TH PERCENTILE

| Use | ITE 50th Rate (Per 1,000 SF or Units) | Parking Required |
|--------------|---------------------------------------|------------------|
| Apartment | 15 | 127 |
| Hotel | 120 | 115 |
| Retail | 3,100,000 | 518 |
| Office | 70,000 | 137 |
| TOTAL | | 789 |

| Shared Parking Demand - WEEKDAY | | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------------------|------------------|------|------------|----------------|-----------------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|
| Land Use | Type | Rate | Percentage | Parking Demand | Mode Adjustment | 6:00 AM | | 7:00 AM | | 8:00 AM | | 9:00 AM | | 10:00 AM | | 11:00 AM | | 12:00 PM | | 1:00 PM | | 2:00 PM | | |
| | | | | | | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking |
| Apartments | Unitary Resident | 0.1 | 2.1% | 1 | 100% | 100% | 0% | 0 | 10% | 0 | 10% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | |
| | Residential | 1.3 | 93.9% | 17 | 100% | 100% | 95% | 14 | 87% | 11 | 55% | 9 | 50% | 9 | 50% | 8 | 40% | 8 | 40% | 7 | 40% | | | |
| | Customer | 1 | 87.2% | 100 | 100% | 100% | 95% | 95 | 90% | 90 | 80% | 70% | 60% | 70% | 65% | 65 | 60% | 65 | 60% | 65 | 60% | | | |
| | Employee | 0.15 | 10.9% | 15 | 100% | 100% | 10% | 5 | 21 | 15% | 43 | 35% | 44 | 40% | 250 | 250 | 313 | 313 | 100% | 417 | 417 | | | |
| | Other | 2.9 | 82.5% | 417 | 100% | 100% | 15% | 4 | 15% | 83 | 35% | 144 | 60% | 250 | 250 | 313 | 313 | 100% | 417 | 417 | 417 | 417 | | |
| Retail | Unitary Retail | 0.3 | 19.4% | 101 | 100% | 100% | 100% | 10 | 13% | 15 | 25% | 25 | 45% | 45 | 70% | 74 | 95% | 94 | 100% | 121 | 120% | 121 | 100% | |
| | Customer | 0.7 | 27.8% | 111 | 100% | 100% | 95% | 0 | 10% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | |
| | Employee | 0.3 | 11.8% | 11 | 100% | 100% | 10% | 0 | 10% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | |
| | Other | 1.5 | 100% | 248 | 100% | 100% | 100% | 383 | 100% | 389 | 100% | 538 | 100% | 532 | 100% | 532 | 100% | 748 | 100% | 748 | 100% | 748 | 100% | |
| | TOTAL | | | | | | | 383 | 100% | 389 | 100% | 538 | 100% | 532 | 100% | 532 | 100% | 748 | 100% | 748 | 100% | 748 | 100% | |
| | | | | | | | | | | | | | | | | | | | | | | | | |
| Land Use | Type | Rate | Percentage | Parking Demand | Mode Adjustment | 3:00 PM | | 4:00 PM | | 5:00 PM | | 6:00 PM | | 7:00 PM | | 8:00 PM | | 9:00 PM | | 10:00 PM | | 11:00 PM | | |
| | | | | | | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking |
| Residential | Unitary Resident | 0.1 | 2.1% | 1 | 100% | 100% | 0% | 0 | 10% | 0 | 10% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | 0 | 20% | |
| | Residential | 1.3 | 93.9% | 17 | 100% | 100% | 95% | 14 | 80% | 9 | 50% | 9 | 50% | 10 | 70% | 13 | 100% | 13 | 100% | 13 | 100% | 13 | 100% | |
| | Customer | 1 | 87.2% | 100 | 100% | 100% | 95% | 70 | 45% | 75 | 60% | 60 | 45% | 80 | 60% | 80 | 95% | 95 | 95% | 95 | 95% | 95 | 95% | |
| | Employee | 0.15 | 10.9% | 15 | 100% | 100% | 10% | 5 | 11% | 11 | 100% | 11 | 40% | 8 | 20% | 3 | 20% | 3 | 20% | 3 | 20% | 3 | 20% | |
| | Other | 2.9 | 82.5% | 417 | 100% | 100% | 15% | 4 | 15% | 83 | 35% | 144 | 60% | 250 | 250 | 313 | 313 | 100% | 417 | 417 | 417 | 417 | | |
| Retail | Unitary Retail | 0.3 | 19.4% | 101 | 100% | 100% | 100% | 10 | 13% | 15 | 25% | 25 | 45% | 25 | 45% | 25 | 45% | 25 | 45% | 25 | 45% | 25 | 45% | |
| | Customer | 0.7 | 27.8% | 111 | 100% | 100% | 95% | 0 | 10% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | |
| | Employee | 0.3 | 11.8% | 11 | 100% | 100% | 10% | 0 | 10% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | 13 | 15% | |
| | Other | 1.5 | 100% | 248 | 100% | 100% | 100% | 383 | 100% | 389 | 100% | 538 | 100% | 532 | 100% | 532 | 100% | 748 | 100% | 748 | 100% | 748 | 100% | |
| | TOTAL | | | | | | | 383 | 100% | 389 | 100% | 538 | 100% | 532 | 100% | 532 | 100% | 748 | 100% | 748 | 100% | 748 | 100% | |

WEEKDAY MAX 719
10% Buffer 72
TOTAL PARKING NEEDED 791

Notes: Time of day percentages from ULI Shared Parking, 3rd Edition.



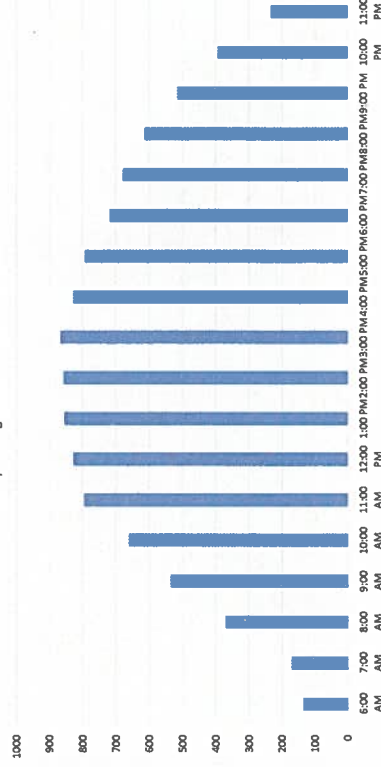
SHARED PARKING ANALYSIS - SATURDAY
BASED ON ITE 50TH PERCENTILE

| Use | ITE 50th Rate (Per 1,000 SF of Floor Area) | Parking Required |
|--------------|--|------------------|
| Apartment | 15 | 18 |
| Hotel | 150 | 117 |
| Retail | 310.000 | 753 |
| Office | 70.000 | 14 |
| TOTAL | | 982 |

| Shared Parking Demand - SATURDAY | | | | | | | | | | | | | | | | | | | | | | | | |
|----------------------------------|-------------|------|------------|----------------|-------------|-----------------|------------------|-----|------------------|-----|------------------|-----|------------------|-----|------------------|-----|------------------|-----|------------------|-----|------------------|-----|------------------|-----|
| Land Use | Type | Rate | Percentage | Parking Demand | Non-Captive | Mode Adjustment | 6:00 AM | | 7:00 AM | | 8:00 AM | | 9:00 AM | | 10:00 AM | | 11:00 AM | | 12:00 PM | | 1:00 PM | | 2:00 PM | |
| | | | | | | | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % |
| Apartments | Whaler | 0.15 | 10.3% | 2 | 100% | 100% | 0% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Residential | 3.2 | 89.7% | 18 | 100% | 100% | 15 | 15 | 15 | 83% | 14 | 82% | 13 | 73% | 12 | 70% | 11 | 68% | 10 | 65% | 10 | 65% | 10 | 65% |
| | Customer | 1 | 87.0% | 102 | 100% | 95% | 97 | 93% | 92 | 89% | 82 | 79% | 82 | 79% | 71 | 70% | 64 | 63% | 64 | 70% | 64 | 70% | 71 | 70% |
| | Hotel | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Retail | Whaler | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Residential | 3.2 | 89.7% | 18 | 100% | 100% | 15 | 15 | 15 | 83% | 14 | 82% | 13 | 73% | 12 | 70% | 11 | 68% | 10 | 65% | 10 | 65% | 10 | 65% |
| | Customer | 1 | 87.0% | 102 | 100% | 95% | 97 | 93% | 92 | 89% | 82 | 79% | 82 | 79% | 71 | 70% | 64 | 63% | 64 | 70% | 64 | 70% | 71 | 70% |
| | Hotel | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Office | Whaler | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Residential | 3.2 | 89.7% | 18 | 100% | 100% | 15 | 15 | 15 | 83% | 14 | 82% | 13 | 73% | 12 | 70% | 11 | 68% | 10 | 65% | 10 | 65% | 10 | 65% |
| | Customer | 1 | 87.0% | 102 | 100% | 95% | 97 | 93% | 92 | 89% | 82 | 79% | 82 | 79% | 71 | 70% | 64 | 63% | 64 | 70% | 64 | 70% | 71 | 70% |
| | Hotel | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| TOTAL | Whaler | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Residential | 3.2 | 89.7% | 18 | 100% | 100% | 15 | 15 | 15 | 83% | 14 | 82% | 13 | 73% | 12 | 70% | 11 | 68% | 10 | 65% | 10 | 65% | 10 | 65% |
| | Customer | 1 | 87.0% | 102 | 100% | 95% | 97 | 93% | 92 | 89% | 82 | 79% | 82 | 79% | 71 | 70% | 64 | 63% | 64 | 70% | 64 | 70% | 71 | 70% |
| | Hotel | 0.15 | 10.3% | 2 | 100% | 100% | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |

WEEKEND MAX 867
10% Buffer 87
TOTAL PARKING NEEDED 954

Notes: Time of day percentages from ULI Shared Parking, 3rd Edition.



SHARED PARKING ANALYSIS - SUNDAY
BASED ON ITE 50TH PERCENTILE

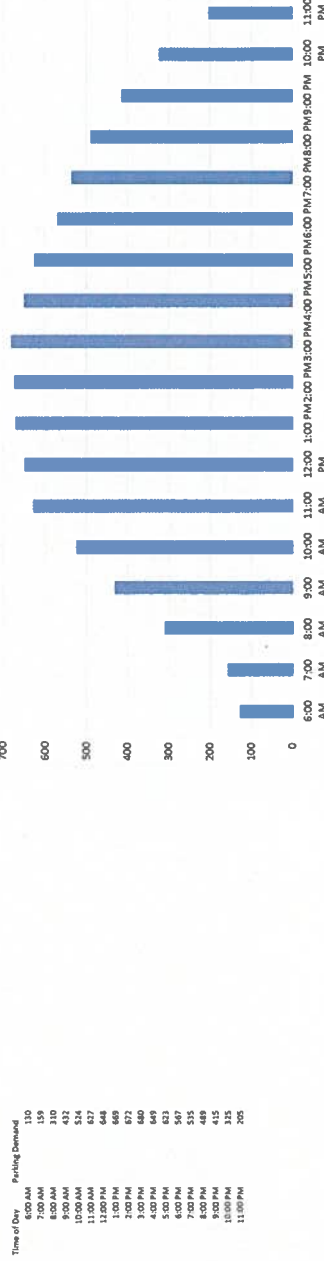
| Use | Intensity (SF per 1,000 SF or Rooms) | Parking Required |
|--------------|--------------------------------------|------------------|
| Apartment | 15 | 17 |
| Hotel | 180 | 17 |
| Office | 31,000 | 54 |
| Office | 70,000 | 34 |
| TOTAL | | 713 |

Raw Parking Demand

| Shared Parking Demand - SUNDAY | | | | | | | | | | | | | | | | | | | | | | | | |
|--------------------------------|----------|------|------------|----------------|-------------|-----------------|------------------|-----|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|------------------|------|
| Land Use | Type | Rate | Percentage | Parking Demand | Non-Captive | Mode Adjustment | 6:00 AM | | 7:00 AM | | 8:00 AM | | 9:00 AM | | 10:00 AM | | 11:00 AM | | 12:00 PM | | 1:00 PM | | 2:00 PM | |
| | | | | | | | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % |
| Apartment | Visitor | 0.15 | 10.1% | 2 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Resident | 1.3 | 89.9% | 18 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 3 | 87.0% | 103 | 100% | 100% | 97 | 95% | 82 | 79% | 71 | 70% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% |
| | Employee | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Other | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| Retail | Visitor | 3.2 | 40.0% | 454 | 100% | 100% | 5 | 5% | 23 | 30% | 138 | 50% | 277 | 75% | 318 | 85% | 431 | 95% | 431 | 95% | 431 | 95% | 431 | 95% |
| | Resident | 0.8 | 10.0% | 113 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Employee | 0.15 | 91.1% | 13 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Other | 0.15 | 91.1% | 13 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| Office | Visitor | 0.15 | 10.1% | 2 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Resident | 1.3 | 89.9% | 18 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 3 | 87.0% | 103 | 100% | 100% | 97 | 95% | 82 | 79% | 71 | 70% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% |
| | Employee | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Other | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| TOTAL | | | | | | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | |
| Shared Parking Demand - SUNDAY | | | | | | | | | | | | | | | | | | | | | | | | |
| Land Use | Type | Rate | Percentage | Parking Demand | Non-Captive | Mode Adjustment | 3:00 PM | | 4:00 PM | | 5:00 PM | | 6:00 PM | | 7:00 PM | | 8:00 PM | | 9:00 PM | | 10:00 PM | | 11:00 PM | |
| | | | | | | | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % | Adjusted Parking | % |
| Residential | Visitor | 0.15 | 10.1% | 2 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Resident | 1.3 | 89.9% | 18 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 3 | 87.0% | 103 | 100% | 100% | 97 | 95% | 82 | 79% | 71 | 70% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% |
| | Employee | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Other | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| Retail | Visitor | 3.2 | 40.0% | 454 | 100% | 100% | 5 | 5% | 23 | 30% | 138 | 50% | 277 | 75% | 318 | 85% | 431 | 95% | 431 | 95% | 431 | 95% | 431 | 95% |
| | Resident | 0.8 | 10.0% | 113 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Employee | 0.15 | 91.1% | 13 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Other | 0.15 | 91.1% | 13 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| Office | Visitor | 0.15 | 10.1% | 2 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Resident | 1.3 | 89.9% | 18 | 100% | 100% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% | 0 | 0% |
| | Customer | 3 | 87.0% | 103 | 100% | 100% | 97 | 95% | 82 | 79% | 71 | 70% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% | 66 | 65% |
| | Employee | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| | Other | 0.15 | 11.0% | 15 | 100% | 100% | 2 | 20% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% | 15 | 100% |
| TOTAL | | | | | | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | | 358 | |

WEEKEND MAX 680
10% Buffer 68
TOTAL PARKING NEEDED 748

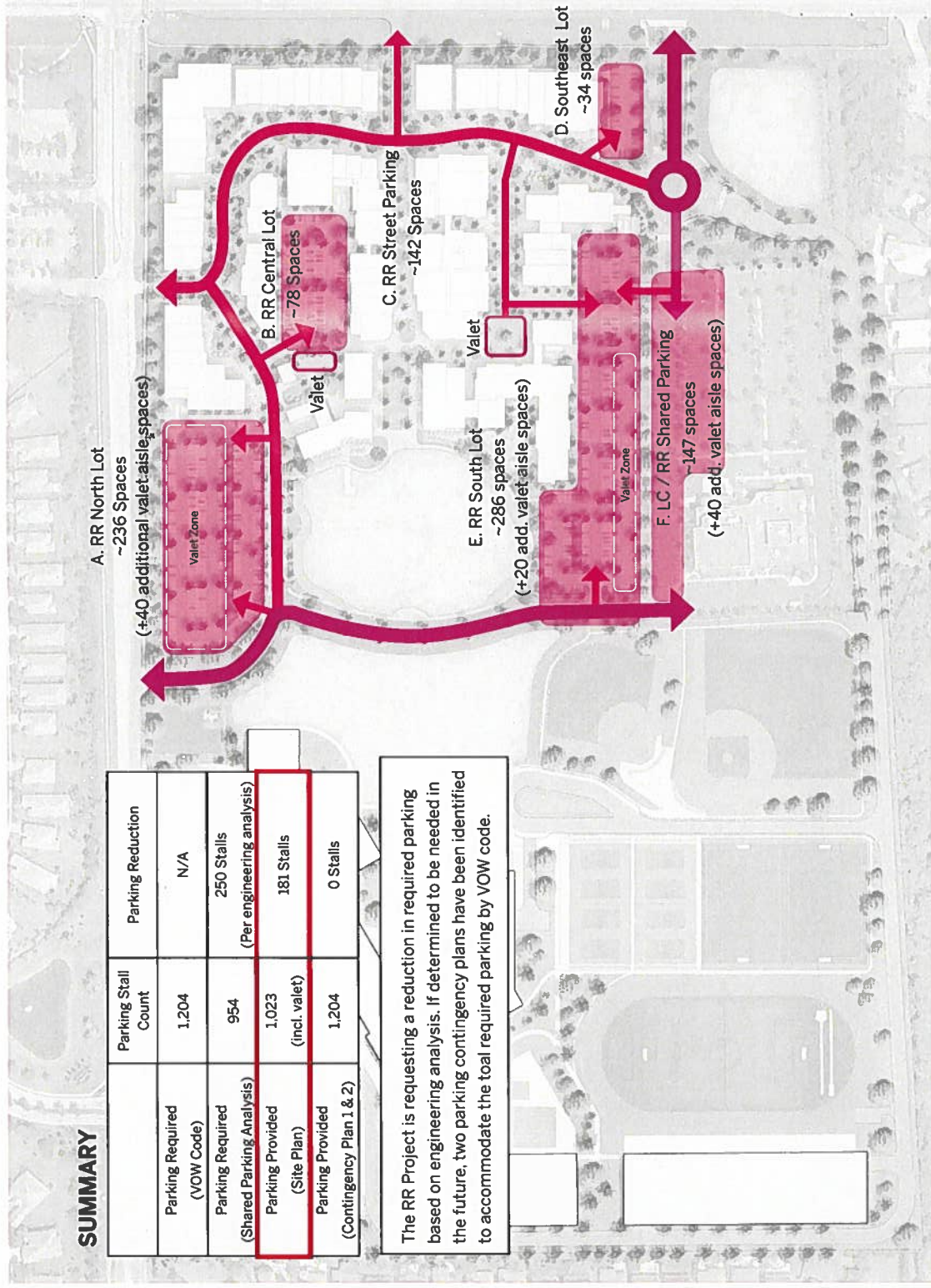
Notes: Time of day percentages from UL Shared Parking, 3rd Edition.



SUMMARY

| Parking Required (VOW Code) | Parking Stall Count | Parking Reduction |
|--|---------------------|---------------------------------------|
| Parking Required (Shared Parking Analysis) | 1,204 | N/A |
| Parking Provided (Site Plan) | 954 | 250 Stalls (Per engineering analysis) |
| Parking Provided (Contingency Plan 1 & 2) | 1,023 (incl. valet) | 181 Stalls |
| | 1,204 | 0 Stalls |

The RR Project is requesting a reduction in required parking based on engineering analysis. If determined to be needed in the future, two parking contingency plans have been identified to accommodate the total required parking by VOW code.



Parking - VOW Code

| Use | Size | Total |
|---------------|---|-------------|
| Retail/Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (1.25 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Shared Parking Analysis

| |
|---------------------------------------|
| 867 Peak Parking Demand |
| 954 Parking Demand (incl. 10% buffer) |

Parking - Site Plan

| Zone | Parking Stalls | Valet | Total |
|--------------|----------------|------------|-------------|
| A | 236 | 40 | 276 |
| B | 78 | | 78 |
| C | 142 | | 142 |
| D | 34 | | 34 |
| E | 286 | 20 | 306 |
| F | 147 | 40 | 187 |
| TOTAL | 923 | 100 | 1023 |

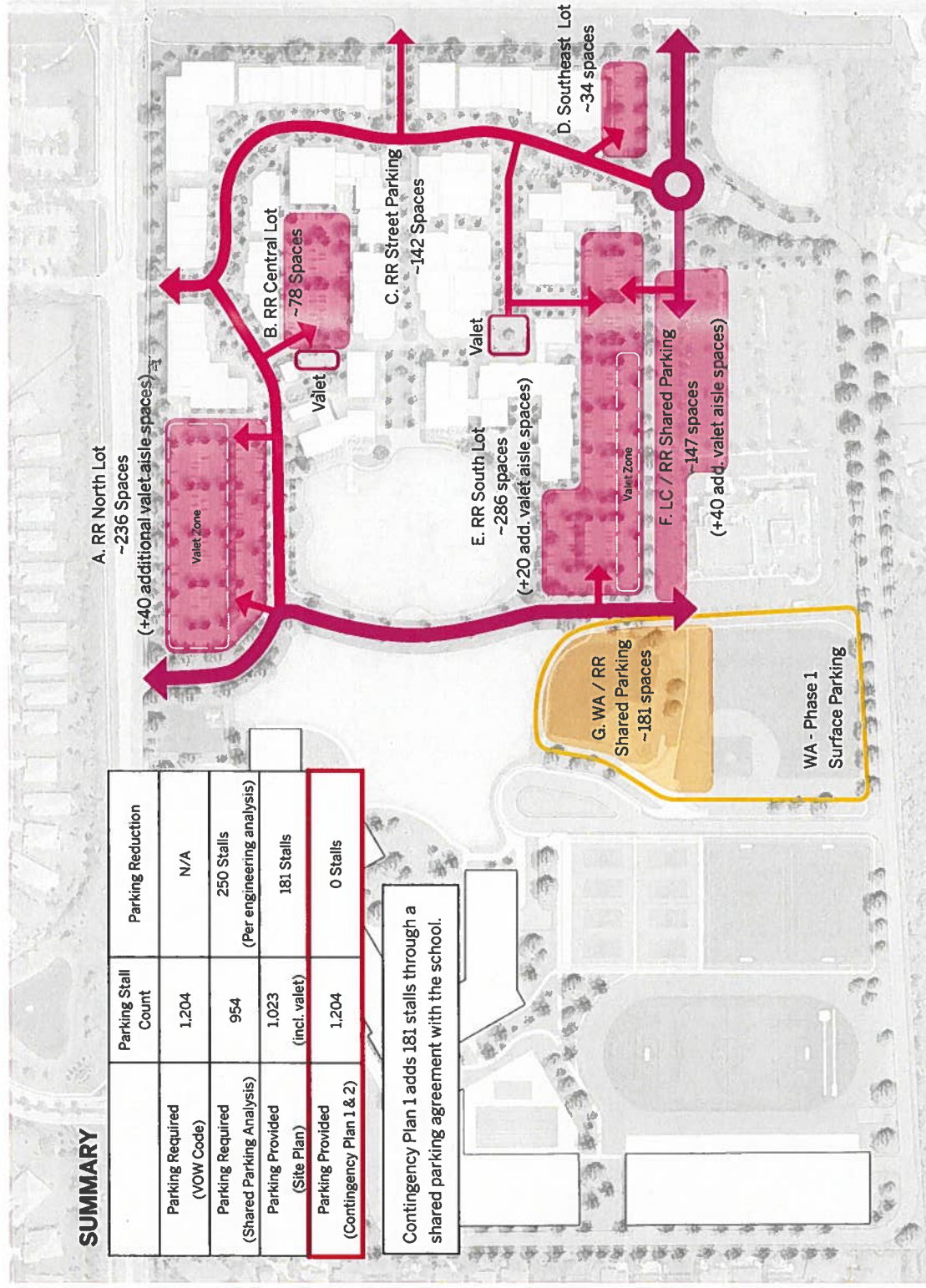
RR Development Parking Plan



SUMMARY

| Parking Required (VOW Code) | Parking Stall Count | Parking Reduction |
|--|---------------------|---------------------------------------|
| Parking Required (Shared Parking Analysis) | 1,204 | N/A |
| Parking Provided (Site Plan) | 954 | 250 Stalls (Per engineering analysis) |
| Parking Provided (Contingency Plan 1 & 2) | 1,023 (incl. valet) | 181 Stalls |
| | 1,204 | 0 Stalls |

Contingency Plan 1 adds 181 stalls through a shared parking agreement with the school.



Parking - VOW Code

| Use | Size | Total |
|---------------|---|-------|
| Retail/Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (1.25 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Contingency Plan 1

| Zone | Parking Stalls | Valet | Shared | Total |
|-------|----------------|-------|--------|-------|
| A | 236 | 40 | | 276 |
| B | 78 | | | 78 |
| C | 142 | | | 142 |
| D | 34 | | | 34 |
| E | 286 | 20 | | 306 |
| F | 147 | 40 | | 187 |
| G | | | 181 | 181 |
| TOTAL | 923 | 100 | 181 | 1204 |

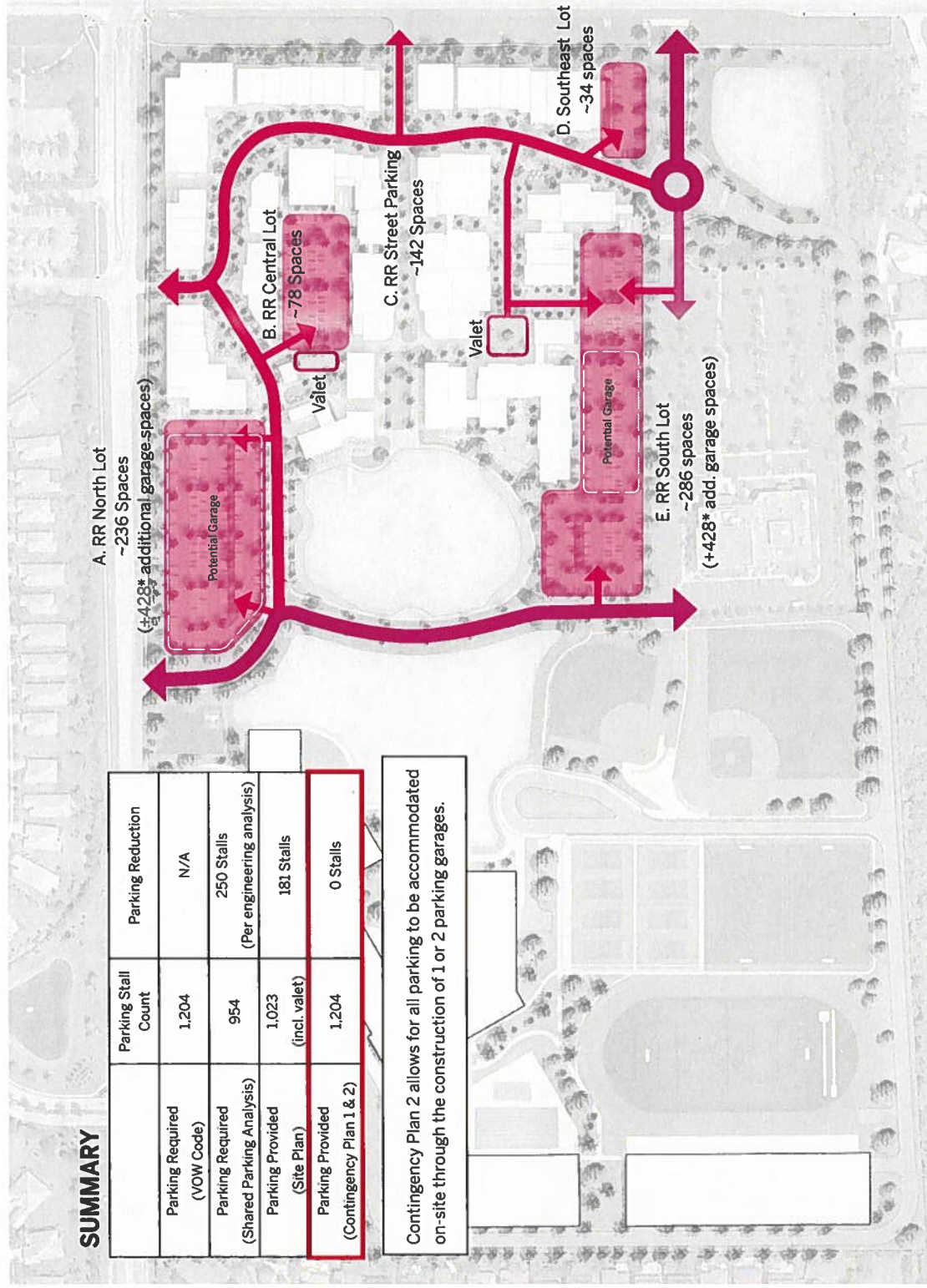
RR Development Parking Contingency Plan 1



SUMMARY

| | Parking Stall Count | Parking Reduction |
|--|---------------------|---------------------------------------|
| Parking Required (VOW Code) | 1,204 | N/A |
| Parking Required (Shared Parking Analysis) | 954 | 250 Stalls (Per engineering analysis) |
| Parking Provided (Site Plan) | 1,023 (incl. valet) | 181 Stalls |
| Parking Provided (Contingency Plan 1 & 2) | 1,204 | 0 Stalls |

Contingency Plan 2 allows for all parking to be accommodated on-site through the construction of 1 or 2 parking garages.



Parking - VOW Code

| Use | Size | Total |
|----------------|--|-------|
| Retail/ Office | 80,000 NSF (1 space / 250 SF) | 320 |
| Retail/ Office | 300,000 NSF (1 space / 500 SF) | 600 |
| Hotel | 180 Rooms (125 spaces / Room + 1 / employee) | 250 |
| Resid. | 15 DU (2.25 spaces / DU) | 34 |
| TOTAL | | 1204 |

Parking - Contingency Plan 2

| Zone | Parking Stalls | Garage / Valet | Total |
|-------|----------------|----------------|--------|
| A | 236 | (428)* | (236)* |
| B | 78 | | 78 |
| C | 142 | | 142 |
| D | 34 | | 34 |
| E | 286 | (428)* | (286)* |
| TOTAL | 776 | 428 | 1204 |

* 428 stalls to be added in one or two garages in Zones A and/or E

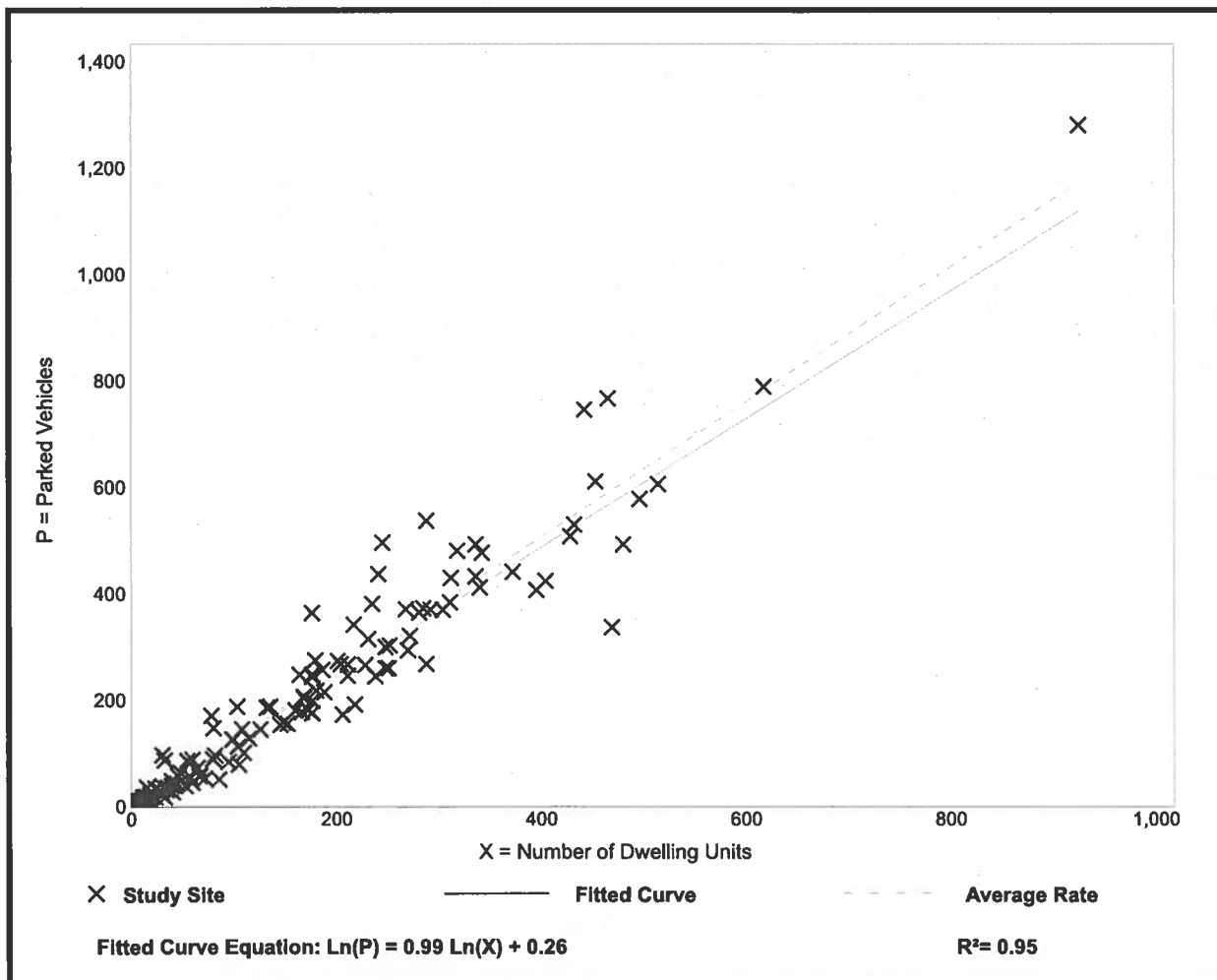
Multifamily Housing - 2+ BR (Low-Rise) - Not Close to Rail Transit (220)

Peak Period Parking Demand vs: Dwelling Units
 On a: Weekday (Monday - Friday)
 Setting/Location: General Urban/Suburban
 Number of Studies: 143
 Avg. Num. of Dwelling Units: 154

Peak Period Parking Demand per Dwelling Unit

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.27 | 0.58 - 3.16 | 1.07 / 1.59 | 1.22 - 1.32 | 0.29 (23%) |

Data Plot and Equation



Multifamily Housing - 2+ BR (Low-Rise) - Not Close to Rail Transit (220)

Peak Period Parking Demand vs: Dwelling Units

On a: Saturday

Setting/Location: General Urban/Suburban

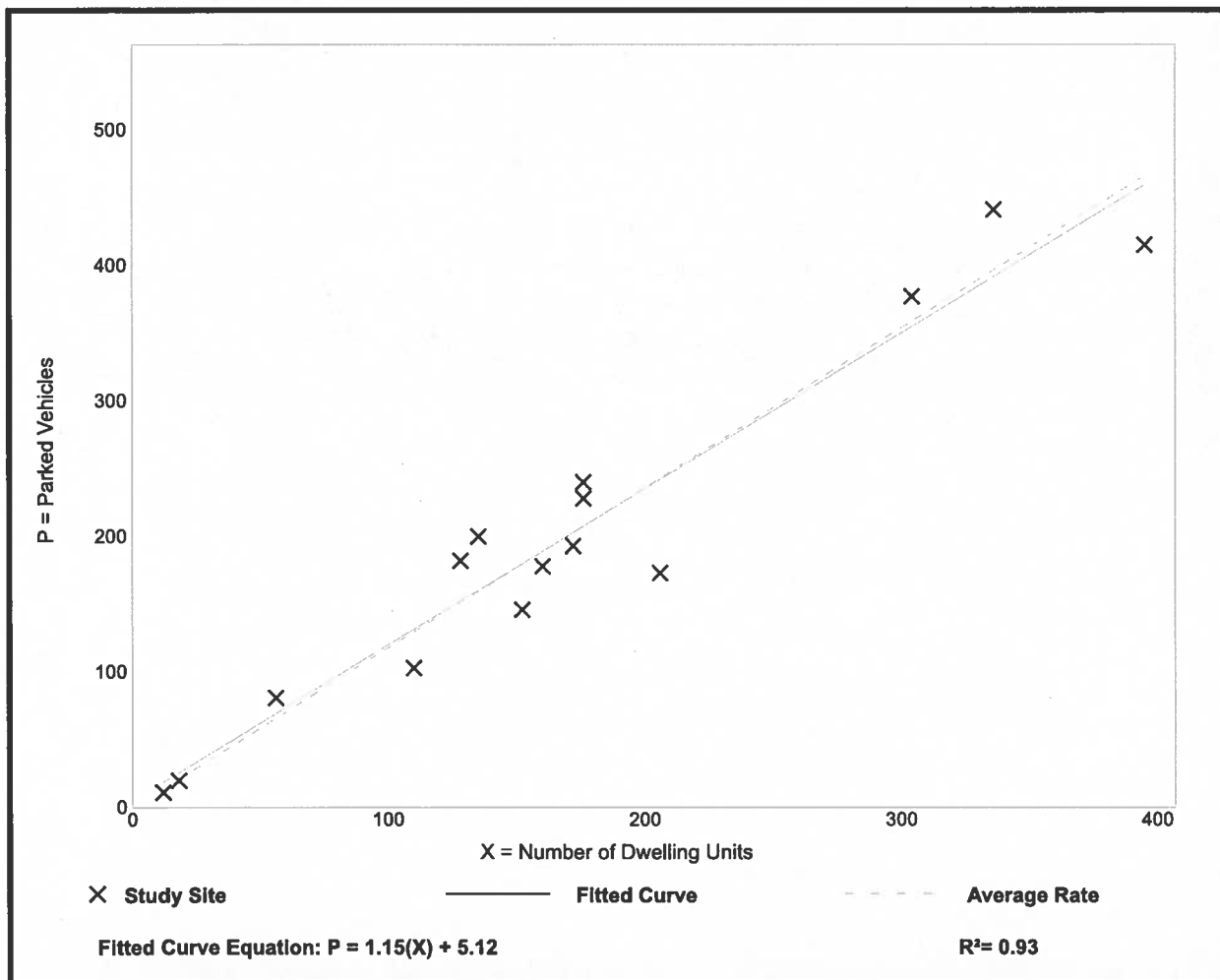
Number of Studies: 15

Avg. Num. of Dwelling Units: 169

Peak Period Parking Demand per Dwelling Unit

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.18 | 0.84 - 1.48 | 1.07 / 1.44 | *** | 0.19 (16%) |

Data Plot and Equation



Multifamily Housing - 2+ BR (Low-Rise) - Not Close to Rail Transit (220)

Peak Period Parking Demand vs: Dwelling Units

On a: Sunday

Setting/Location: General Urban/Suburban

Number of Studies: 3

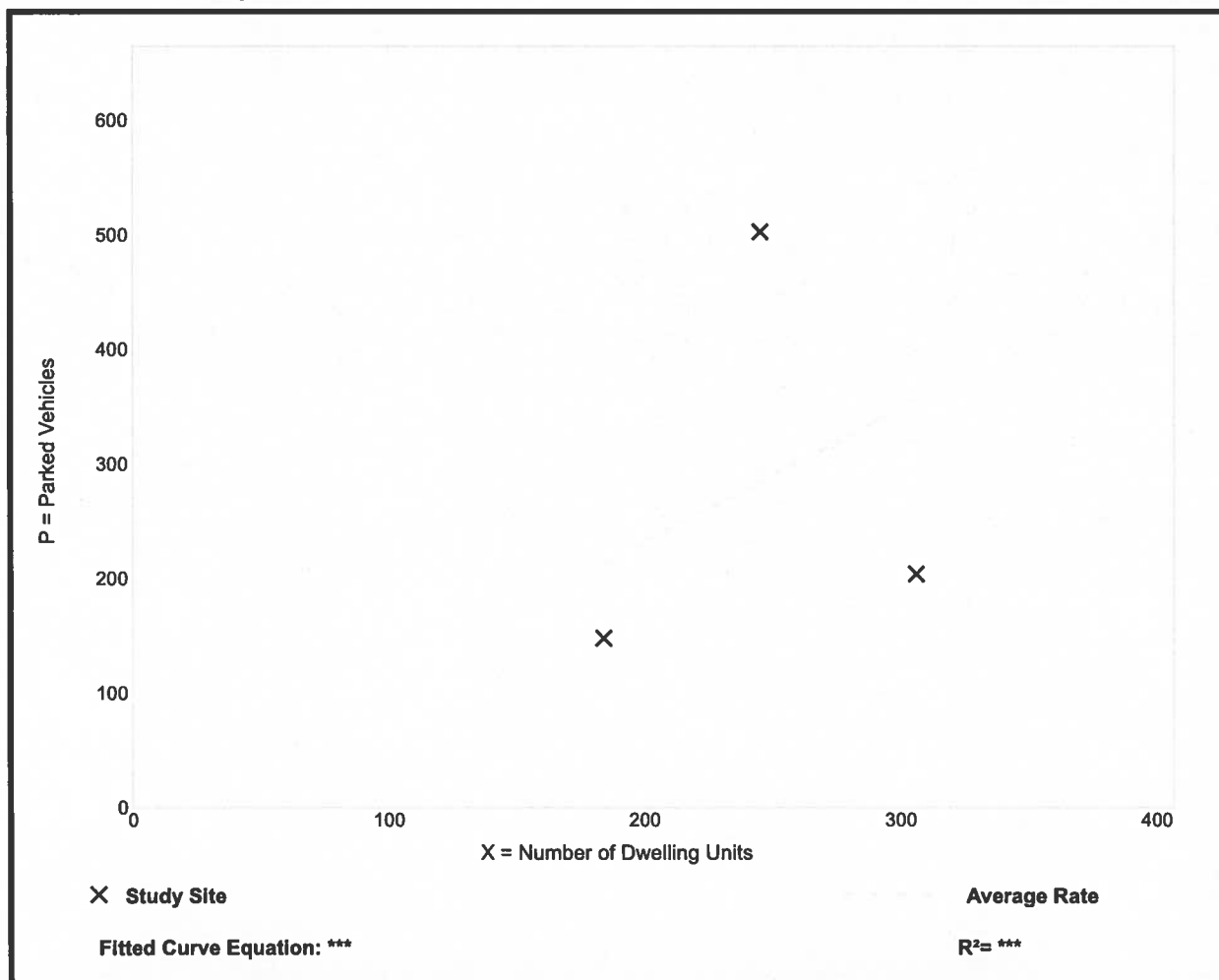
Avg. Num. of Dwelling Units: 245

Peak Period Parking Demand per Dwelling Unit

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.16 | 0.67 - 2.05 | 0.71 / 2.05 | *** | 0.77 (66%) |

Data Plot and Equation

Caution – Small Sample Size



Land Use: 310 Hotel

Description

A hotel is a place of lodging that provides sleeping accommodations and supporting facilities such as a full-service restaurant, concierge service, valet parking, cocktail lounge, meeting rooms, banquet room, and convention facilities. A hotel typically provides a swimming pool or another recreational facility such as a fitness room.

Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday (four study sites) and a Saturday (three study sites) in a general urban/suburban setting.

| Hour Beginning | Percent of Peak Parking Demand | |
|-----------------|--------------------------------|----------|
| | Weekday | Saturday |
| 12:00–4:00 a.m. | 100 | 100 |
| 5:00 a.m. | — | — |
| 6:00 a.m. | 90 | 80 |
| 7:00 a.m. | 90 | 81 |
| 8:00 a.m. | 90 | 80 |
| 9:00 a.m. | 80 | 79 |
| 10:00 a.m. | 74 | 70 |
| 11:00 a.m. | 67 | 65 |
| 12:00 p.m. | 65 | 61 |
| 1:00 p.m. | 61 | 57 |
| 2:00 p.m. | 59 | 48 |
| 3:00 p.m. | 58 | 50 |
| 4:00 p.m. | 61 | 55 |
| 5:00 p.m. | 60 | 59 |
| 6:00 p.m. | 62 | 66 |
| 7:00 p.m. | 65 | 76 |
| 8:00 p.m. | 70 | 76 |
| 9:00 p.m. | 75 | 78 |
| 10:00 p.m. | 88 | 87 |
| 11:00 p.m. | 97 | 95 |

Additional Data

Parking demand at a hotel is related to the presence of supporting facilities. A hotel with a convention facility, meeting rooms, restaurant, or banquet space, may include parking demand generated by event attendees who are not hotel guests. As a result, peak parking demand for the hotel may occur during the afternoon or evening instead of the typical hotel peak parking demand that occurs overnight. To illustrate, for four of the 12 study sites in the database, the time-of-day parking demand distribution clearly demonstrates that an event took place during the evening of the study period. The peak parking demand at these four sites ranged between 50 and 100 percent greater than that of the peak overnight parking demand for hotel guests.

The database for this land use does not include information on potential independent variables for the generation of event-related parking demand (such as meeting facility GFA or event attendees) during the count period. For that reason, the peak parking demand displayed in the data plots only includes a site's overnight peak parking demand (i.e., parking demand associated with overnight guests and hotel staff). The plots do not contain peak parking demands generated by other supporting facilities or events that occur outside of the typical overnight peak parking period.

Parking demand at all lodging land uses may be related to the recent emergence of transportation network companies (TNCs) (also referred to as ride-share or ride-hailing companies). Hotel parking demand may be reduced if a hotel guest uses a TNC service for hotel drop-off or pick-up rather than using a rental car and parking on-site. Additional data are needed in order to measure and understand the potential impact of TNCs on lodging land use parking demand.

The average parking supply ratio for the nine study sites with parking supply information is 1.1 spaces per room. The average peak parking occupancy at the four sites with event parking is 69 percent. For the nine sites with overnight parking demand counted, the average peak parking occupancy is 54 percent.

The sites were surveyed in the 1990s, the 2000s, and the 2020s in California, and Washington.

For all lodging uses, it is important to collect data on occupied rooms as well as total rooms. Future data submissions should indicate the presence of supporting facilities and the level of activity/event taking place during the count period (e.g., full, empty, partially active; number of people attending a meeting/banquet). Data on the presence and usage of TNC service should be documented for all lodging land uses for future analysis purposes.

Source Numbers

217, 315, 401, 438, 603

Hotel (310)

Peak Period Parking Demand vs: Rooms

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

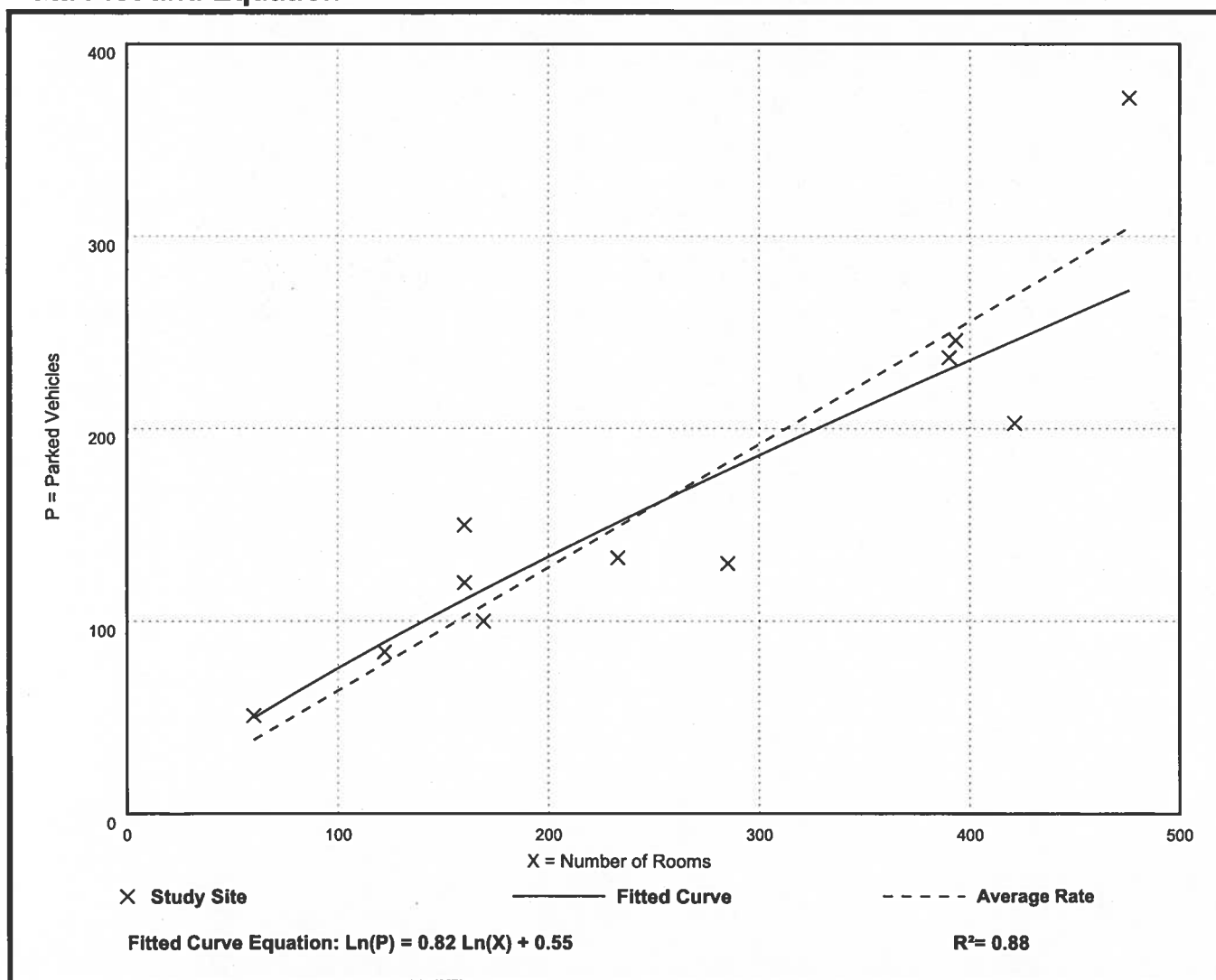
Number of Studies: 11

Avg. Num. of Rooms: 261

Peak Period Parking Demand per Room

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 0.64 | 0.46 - 0.94 | 0.59 / 0.87 | *** | 0.14 (22%) |

Data Plot and Equation



Hotel (310)

Peak Period Parking Demand vs: Rooms

On a: **Saturday**

Setting/Location: **General Urban/Suburban**

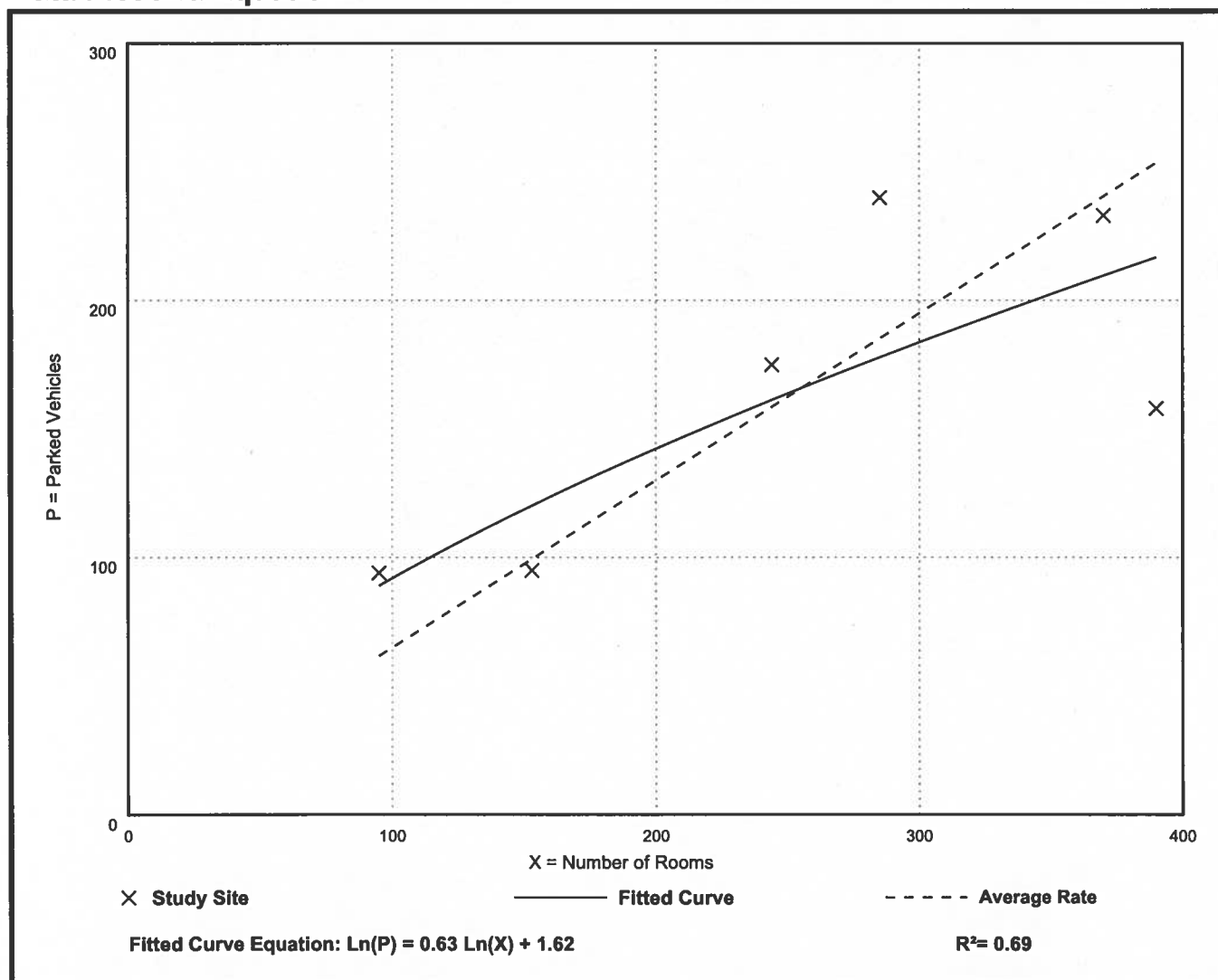
Number of Studies: 6

Avg. Num. of Rooms: 256

Peak Period Parking Demand per Room

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 0.65 | 0.41 - 0.99 | 0.62 / 0.98 | *** | 0.19 (29%) |

Data Plot and Equation



Land Use: 820 Shopping Center (>150k)

Description

A shopping center is an integrated group of commercial establishments that is planned, developed, owned, and managed as a unit. Each study site in this land use has at least 150,000 square feet of gross leasable area (GLA). It often has more than one anchor store. Various names can be assigned to a shopping center within this size range, depending on its specific size and tenants, such as community center, regional center, superregional center, fashion center, and power center.

A shopping center of this size typically contains more than retail merchandising facilities. Office space, a movie theater, restaurants, a post office, banks, a health club, and recreational facilities are common tenants.

A shopping center of this size can be enclosed or open-air. Parking demand generated at a shopping center is based upon the total GLA of the center. In the case of a smaller center without an enclosed mall or peripheral buildings, the GLA is the same as the gross floor area (GFA) of the building.

The 150,000 square feet GLA threshold value between this shopping center land use and shopping plaza (Land Use 821) is based on an examination of parking demand data. For a shopping plaza that is smaller than the threshold value, the presence or absence of a supermarket within the plaza has a noticeable effect on site parking demand. For a shopping center that is larger than the threshold value, the parking demand generated by its other major tenants appears to mask the effects of the presence or absence of an on-site supermarket.

Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a Monday-Thursday (five study sites), a Friday (five study sites), and a Saturday (six study sites).

| Hour Beginning | Percent of Peak Parking Demand | | |
|-----------------|--------------------------------|--------|----------|
| | Weekday (Monday-Thursday) | Friday | Saturday |
| 12:00-4:00 a.m. | — | — | — |
| 5:00 a.m. | — | — | — |
| 6:00 a.m. | — | — | — |
| 7:00 a.m. | — | — | — |
| 8:00 a.m. | — | — | — |
| 9:00 a.m. | — | — | — |
| 10:00 a.m. | 47 | — | 67 |
| 11:00 a.m. | 69 | 88 | 84 |
| 12:00 p.m. | 97 | 93 | 94 |
| 1:00 p.m. | 100 | 97 | 98 |
| 2:00 p.m. | 94 | 95 | 100 |
| 3:00 p.m. | 87 | 100 | 91 |
| 4:00 p.m. | 82 | 95 | 72 |
| 5:00 p.m. | 84 | 92 | 59 |
| 6:00 p.m. | 82 | 87 | — |
| 7:00 p.m. | — | — | — |
| 8:00 p.m. | — | — | — |
| 9:00 p.m. | — | — | — |
| 10:00 p.m. | — | — | — |
| 11:00 p.m. | — | — | — |

Additional Data

Many shopping centers, in addition to the integrated unit of shops in one building or enclosed around a mall, include outparcels (peripheral buildings or pads located on the perimeter of the center adjacent to the streets and major access points). These buildings are typically drive-in banks, retail stores, restaurants, or small offices. Although the data herein do not indicate which of the study sites include peripheral buildings, it can be assumed that some of the data show their effect.

The average parking supply ratios for the study sites with parking supply information are the following:

- 3.9 spaces per 1,000 square feet GLA (23 sites) in a general urban/suburban setting
- 3.2 spaces per 1,000 square feet GLA (1 site) in a dense multi-use urban setting

The average peak parking occupancy at these 24 sites is 62 percent.

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Alberta (CAN), California, Illinois, Kansas, Maine, Maryland, Massachusetts, Minnesota, New Jersey, New York, Ontario (CAN), Oregon, Pennsylvania, Tennessee, Texas, and Virginia.

Future data submissions should attempt to provide information on the composition of each study site (types and number of stores, restaurants, or other tenants within the shopping center).

Source Numbers

89, 145, 152, 179, 224, 313, 315, 431, 433, 436, 441, 511, 525, 542, 565, 604, 605, 615, 620, 621, 628, 634

Shopping Center (>150k) (820)

Peak Period Parking Demand vs: 1000 Sq. Ft. GLA

On a: Weekday (Monday - Thursday)

Setting/Location: General Urban/Suburban

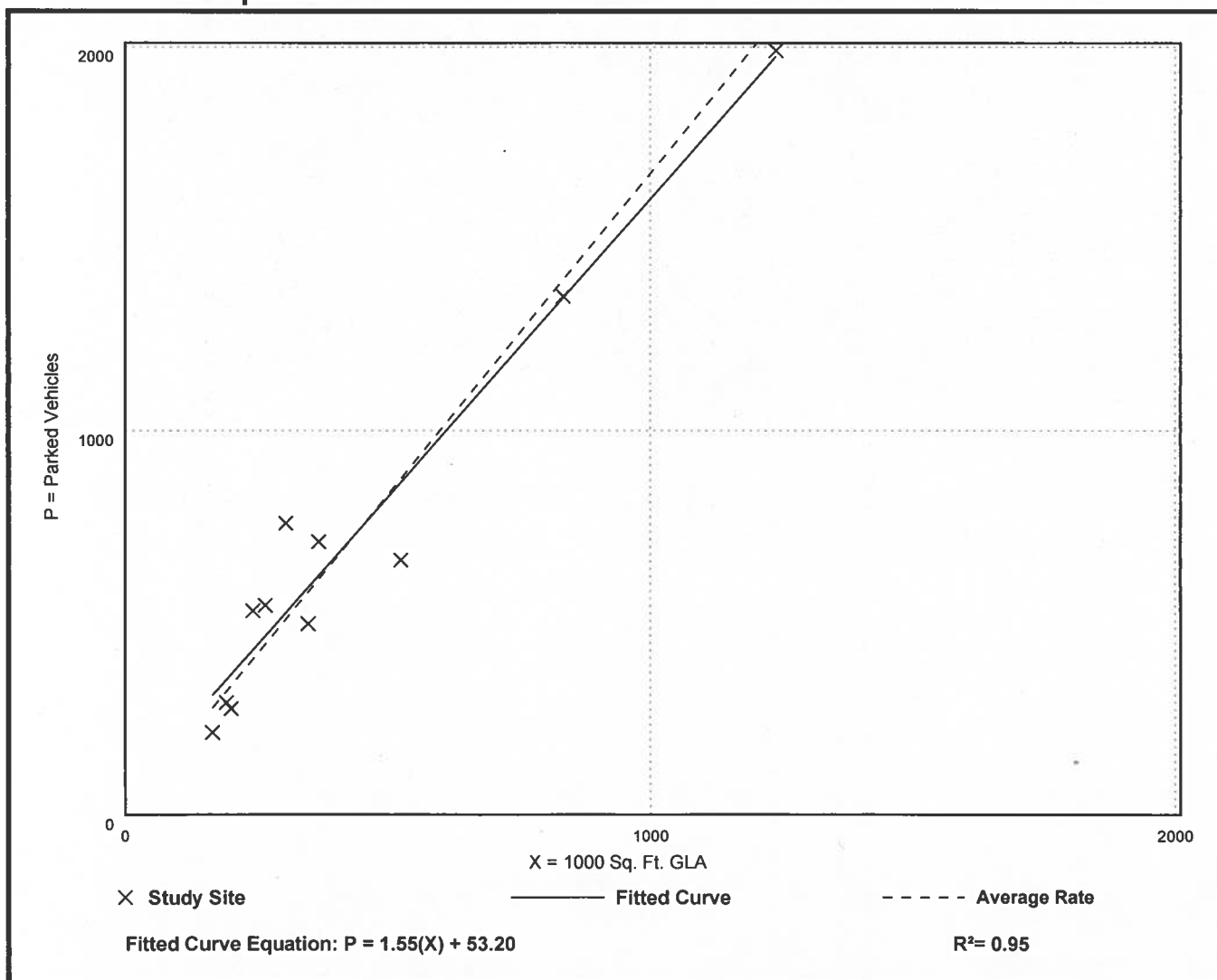
Number of Studies: 11

Avg. 1000 Sq. Ft. GLA: 426

Peak Period Parking Demand per 1000 Sq. Ft. GLA

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.67 | 1.27 - 2.49 | 1.43 / 2.25 | *** | 0.34 (20%) |

Data Plot and Equation



Shopping Center (>150k) (820)

Peak Period Parking Demand vs: 1000 Sq. Ft. GLA

On a: Saturday

Setting/Location: General Urban/Suburban

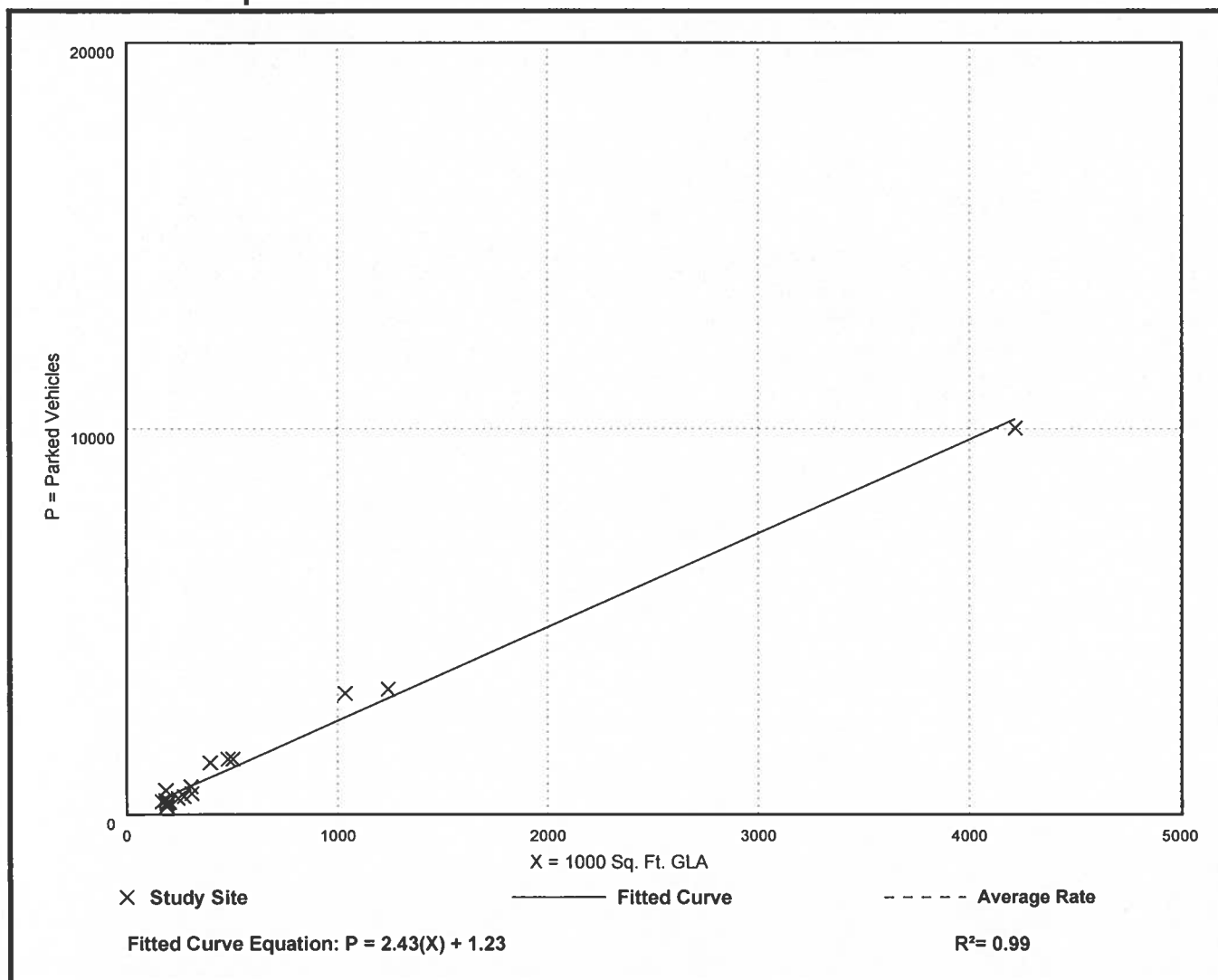
Number of Studies: 18

Avg. 1000 Sq. Ft. GLA: 584

Peak Period Parking Demand per 1000 Sq. Ft. GLA

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 2.43 | 1.09 - 3.40 | 1.73 / 3.08 | *** | 0.51 (21%) |

Data Plot and Equation



Shopping Center (>150k) (820)

Peak Period Parking Demand vs: 1000 Sq. Ft. GLA

On a: Sunday

Setting/Location: General Urban/Suburban

Number of Studies: 3

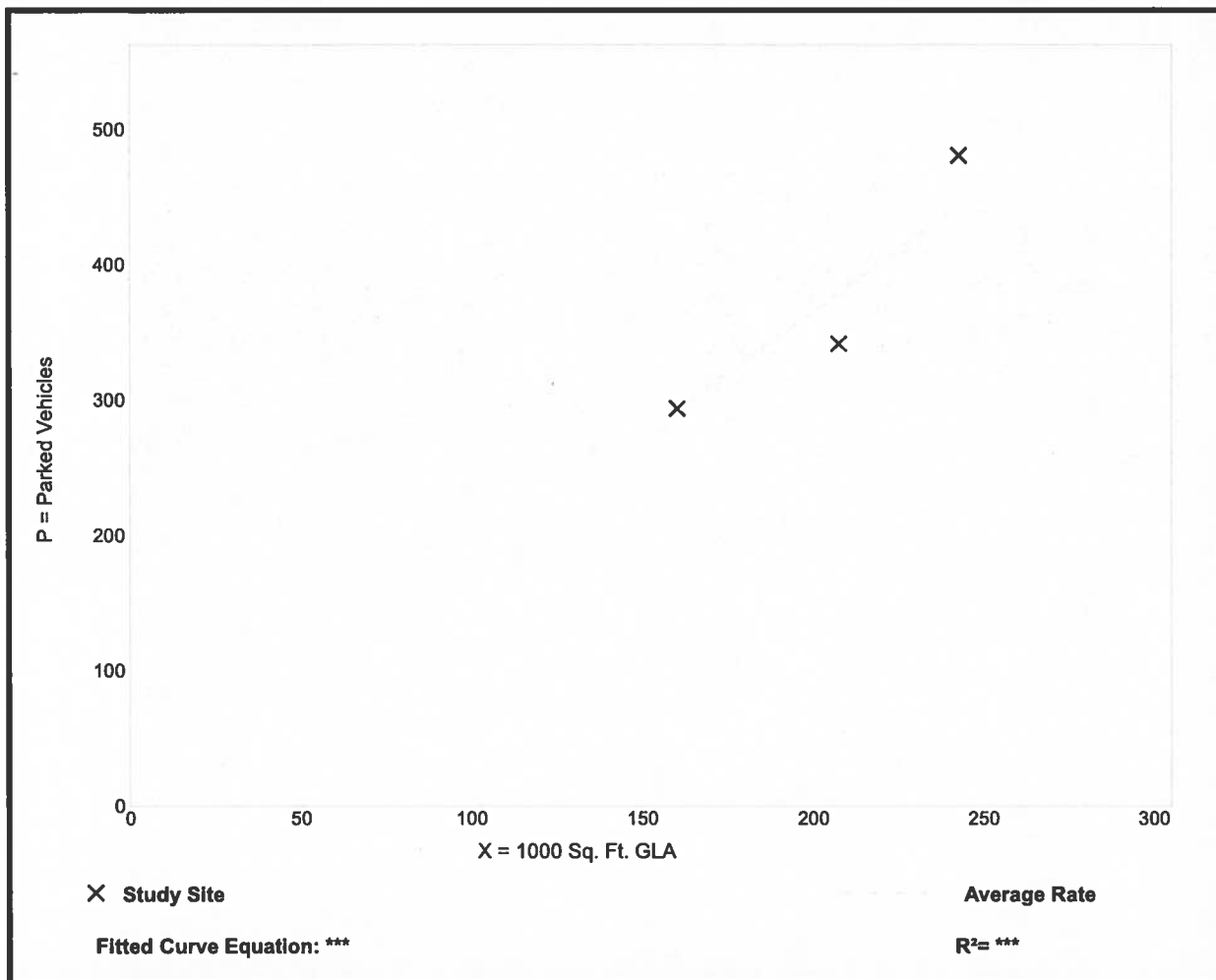
Avg. 1000 Sq. Ft. GLA: 203

Peak Period Parking Demand per 1000 Sq. Ft. GLA

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.83 | 1.65 - 1.98 | 1.71 / 1.98 | *** | 0.18 (10%) |

Data Plot and Equation

Caution – Small Sample Size



Land Use: 710 General Office Building

Description

A general office building is a building with multiple tenants that employ persons in the management, direction, or conduct of legal, accounting, engineering, consulting, real estate, insurance, financial, or other professional services. A general office building with a gross floor area of 10,000 square feet or less is classified as a small office building (Land Use 712).

Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday at 19 study sites in a general urban/suburban setting.

| Hour Beginning | Percent of Weekday Peak Parking Demand |
|-----------------|--|
| | General Urban/Suburban |
| 12:00-4:00 a.m. | — |
| 5:00 a.m. | — |
| 6:00 a.m. | — |
| 7:00 a.m. | 13 |
| 8:00 a.m. | 47 |
| 9:00 a.m. | 87 |
| 10:00 a.m. | 99 |
| 11:00 a.m. | 100 |
| 12:00 p.m. | 86 |
| 1:00 p.m. | 84 |
| 2:00 p.m. | 93 |
| 3:00 p.m. | 93 |
| 4:00 p.m. | 85 |
| 5:00 p.m. | 57 |
| 6:00 p.m. | 21 |
| 7:00 p.m. | — |
| 8:00 p.m. | — |
| 9:00 p.m. | — |
| 10:00 p.m. | — |
| 11:00 p.m. | — |

Additional Data

For the seven study sites with parking supply information and located in a dense multi-use urban setting, the average parking supply ratio is 2.9 spaces per 1,000 square feet GFA. At these sites, the average peak parking occupancy is 56 percent.

For the 63 study sites with parking supply information and located in a general urban/suburban setting, the average parking supply ratio is 3.3 spaces per 1,000 square feet GFA. At these sites, the average peak parking occupancy is 60 percent.

For nine study sites, parking demand data were collected on a Saturday as well as a weekday. For those sites, peak Saturday parking demand averages 13 percent of the peak weekday parking demand.

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Alabama, Arizona, California, Colorado, District of Columbia, Georgia, Illinois, Indiana, Maine, Maryland, Massachusetts, Minnesota, Montana, New Jersey, New York, Ontario (CAN), Oregon, Pennsylvania, Texas, Utah, Virginia, Washington, and Wisconsin.

Source Numbers

122, 201, 211, 217, 276, 425, 431, 433, 436, 438, 440, 516, 531, 540, 551, 555, 556, 567, 571, 572, 588, 607, 618, 622, 633

General Office Building (710)

Peak Period Parking Demand vs: 1000 Sq. Ft. GFA

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

Number of Studies: 77

Avg. 1000 Sq. Ft. GFA: 131

Peak Period Parking Demand per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|--|
| 1.95 | 0.50 - 3.60 | 1.68 / 2.98 | 1.79 - 2.11 | 0.70 (36%) |

Data Plot and Equation

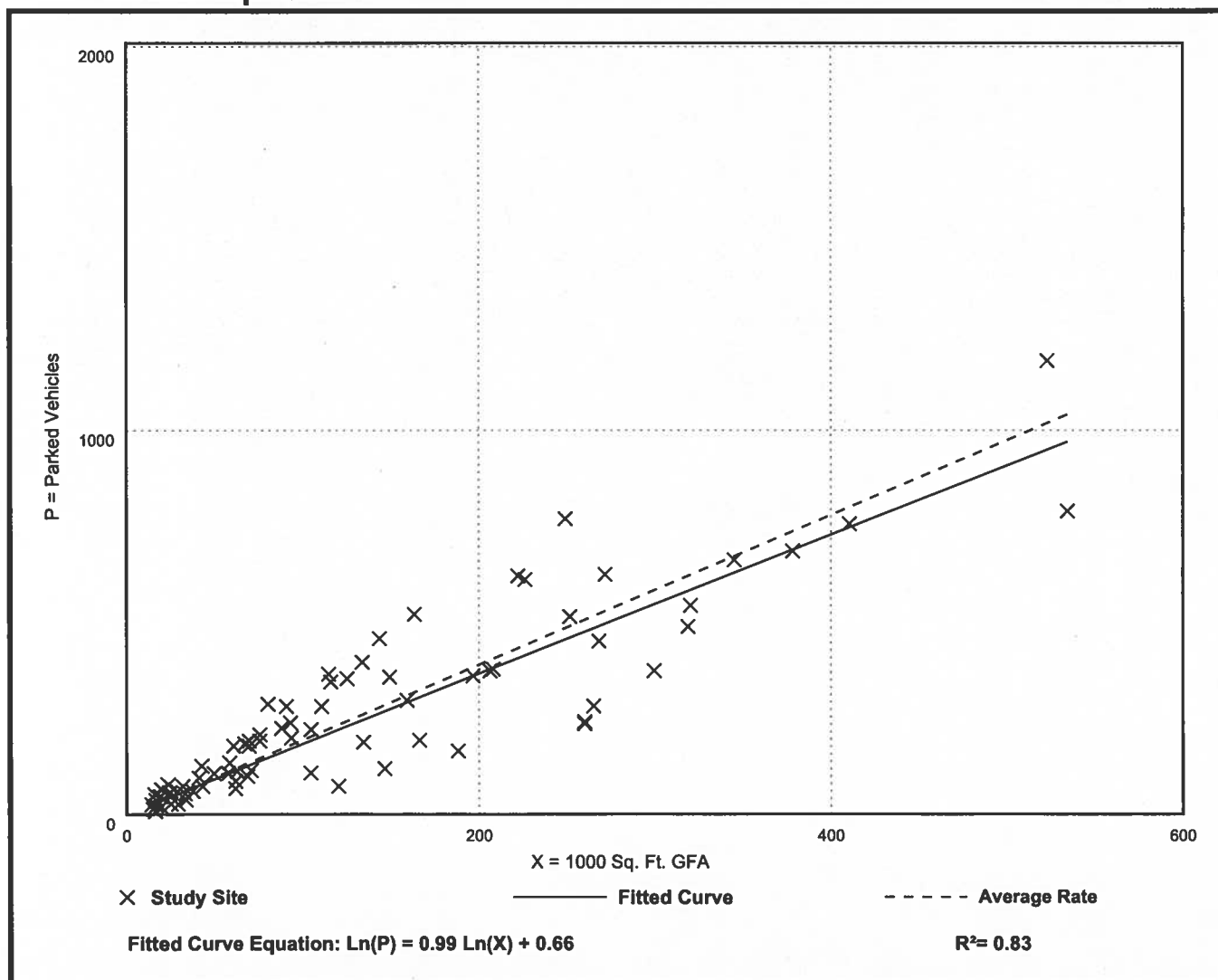


FIGURE 2-4 Weekday Time-of-Day Adjustments

| Land use | | 6 a.m. | 7 a.m. | 8 a.m. | 9 a.m. | 10 a.m. | 11 a.m. | 12 p.m. | 1 p.m. | 2 p.m. | 3 p.m. | 4 p.m. | 5 p.m. | 6 p.m. | 7 p.m. | 8 p.m. | 9 p.m. | 10 p.m. | 11 p.m. | 12 a.m. |
|--|------------------|--------|--------|--------|--------|---------|---------|---------|--------|--------|--------|--------|--------|--------|--------|--------|--------|---------|---------|---------|
| Retail typical | Visitors | 1% | 5% | 15% | 35% | 60% | 75% | 100% | 100% | 95% | 85% | 85% | 85% | 90% | 80% | 65% | 45% | 15% | 5% | 0% |
| | December | 1% | 5% | 15% | 30% | 55% | 75% | 90% | 100% | 100% | 95% | 80% | 85% | 90% | 90% | 85% | 50% | 30% | 10% | 0% |
| | Late December | 1% | 5% | 10% | 20% | 40% | 65% | 90% | 100% | 100% | 100% | 95% | 85% | 70% | 55% | 40% | 25% | 15% | 5% | 0% |
| All | Employees | 10% | 15% | 25% | 45% | 75% | 95% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 60% | 40% | 20% | 0% |
| | | | | | | | | | | | | | | | | | | | | |
| Supermarket/ grocery | Visitors | 5% | 20% | 30% | 50% | 60% | 67% | 85% | 90% | 95% | 97% | 100% | 100% | 100% | 85% | 55% | 35% | 20% | 5% | 5% |
| | Employees | 20% | 30% | 40% | 80% | 90% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 50% | 35% | 20% | 20% | 20% | 20% |
| Pharmacy | Visitors | 5% | 20% | 30% | 60% | 60% | 67% | 85% | 90% | 95% | 97% | 100% | 100% | 100% | 85% | 55% | 35% | 20% | 5% | 5% |
| | Employees | 20% | 30% | 40% | 80% | 90% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 50% | 35% | 20% | 20% | 20% | 20% |
| Discount stores/ superstores | Visitors | 15% | 35% | 45% | 65% | 75% | 85% | 100% | 100% | 100% | 100% | 95% | 85% | 75% | 60% | 45% | 30% | 10% | 5% | 1% |
| | Employees | 25% | 45% | 55% | 75% | 85% | 100% | 100% | 100% | 100% | 100% | 100% | 95% | 85% | 70% | 55% | 40% | 20% | 20% | 20% |
| Home improvement stores/garden | Visitors | 15% | 20% | 35% | 55% | 85% | 99% | 100% | 99% | 98% | 90% | 85% | 80% | 75% | 60% | 50% | 30% | 10% | 0% | 0% |
| | Employees | 25% | 30% | 45% | 65% | 95% | 100% | 100% | 100% | 100% | 100% | 95% | 90% | 85% | 70% | 60% | 40% | 20% | 0% | 0% |
| Food and beverage | | | | | | | | | | | | | | | | | | | | |
| Fine/casual dining | Visitors | 0% | 0% | 0% | 0% | 15% | 40% | 75% | 75% | 65% | 40% | 50% | 75% | 95% | 100% | 100% | 100% | 95% | 75% | 25% |
| | Employees | 0% | 20% | 50% | 75% | 90% | 90% | 90% | 90% | 90% | 75% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 85% | 35% |
| Family restaurant | Visitors | 25% | 50% | 60% | 75% | 85% | 90% | 100% | 90% | 50% | 45% | 45% | 75% | 80% | 80% | 80% | 60% | 55% | 75% | 25% |
| | Employees | 50% | 75% | 90% | 90% | 100% | 100% | 100% | 100% | 100% | 75% | 75% | 95% | 95% | 95% | 95% | 80% | 65% | 65% | 35% |
| Fast casual/ fast food/food court/food halls | Visitors | 5% | 10% | 20% | 30% | 55% | 85% | 100% | 100% | 90% | 60% | 55% | 60% | 85% | 80% | 50% | 30% | 20% | 10% | 5% |
| | Employees | 20% | 20% | 30% | 40% | 75% | 100% | 100% | 100% | 95% | 70% | 60% | 70% | 90% | 90% | 60% | 40% | 30% | 20% | 20% |
| Bar/lounge/ nightclub | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 25% | 50% | 75% | 100% | 100% | 75% | 50% |
| | Employees | 0% | 0% | 0% | 5% | 5% | 5% | 5% | 10% | 10% | 10% | 20% | 45% | 70% | 100% | 100% | 100% | 100% | 90% | 60% |
| Entertainment | | | | | | | | | | | | | | | | | | | | |
| Family entertainment | Visitors | 0% | 0% | 0% | 0% | 45% | 65% | 85% | 95% | 100% | 95% | 90% | 70% | 60% | 45% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 0% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 70% | 55% | 10% | 5% | 5% | 5% | 5% |
| Active entertainment | Visitors | 0% | 0% | 0% | 0% | 25% | 65% | 85% | 90% | 95% | 95% | 90% | 95% | 100% | 95% | 90% | 65% | 10% | 0% | 0% |
| | Employees | 5% | 5% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 75% | 10% | 5% | 5% |
| Adult active entertainment | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 25% | 50% | 75% | 100% | 100% | 100% | 100% |
| | Employees | 0% | 0% | 0% | 5% | 5% | 5% | 5% | 10% | 10% | 10% | 20% | 45% | 70% | 100% | 100% | 100% | 100% | 100% | 100% |
| All movies typical | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 20% | 45% | 55% | 55% | 55% | 60% | 60% | 80% | 100% | 100% | 80% | 65% | 40% |
| | Late December | 0% | 0% | 0% | 0% | 0% | 0% | 35% | 60% | 75% | 80% | 80% | 80% | 70% | 80% | 100% | 100% | 85% | 70% | 55% |
| | All | 0% | 0% | 0% | 0% | 0% | 10% | 50% | 60% | 60% | 75% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 70% | 50% |
| Live theater | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 25% | 100% | 100% | 0% | 0% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 30% | 30% | 30% | 30% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |
| Outdoor amphitheater | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 25% | 100% | 100% | 0% | 0% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 30% | 30% | 30% | 30% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |
| Public park/ destination open space | Visitors | 1% | 5% | 10% | 25% | 50% | 65% | 85% | 95% | 100% | 95% | 90% | 70% | 90% | 100% | 100% | 100% | 80% | 50% | 10% |
| | Employees | 5% | 10% | 25% | 50% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 100% | 100% | 100% | 100% | 100% | 60% | 20% |
| Museum/ aquarium | Visitors | 0% | 0% | 0% | 0% | 45% | 65% | 85% | 95% | 100% | 95% | 90% | 85% | 60% | 30% | 10% | 0% | 0% | 0% | 0% |
| | Employees | 5% | 5% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 75% | 10% | 5% | 0% | 0% | 5% | 5% |
| Arena No matinee | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 1% | 10% | 25% | 100% | 100% | 85% | 0% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 30% | 30% | 30% | 30% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |

(continued on next page)

FIGURE 2-4 (continued)

| Land use | | 6 a.m. | 7 a.m. | 8 a.m. | 9 a.m. | 10 a.m. | 11 a.m. | 12 p.m. | 1 p.m. | 2 p.m. | 3 p.m. | 4 p.m. | 5 p.m. | 6 p.m. | 7 p.m. | 8 p.m. | 9 p.m. | 10 p.m. | 11 p.m. | 12 a.m. |
|--------------------------------------|----------------------|--------|--------|--------|--------|---------|---------|---------|--------|--------|--------|--------|--------|--------|--------|--------|--------|---------|---------|---------|
| Entertainment (continued) | | | | | | | | | | | | | | | | | | | | |
| Pro football stadium 8 p.m. start | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 5% | 5% | 5% | 5% | 5% | 5% | 10% | 50% | 100% | 100% | 85% | 25% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 30% | 30% | 30% | 30% | 30% | 100% | 100% | 100% | 100% | 100% | 25% | 10% |
| Pro baseball stadium | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 5% | 5% | 5% | 5% | 5% | 5% | 10% | 50% | 100% | 100% | 85% | 25% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 30% | 30% | 30% | 30% | 30% | 100% | 100% | 100% | 100% | 100% | 25% | 10% |
| Health club | Visitors | 70% | 40% | 40% | 70% | 70% | 80% | 60% | 70% | 70% | 70% | 80% | 90% | 100% | 90% | 80% | 70% | 35% | 10% | 0% |
| | Employees | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 100% | 100% | 75% | 50% | 20% | 20% | 20% | 0% |
| Public library | Visitors | 0% | 0% | 0% | 100% | 100% | 98% | 98% | 78% | 72% | 65% | 70% | 79% | 60% | 50% | 40% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 10% | 50% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 75% | 50% | 20% | 10% | 0% | 0% | 0% |
| Daycare center | Visitors | 0% | 2% | 25% | 75% | 20% | 20% | 20% | 20% | 20% | 20% | 100% | 50% | 20% | 5% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 50% | 75% | 90% | 90% | 90% | 90% | 90% | 90% | 100% | 100% | 100% | 60% | 40% | 10% | 0% | 0% | 0% | 0% |
| Convention center | Visitors | 0% | 0% | 50% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 50% | 30% | 30% | 10% | 0% | 0% | 0% |
| | Employees | 5% | 30% | 33% | 33% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 70% | 40% | 25% | 20% | 20% | 5% | 0% | 0% |
| Hotel and residential | | | | | | | | | | | | | | | | | | | | |
| Hotel-business | Visitors | 95% | 90% | 80% | 70% | 60% | 60% | 55% | 55% | 60% | 60% | 65% | 70% | 75% | 75% | 80% | 85% | 95% | 100% | 100% |
| Hotel-leisure | Visitors | 95% | 95% | 90% | 80% | 70% | 70% | 65% | 65% | 70% | 70% | 75% | 80% | 85% | 85% | 90% | 95% | 95% | 100% | 100% |
| Employee | Employees | 10% | 30% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 70% | 70% | 40% | 20% | 20% | 20% | 20% | 10% | 5% |
| Restaurant/ lounge | Visitors | 0% | 10% | 30% | 10% | 10% | 5% | 100% | 100% | 33% | 10% | 10% | 30% | 55% | 60% | 70% | 67% | 60% | 40% | 30% |
| Meeting/banquet (<100 sq ft/key) | Visitors | 0% | 0% | 30% | 60% | 60% | 60% | 65% | 65% | 65% | 65% | 65% | 100% | 100% | 100% | 100% | 100% | 50% | 0% | 0% |
| Convention (>100 sq ft/key) | Visitors | 0% | 0% | 50% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 50% | 30% | 30% | 10% | 0% | 0% | 0% |
| Employee | Employees | 10% | 10% | 60% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 60% | 40% | 40% | 20% | 0% | 0% | 0% |
| Residential guest | Visitors | 0% | 10% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 40% | 60% | 100% | 100% | 100% | 100% | 80% | 50% |
| Resident reserved | Residents | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% |
| Residential suburban | Residents | 95% | 80% | 67% | 55% | 50% | 45% | 40% | 40% | 40% | 40% | 45% | 50% | 60% | 70% | 80% | 85% | 95% | 97% | 100% |
| Residential urban | Residents | 95% | 85% | 75% | 65% | 60% | 55% | 50% | 50% | 50% | 55% | 60% | 65% | 70% | 75% | 80% | 85% | 95% | 97% | 100% |
| Active senior housing | Visitors & employees | 95% | 97% | 100% | 100% | 99% | 98% | 98% | 99% | 98% | 100% | 99% | 94% | 96% | 98% | 97% | 97% | 97% | 98% | 98% |
| | Residents | 95% | 97% | 100% | 100% | 99% | 98% | 98% | 99% | 98% | 100% | 99% | 94% | 96% | 98% | 97% | 97% | 97% | 98% | 98% |
| Office | | | | | | | | | | | | | | | | | | | | |
| Office | Visitors | 0% | 1% | 20% | 60% | 100% | 45% | 15% | 45% | 95% | 45% | 15% | 10% | 5% | 2% | 1% | 0% | 0% | 0% | 0% |
| | Employees unreserved | 3% | 15% | 50% | 90% | 100% | 100% | 85% | 85% | 95% | 95% | 85% | 60% | 25% | 15% | 5% | 3% | 1% | 0% | 0% |
| | Employees reserved | 00% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% |
| Medical/ dental office | Visitors | 0% | 0% | 90% | 90% | 100% | 100% | 30% | 90% | 100% | 100% | 90% | 80% | 67% | 30% | 15% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 20% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 67% | 30% | 15% | 0% | 0% | 0% | 0% |
| Bank (drive-in branch) | Visitors | 0% | 0% | 50% | 90% | 100% | 50% | 50% | 50% | 70% | 50% | 80% | 100% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 0% | 60% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |

Source: See chapter 4 discussions for each land use.

FIGURE 2-5 Weekend Time-of-Day Adjustments

| Land use | | 6 a.m. | 7 a.m. | 8 a.m. | 9 a.m. | 10 a.m. | 11 a.m. | 12 p.m. | 1 p.m. | 2 p.m. | 3 p.m. | 4 p.m. | 5 p.m. | 6 p.m. | 7 p.m. | 8 p.m. | 9 p.m. | 10 p.m. | 11 p.m. | 12 a.m. |
|--|-----------|-----------|-----------|-----------|-----------|------------|------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------------|------------|------------|
| Retail typical | Visitors | 1% | 5% | 30% | 50% | 70% | 90% | 95% | 100% | 100% | 95% | 90% | 80% | 75% | 70% | 65% | 50% | 30% | 10% | 0% |
| December | Visitors | 1% | 5% | 10% | 35% | 60% | 85% | 100% | 100% | 100% | 100% | 90% | 80% | 65% | 60% | 55% | 50% | 35% | 15% | 1% |
| Late December | Visitors | 1% | 5% | 10% | 20% | 40% | 60% | 80% | 95% | 100% | 100% | 95% | 85% | 70% | 60% | 50% | 30% | 20% | 10% | 0% |
| All | Employees | 10% | 15% | 40% | 75% | 85% | 95% | 100% | 100% | 100% | 100% | 100% | 95% | 85% | 80% | 75% | 65% | 45% | 15% | 0% |
| Supermarket/ grocery | Visitors | 10% | 25% | 50% | 75% | 95% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 50% | 33% | 25% | 15% | 5% | 4% | 3% |
| | Employees | 15% | 35% | 70% | 85% | 100% | 100% | 100% | 100% | 85% | 75% | 60% | 55% | 45% | 40% | 30% | 20% | 10% | 10% | 5% |
| Pharmacy | Visitors | 8% | 25% | 50% | 75% | 95% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 50% | 33% | 25% | 15% | 5% | 4% | 3% |
| | Employees | 15% | 35% | 70% | 85% | 100% | 100% | 100% | 100% | 85% | 75% | 60% | 55% | 45% | 40% | 30% | 20% | 10% | 10% | 5% |
| Discount stores/ superstores | Visitors | 10% | 15% | 20% | 30% | 45% | 65% | 85% | 95% | 100% | 100% | 100% | 95% | 80% | 60% | 45% | 30% | 10% | 5% | 1% |
| | Employees | 20% | 25% | 30% | 40% | 55% | 75% | 95% | 100% | 100% | 100% | 100% | 100% | 90% | 70% | 55% | 40% | 20% | 15% | 0% |
| Home improvement stores/garden | Visitors | 15% | 20% | 35% | 55% | 60% | 80% | 95% | 100% | 95% | 95% | 80% | 75% | 75% | 80% | 90% | 70% | 10% | 0% | 9% |
| | Employees | 25% | 30% | 45% | 65% | 70% | 90% | 100% | 100% | 100% | 100% | 90% | 85% | 85% | 90% | 100% | 80% | 20% | 0% | 0% |
| Food and beverage | | | | | | | | | | | | | | | | | | | | |
| Fine/casual dining | Visitors | 0% | 0% | 0% | 0% | 0% | 15% | 50% | 55% | 45% | 45% | 45% | 60% | 90% | 95% | 100% | 90% | 90% | 90% | 50% |
| | Employees | 0% | 20% | 30% | 60% | 75% | 75% | 75% | 75% | 75% | 75% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 85% | 50% |
| Family restaurant | Visitors | 10% | 25% | 45% | 70% | 90% | 90% | 100% | 85% | 65% | 40% | 45% | 60% | 70% | 70% | 65% | 30% | 25% | 15% | 10% |
| | Employees | 50% | 75% | 90% | 90% | 100% | 100% | 100% | 100% | 100% | 75% | 75% | 95% | 95% | 95% | 95% | 80% | 65% | 65% | 35% |
| Fast casual/ fast food/food court/food halls | Visitors | 5% | 10% | 20% | 30% | 55% | 85% | 100% | 100% | 90% | 60% | 55% | 60% | 85% | 80% | 50% | 30% | 20% | 10% | 5% |
| | Employees | 15% | 20% | 30% | 40% | 75% | 100% | 100% | 100% | 95% | 70% | 60% | 70% | 90% | 90% | 60% | 40% | 30% | 20% | 20% |
| Bar/lounge/ nightclub | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 25% | 50% | 75% | 100% | 100% | 100% | 100% |
| | Employees | 0% | 0% | 0% | 5% | 5% | 5% | 5% | 10% | 10% | 10% | 20% | 45% | 70% | 100% | 100% | 100% | 100% | 100% | 100% |
| Entertainment | | | | | | | | | | | | | | | | | | | | |
| Family entertainment | Visitors | 0% | 0% | 0% | 0% | 25% | 65% | 85% | 90% | 95% | 95% | 90% | 95% | 100% | 95% | 90% | 65% | 10% | 0% | 0% |
| | Employees | 5% | 5% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 75% | 10% | 5% | 5% |
| Active entertainment | Visitors | 0% | 0% | 0% | 0% | 25% | 65% | 85% | 90% | 95% | 95% | 90% | 95% | 100% | 95% | 90% | 65% | 10% | 0% | 0% |
| | Employees | 5% | 5% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 90% | 100% | 100% | 100% | 100% | 75% | 10% | 5% | 5% |
| Adult active entertainment | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 25% | 50% | 75% | 100% | 100% | 100% | 100% |
| | Employees | 0% | 0% | 0% | 5% | 5% | 5% | 5% | 10% | 10% | 10% | 20% | 45% | 70% | 100% | 100% | 100% | 100% | 100% | 100% |
| All movies typical | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 20% | 45% | 55% | 55% | 55% | 60% | 60% | 80% | 100% | 100% | 100% | 80% | 50% |
| Late December | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 35% | 60% | 75% | 80% | 80% | 80% | 70% | 80% | 100% | 100% | 100% | 85% | 70% |
| All | Employees | 0% | 0% | 0% | 0% | 0% | 0% | 50% | 60% | 60% | 75% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 70% | 50% |
| Live theater | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 17% | 67% | 67% | 1% | 1% | 1% | 25% | 100% | 100% | 0% | 0% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 100% | 100% | 100% | 30% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |
| Outdoor amphitheater | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 17% | 67% | 67% | 1% | 1% | 1% | 25% | 100% | 100% | 0% | 0% | 0% |
| | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 100% | 100% | 100% | 30% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |
| Public park/ destination open space | Visitors | 0% | 0% | 0% | 1% | 30% | 60% | 75% | 90% | 97% | 100% | 98% | 85% | 70% | 80% | 100% | 100% | 95% | 50% | 10% |
| | Employees | 0% | 0% | 10% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 80% |
| Museum/ aquarium | Visitors | 0% | 0% | 0% | 0% | 45% | 65% | 85% | 95% | 100% | 95% | 90% | 85% | 60% | 30% | 10% | 0% | 0% | 0% | 0% |
| | Employees | 5% | 5% | 5% | 25% | 75% | 100% | 100% | 100% | 100% | 100% | 100% | 80% | 75% | 10% | 5% | 0% | 0% | 5% | 5% |
| Arena | Visitors | 0% | 0% | 0% | 1% | 1% | 1% | 1% | 25% | 95% | 95% | 81% | 1% | 1% | 25% | 100% | 100% | 0% | 0% | 0% |
| No matinee | Employees | 0% | 10% | 10% | 20% | 20% | 20% | 30% | 100% | 100% | 100% | 100% | 30% | 100% | 100% | 100% | 100% | 30% | 10% | 5% |

(continued on next page)

FIGURE 2-5 (continued)

| Land use | | 6 a.m. | 7 a.m. | 8 a.m. | 9 a.m. | 10 a.m. | 11 a.m. | 12 p.m. | 1 p.m. | 2 p.m. | 3 p.m. | 4 p.m. | 5 p.m. | 6 p.m. | 7 p.m. | 8 p.m. | 9 p.m. | 10 p.m. | 11 p.m. | 12 a.m. |
|--------------------------------------|----------------------|-----------|-----------|-----------|-----------|------------|------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------------|------------|------------|
| Entertainment (continued) | | | | | | | | | | | | | | | | | | | | |
| Pro football stadium 8 p.m. start | Visitors | 0% | 0% | 1% | 1% | 5% | 5% | 50% | 100% | 100% | 85% | 25% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 5% | 10% | 20% | 30% | 30% | 100% | 100% | 100% | 100% | 25% | 10% | 5% | 5% | 0% | 0% | 0% | 0% | 0% |
| Pro baseball stadium | Visitors | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 25% | 50% | 90% | 100% | 100% | 100% | 0% | 0% |
| | Employees | 0% | 0% | 0% | 5% | 5% | 5% | 5% | 5% | 5% | 5% | 20% | 75% | 75% | 100% | 100% | 100% | 100% | 100% | 100% |
| Health club | Visitors | 80% | 45% | 35% | 50% | 35% | 50% | 50% | 30% | 25% | 30% | 55% | 100% | 95% | 60% | 30% | 10% | 1% | 1% | 0% |
| | Employees | 50% | 50% | 50% | 50% | 50% | 50% | 50% | 50% | 50% | 50% | 75% | 100% | 100% | 75% | 50% | 20% | 20% | 20% | 0% |
| Public library | Visitors | 0% | 0% | 0% | 0% | 100% | 90% | 80% | 65% | 50% | 35% | 11% | 5% | 5% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 0% | 10% | 50% | 100% | 100% | 100% | 100% | 100% | 50% | 10% | 10% | 10% | 10% | 0% | 0% | 0% | 0% | 0% |
| Daycare center | Visitors | 0% | 2% | 25% | 75% | 20% | 20% | 20% | 20% | 20% | 20% | 100% | 50% | 20% | 5% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 50% | 75% | 90% | 90% | 90% | 90% | 90% | 90% | 100% | 100% | 100% | 60% | 40% | 10% | 0% | 0% | 0% | 0% |
| Convention center | Visitors | 0% | 0% | 50% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 50% | 30% | 30% | 10% | 0% | 0% | 0% |
| | Employees | 5% | 30% | 33% | 33% | 100% | 100% | 100% | 100% | 100% | 100% | 90% | 70% | 40% | 25% | 20% | 20% | 5% | 0% | 0% |
| Hotel and residential | | | | | | | | | | | | | | | | | | | | |
| Hotel-business | Visitors | 95% | 90% | 80% | 70% | 60% | 60% | 55% | 55% | 60% | 60% | 65% | 70% | 75% | 75% | 80% | 85% | 95% | 100% | 100% |
| Hotel-leisure | Visitors | 95% | 95% | 90% | 80% | 70% | 70% | 65% | 65% | 70% | 70% | 75% | 80% | 85% | 85% | 90% | 95% | 95% | 100% | 100% |
| Employee | Employees | 10% | 30% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 70% | 70% | 40% | 20% | 20% | 20% | 20% | 10% | 5% |
| Restaurant/ lounge | Visitors | 0% | 10% | 30% | 10% | 10% | 5% | 100% | 100% | 33% | 10% | 10% | 30% | 55% | 60% | 70% | 67% | 60% | 40% | 30% |
| Meeting/banquet (<100 sq ft/key) | Visitors | 0% | 0% | 30% | 60% | 60% | 60% | 65% | 65% | 65% | 65% | 65% | 100% | 100% | 100% | 100% | 100% | 50% | 0% | 0% |
| Convention (>100 sq ft/key) | Visitors | 0% | 0% | 50% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 50% | 30% | 30% | 10% | 0% | 0% | 0% |
| Employee | Employees | 10% | 10% | 60% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 60% | 10% | 10% |
| Residential guest | Visitors | 0% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 20% | 40% | 60% | 100% | 100% | 100% | 100% | 80% | 50% |
| Resident reserved | Residents | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% |
| Residential suburban | Residents | 100% | 95% | 88% | 80% | 75% | 70% | 68% | 65% | 65% | 68% | 71% | 74% | 77% | 80% | 83% | 86% | 89% | 92% | 100% |
| Residential urban | Residents | 90% | 85% | 80% | 75% | 70% | 69% | 68% | 67% | 66% | 55% | 60% | 55% | 50% | 55% | 65% | 75% | 85% | 90% | 100% |
| Active senior housing | Visitors | 94% | 98% | 97% | 95% | 93% | 94% | 97% | 99% | 100% | 100% | 99% | 98% | 98% | 98% | 97% | 95% | 94% | 98% | 98% |
| | Employees | 94% | 98% | 97% | 95% | 93% | 94% | 97% | 99% | 100% | 100% | 99% | 98% | 98% | 98% | 97% | 95% | 94% | 98% | 98% |
| Office | | | | | | | | | | | | | | | | | | | | |
| Office | Visitors | 0% | 20% | 60% | 80% | 90% | 100% | 90% | 80% | 60% | 40% | 20% | 10% | 5% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees unreserved | 0% | 20% | 60% | 80% | 90% | 100% | 90% | 80% | 60% | 40% | 20% | 10% | 5% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees reserved | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% | 100% |
| Medical/ dental office | Visitors | 0% | 0% | 90% | 90% | 100% | 100% | 30% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 20% | 100% | 100% | 100% | 100% | 100% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |
| Bank (drive-in branch) | Visitors | 0% | 0% | 25% | 40% | 75% | 100% | 90% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |
| | Employees | 0% | 0% | 90% | 100% | 100% | 100% | 100% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% | 0% |

Source: See chapter 4 discussions for each land use.



WINGROVE ACADEMY

Wellington, FL

SCHOOL TRAFFIC OPERATIONAL MANAGEMENT PLAN

PREPARED FOR:

Related Ross
360 South Rosemary Avenue
Suite 800
West Palm Beach, FL 33401

JOB NO. 25-024

DATE: 05/14/2025
Revised: 09/22/2025
Revised: 11/14/2025

Bryan G. Kelley, Professional Engineer, State of Florida, License No. 74006

This item has been digitally signed and sealed by Bryan G. Kelley, P.E., on 11/14/25.

Printed Copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

Digitally signed by
Bryan Kelley
Date: 2025.11.18
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OPERATIONAL PLAN

1.0 INTRODUCTION

The overall K-Park MUPD is located in the southwest corner of Stribling Way and State Road 7 in the Village of Wellington, Florida. The proposed plan of development for the overall MUPD is to consist of the following uses:

- 215 Multi-Family Residential DU
- 180 Room Hotel
- 1,750 Student K-12 Private School
- 75,000 SF Office
- 335,000 SF Shopping Plaza (Including +/- 105,000 SF of Restaurant and up to 20,000 SF of outdoor seating area)

The purpose of this report is to provide a Traffic Operational Plan detailing the school's proposed operations, onsite queueing, and circulation, as well as to present solutions to mitigate project impacts and minimize the effects to the surrounding roadway network. The proposed private school will be located on the western half of the site. A detailed review of the school operations was performed with specific emphasis on onsite operations and reducing the impact to the surrounding roadway system. Extracurricular activities, sporting events, parent meetings, and other gatherings that take place at the school will be organized to minimize offsite traffic impacts. Each activity will be carefully planned and will be limited to a specific number that corresponds to both onsite parking as well as specific traffic management procedures.

The school will be developed in 2 phases. The 1st phase will consist of up to 1,008 students and will include surface parking lots instead of a structured parking lot. The 2nd phase will consist of up to 1,750 students and a structured parking garage. The traffic operational plan is very similar between the two. This operational management plan will focus on the full buildout but will identify any differences between the two phases.

2.0 TRAFFIC GENERATION

A comprehensive Traffic Impact Statement for the proposed project has been prepared concurrently with the School Traffic Operational Management Plan. The anticipated peak hour traffic generation for the approximate 1,750 student private school with no reduction for internal capture or pass-by credits may be summarized as follows:

| | | |
|-----------------------------------|---|---------|
| Daily Trip Generation | = | 4,340 |
| A.M. Peak Hour Traffic Generation | = | 988 pht |
| P.M. Peak Hour Traffic Generation | = | 298 pht |

Note the ITE trip generation rates for a K-12th grade private school were reduced for the AM peak hour due to the proposed multiple staggered start and end times. The AM peak hour trip generation reflects a maximum of 1,250 students (based on grade levels) arriving within a one-hour time frame. The trip generation details are further documented in the Traffic Impact Statement.

3.0 STAGGERED START/END TIMES

The school will commit to multiple staggered start and end times at full enrollment of approximately 1,750 students. The specific school times have not been determined at this time but the different grade levels and number of students are proposed as follows:

Staggered School Grade Levels

| Grade Level | Students |
|---|----------|
| Early Education (PreK-1 st Grade) | +/- 240 |
| Elementary School (2 nd - 4 th Grade) | +/- 438 |
| Middle School (5 th - 8 th Grade) | +/- 391 |
| High School (9 th - 12 th Grade) | +/- 681 |

4.0 DRIVEWAY ACCESS

The overall access for the K-Park MUPD is proposed via the following driveway connections:

1. Full access driveway on Stribling Way approximately 2,300 feet west of State Road 7 (school traffic signal or roundabout proposed)
2. Full access driveway connection on Stribling Way approximately 1/4 mile west of State Road 7
3. Right in, right out driveway connection to Stribling Way approximately 500 feet west of State Road 7
4. Right in, right out driveway connection to State Road 7 approximately 500 feet south of Stribling Way
5. Full access signalized driveway connection to State Road 7 approximately 1,100 feet south of Stribling Way

The driveway locations are further depicted in Appendix A attached to this report. The school primary access will occur on State Road 7 utilizing driveway 5 above. Except for the early education students, parent drop off, pick up, bus routes, and staff members will use the State Road 7 to access (ingress) the school property.

When exiting the school, parents, students, staff, will have their choice to exit via State Road 7 or Stribling Way. However, buses will be required to utilize State Road 7 for egress. Cross access will also be available to the mixed use site via the internal roundabout in front of the Life Church property.

Access for the early education (Prek-1st grade) will be via Stribling Way (Driveway 1). The traffic circulation is further depicted in Appendix C attached to this report.

5.0 DELIVERIES

The school will contract with vendors as applicable for different services. Deliveries will be scheduled to avoid peak drop-off and pick-up hours. Deliveries and service access drop-off is located next to bus loop.

6.0 BUSES

As stated previously, all buses will arrive and depart via State Road 7. The bus drop off and pick up loop is located east of the tennis courts and west of the baseball fields. The bus drop off area will be the same for all grade levels. The number of buses for the school will be determined at a later stage in development and may fluctuate from year to year depending on demand. Not all buses will arrive at the same time. Appendix B provides a map of the anticipated bus circulation routes when arriving and departing the site. A staff member will be present at the driveway entrance to ensure that parents do not try and drop students off at this location.

7.0 PEDESTRIANS AND BICYCLISTS

Children riding their bikes and walking to school will be accommodated via the existing sidewalk system along Stribling Way. Once onsite, internal sidewalks and paths are provided that will allow for students to safely access both the elementary school and the middle/high school. Crossing guards and/or staff members will assist onsite to ensure pedestrian and bicycle safety.

8.0 ONSITE QUEUING AND COMPREHENSIVE TRAFFIC CONTROL

Extensive site planning and detailed analyses have been performed to minimize the potential for adverse impacts.

School faculty will typically arrive early and leave late and thus will have minimal impact with the student drop off and pick up times. The faculty parking lot is located in the parking garage on the west side of the school for Phase 2. For Phase 1, the faculty will primarily park in the surface parking lot east of the bus loop and the surface parking lot to the west of the track. A limited number of faculty and administrative parking spaces will be located in the parking lot in front of the elementary and middle/high school buildings.

As previously stated, parents (excluding early education) will utilize the State Road 7 entrance. Once parents turn into the school, they will drive west past the guard house. Parents will be required to have placards (or something similar) on their windshields to be granted access onsite. Once past the guardhouse, parents will circulate around the site before arriving at either the elementary or middle/high school drop off/pick line. The roadway from State Road 7 will be 3-lanes allowing for two ingress lanes and one egress lane. The two ingress lanes provide flexibility for the school for different operational situations. If needed, both ingress lanes can be used for vehicle queueing. If two lanes are utilized for vehicle queueing, they will be merged to one lane just prior to the school drop off area. The elementary school drop off will occur in front of the elementary school building on the north side of the site. The middle/high school drop off will occur on the ground floor of the parking garage for Phase 2 and on the east side of the surface parking lot in Phase 1. Faculty and student drivers will use a different entrance to the parking garage than the parent drop off line.

The visitor parking spaces will be located in front of the middle/high and elementary schools. During non-arrival and dismissal times, visitors will be able to enter the site via Stribling Way or State Road 7. During arrival and dismissal times, all visitors must enter via State Road 7. The purpose of this requirement is to ensure efficient operations and no vehicle spillover on Stribling Way.

A comprehensive Traffic Operational Plan graphic has been prepared and is attached in Appendix "C" with this report. The queue line for the middle/high school can accommodate approximately 15 vehicles in the drop off/pick up area and a total of approximately 151 vehicles onsite based on one vehicle every 22 feet. The additional queueing option utilizes the 2nd ingress lane allows for approximately an additional 110 vehicle stacking spaces. Therefore, the additional queueing option can accommodate approximately 261 ingress stacking spaces. It should be noted that based on the detailed queueing calculations provided below it is not expected that the additional queueing option would ever be needed. However, it is provided as an option and as a "fail-safe" and to provide additional support that vehicle queueing will not spill over onto State Road 7 or Stribling Way.

8.0 ONSITE QUEUING AND COMPREHENSIVE TRAFFIC CONTROL (CONTINUED)

The Elementary School drop off can accommodate approximately 18 vehicles in the drop off/pick up area and a total of approximately 214 vehicles onsite utilizing the State Road 7 entrance.

Trained Staff Members

School operations will utilize teachers, administration, and other staff members at the drop-off/pick-up area as follows (please refer to the Traffic Operational Plan exhibits attached with this report for the staff locations and additional details):

Loading/Unloading Personnel

Teachers, administration, and other staff members will assist with the loading and unloading of students. It is anticipated that approximately 8 staff members (1 per 2 vehicles) will assist in the process.

Traffic Control Personnel

Staff members will be located at key locations throughout the site to assist with traffic circulation. In some cases, this may involve stopping one direction of traffic to allow for another driveway to flow. The attached Traffic Operational Plan exhibits document the proposed locations where a staff member will be assigned for traffic control. However, these locations may only be needed for arrival or dismissal times and not both.

Bus Loop Personnel

A staff member will be located at the entrance to the access roadway for the bus loop. This staff member will assist in traffic control when necessary, such as in the afternoon to help vehicles and buses exit the driveway. This staff member will also ensure that only authorized vehicles enter the access roadway and not allow parents to drop off students at alternative locations.

Crossing Guard

A crossing guard and/or staff member will be located on the south side of the Stribling Way and school entrance round intersection and at the crossing location in front of the Village Center. The crossing guard will stop traffic as necessary to facilitate safety for pedestrians and cyclists. There is not expected to be a significant number of pedestrians or bicyclists entering the site due to the school being a private school and its location on State Road 7 at Stribling Way.

8.0 ONSITE QUEUING AND COMPREHENSIVE TRAFFIC CONTROL (CONTINUED)

Queuing Operations

Based on Palm Beach County Traffic guidelines, school queuing for a private school can be estimated at approximately 0.166 vehicles per student. Based on the previously documented staggering schedule, the maximum queue for each of the pickup time periods may be summarized as follows:

School Stacking Capacity

| Grade Level | Students | Queue Required (vehicles) | Queue Required (feet) | Approximate Queue Provided (vehicles/feet) |
|---|----------|---------------------------|-----------------------|--|
| Early Education (PreK-1 st Grade) | +/- 240 | 40 | 880 | 47 vehicles/ 1,030 feet |
| Elementary School (2 nd – 4 th Grade) | +/- 438 | 73 | 1,606 | 214 vehicles/ 4,720 feet |
| Middle School (5 th – 8 th Grade) | +/- 391 | 65 | 1,430 | 151 vehicles/ 3,340 feet (expandable to 261 vehicles/5,760 feet) |
| High School (9 th – 12 th Grade) | +/- 681 | 113 | 2,486 | 151 vehicles/ 3,340 feet (expandable to 261 vehicles/5,760 feet) |

The queue is estimated at one vehicle every 22 feet.

It is likely the early education queuing is overstated as many of the parents will elect to park and walk in/pick up their students instead of dropping off/picking up at the curbside. The high school queueing may also be overstated since many of the 11th and 12th grade students will have parking passes and drive to school.

As shown above, the proposed school queuing will be accommodated onsite with a significant amount of additional storage available and will not spill over to State Road 7 or Stribling Way.

9.0 STUDENT PARKING

Student parking will be available in the parking garage west of the middle/high school building. The school will designate 185 parking spaces for high school students, which includes 1 space for every 12th grader and 1 space for every ten 11th graders. The student parking spaces will be designated by number and students will be required to register their vehicle with the school and receive a parking pass.

10.0 OVERALL PARKING

The school is proposing the following parking metrics as part of the application:

Proposed Parking Metrics – Full Build Out (Phase 2)

| | |
|---|---------------------|
| 1 space for each faculty and staff member (426 faculty/staff members) | = 426 |
| 1 visitor space for every 50 students (1750 Students) | = 35 |
| 1 space for every 10 students in Grade 11 (168 Students) | = 17 |
| 1 space for every student in Grade 12 (168 Students) | = 168 |
| Total Parking Required | = 646 Spaces |

As shown above, the required parking based on the proposed parking metrics is 646 spaces. The school is currently proposing a total of 646 parking spaces as part of Phase 2. Note the number of proposed parking spaces may change slightly during the development process but will maintain at least 646 parking spaces. The majority of the parking will be located in the structured parking garage and the surface parking lot to the north of the school buildings. The parking locations are shown in Appendix C.

For Phase 1, the parking metrics are proposed as follows:

Proposed Parking Metrics – Phase 1

| | |
|---|---------------------|
| 1 space for each faculty and staff member (275 faculty/staff members) | = 275 |
| 1 visitor space for every 50 students (1008 Students) | = 21 |
| 1 space for every 10 students in Grade 11 (96 Students) | = 10 |
| 1 space for every student in Grade 12 (96 Students) | = 96 |
| Total Parking Required | = 402 Spaces |

A minimum of 402 parking spaces will be provided as part of the Phase 1 development.

11.0 AFTER SCHOOL ACTIVITIES

Many students will stay after school on certain days as the school offers a variety of afterschool activities and programs. The afterschool activities will help disperse peak afternoon traffic and lessen the onsite and offsite traffic impact.

12.0 EVENT PARKING

As shown in the Site Plan, 646 parking spaces are provided onsite. However, since the vehicle queueing line provides two ingress lanes, the outside lane can be used for event parking if ever desired. The bus loop can also be used in a similar manner to allow for additional parking. Events will be carefully planned to ensure that sufficient parking is available. The majority of events will be staggered by grade levels by day and/or time to ensure the event parking can be accommodated. Staff members will be present prior to any event to assist with the additional parking. The additional outside stacking lane along with the bus loop can accommodate an additional 148 parking spaces if needed for events.

13.0 TRAFFIC CONTROL MONITORING

The school is committed to monitoring the onsite traffic operations to ensure the drop off and pick up process moves efficiently and safely. The Operational Management Plan will be reviewed by the school annually. If any changes to the traffic operations are determined to be necessary, the school will make the necessary operational adjustments and inform the Village.

14.0 RAMP UP SCHEDULE

The school will not open at full enrollment in the first year of operations. Instead, the school will ramp up the number of students over several years until full enrollment is reached. The slow ramp up will allow the school to make any traffic or other operational improvements necessary early on prior to full enrollment.

15.0 GATEHOUSES

A gatehouse will be provided for security purposes at the entrance to both Stribling Way and State Road 7. During student arrival and dismissal times, the gates will operate in a way to allow for free and efficient traffic flow. During non-peak times, the gates will be closed and require security clearance for guests. The gatehouses will be staffed during school hours.

16.0 CONCLUSION

The following summary of items has been presented as part of the overall Operational Plan for the school:

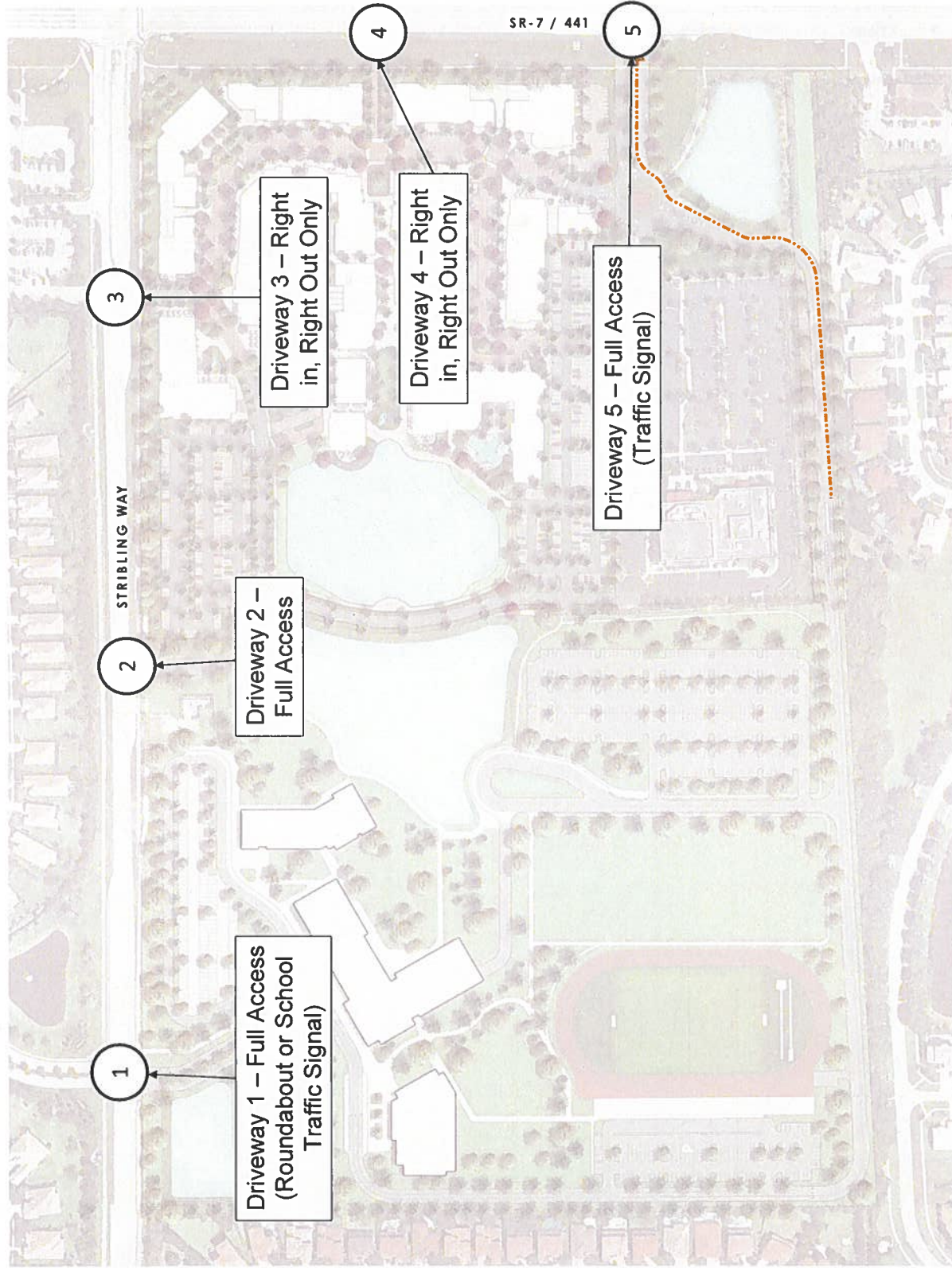
1. Trained staff will be utilized to ensure the efficient flow of traffic throughout the site.
2. Controlled delivery schedules at off-peak time periods, completely outside the peak drop-off and pick-up time periods.
3. Parents and students will be instructed each year on the proper use of crosswalks and safe walking and biking habits.
4. The school is proposing to have multiple staggered start and end times to reduce peak onsite and offsite traffic.
5. The Site Plan has been designed to accommodate substantial onsite vehicle queuing.
6. Employees, student drivers, and Grades 2 through 12 will arrive from State Road 7 ensuring no spillover onto Stribling Way.
7. The applicant will monitor traffic operations and will provide any required operational adjustments if needed.
8. Events will be carefully planned and typically staggered by grade levels so that sufficient parking is provided.

Pb X:/docs/trafficdrainage/25024.operationalstudy.rev3



APPENDIX A

DRIVEWAY ACCESS EXHIBIT



Overall Driveway Access | School and Mixed Use



APPENDIX B

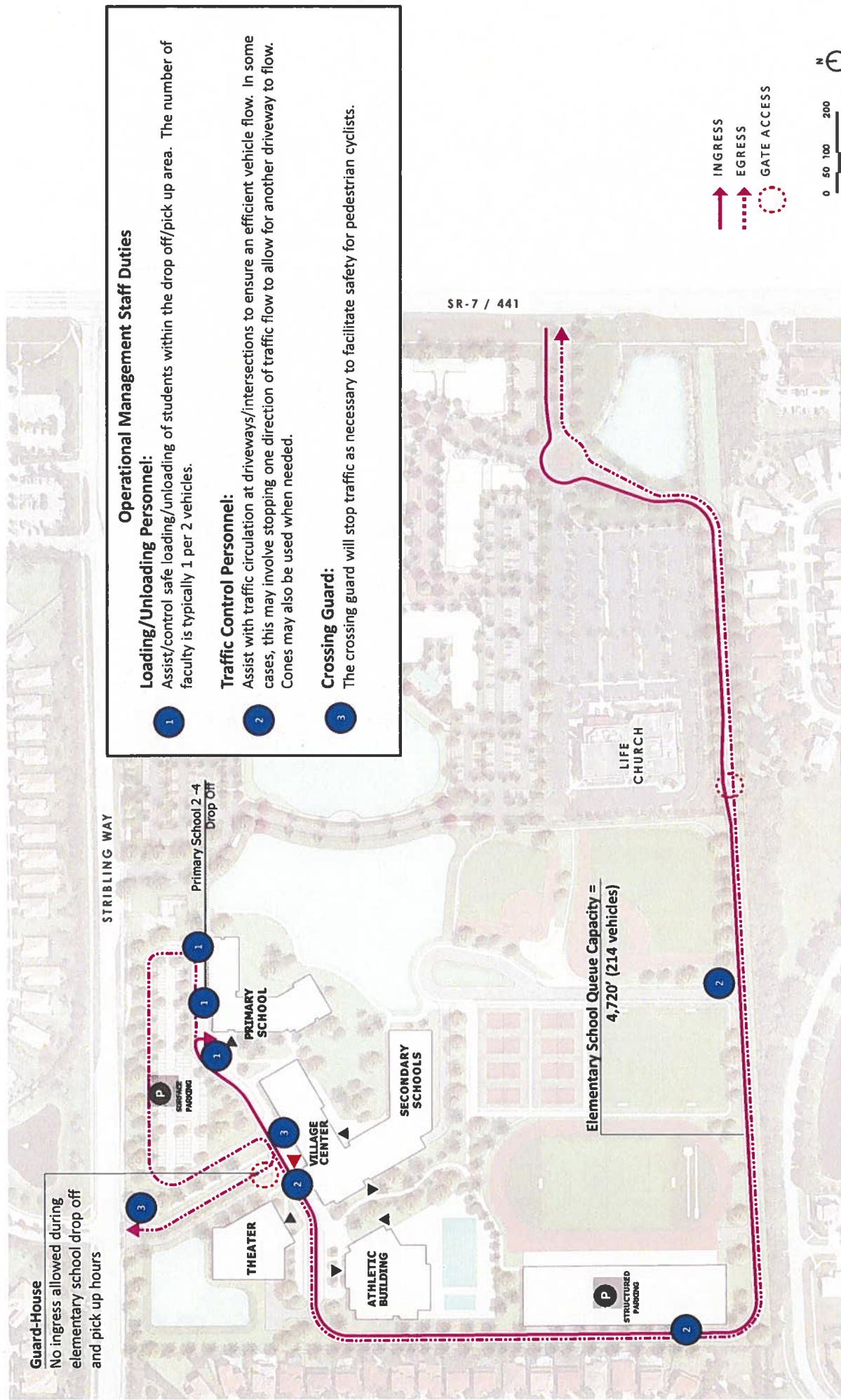
BUS CIRCULATION ROUTES



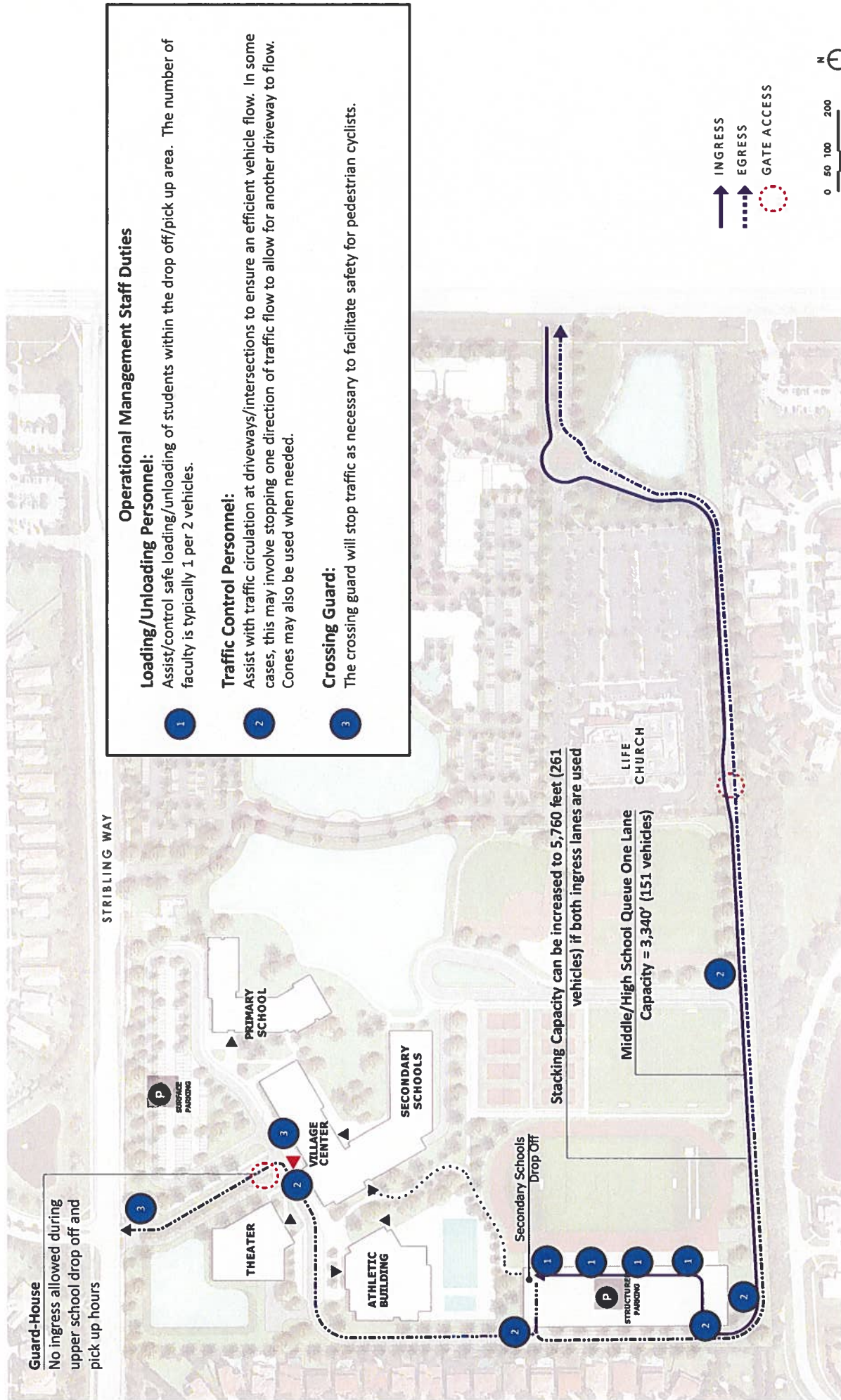
APPENDIX C

OPERATIONAL PLAN – FULL BUILDOUT

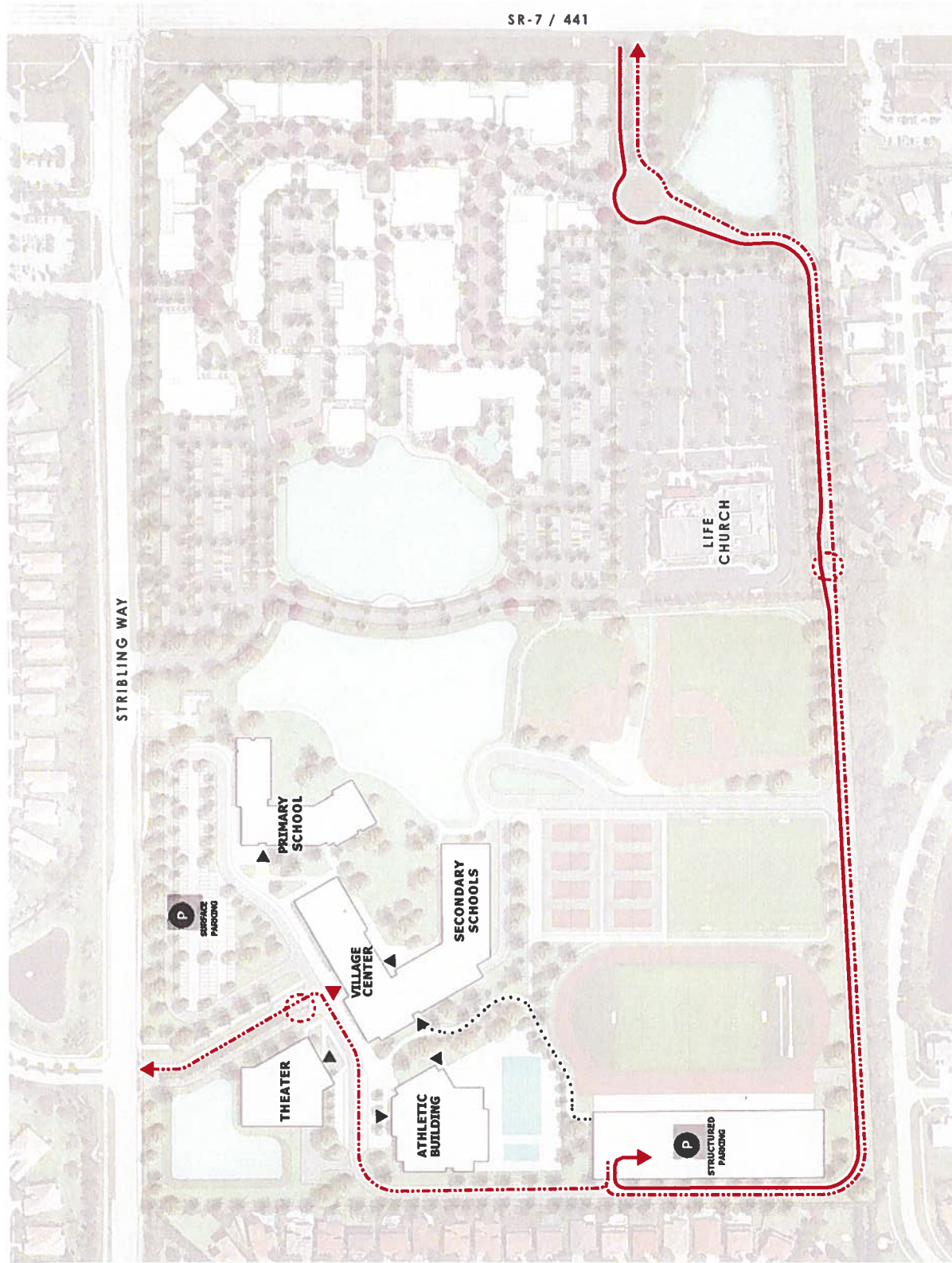




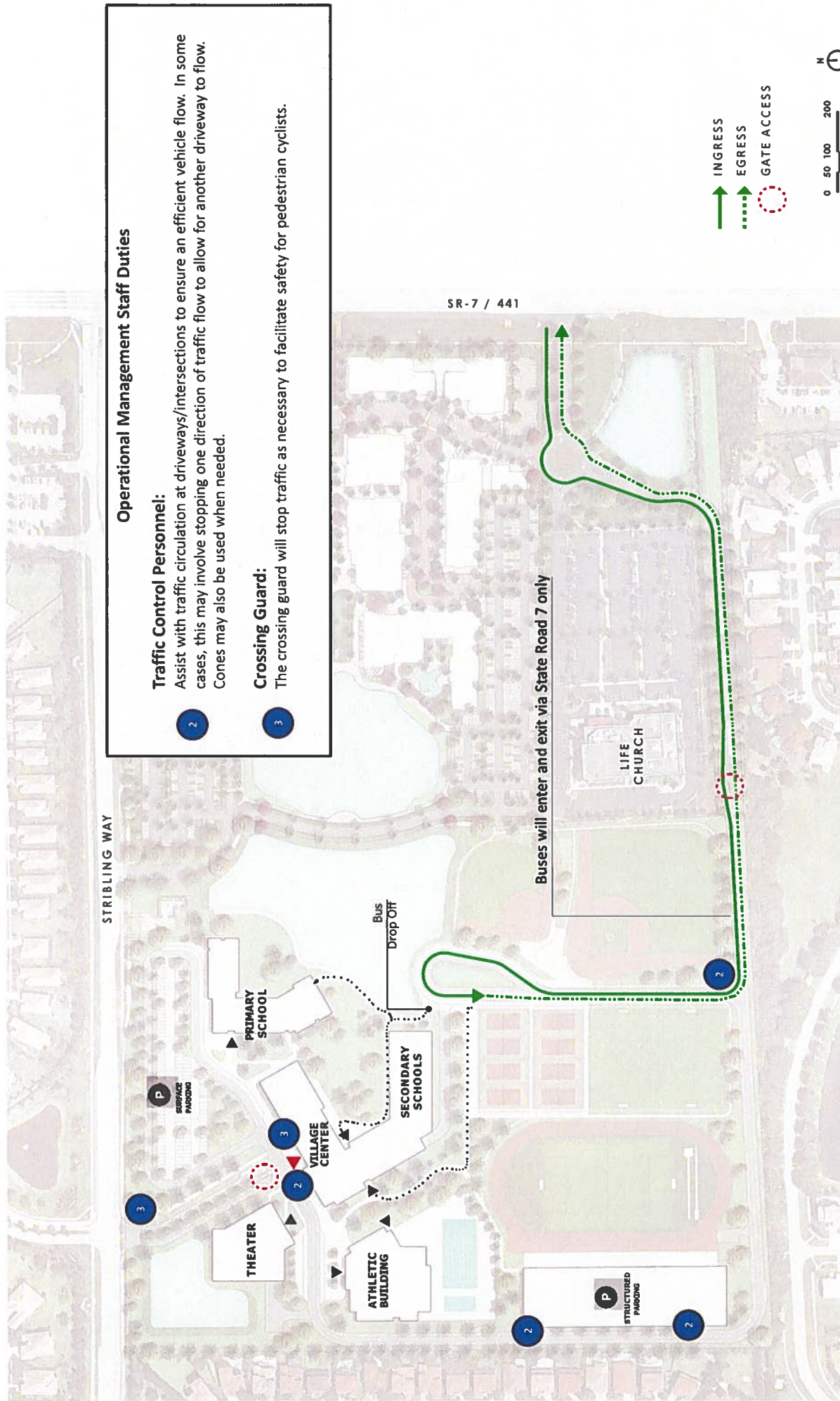
Full Build Site Circulation | Primary School 2 - 4 Drop-off



Full Build Site Circulation | Secondary Schools Drop-off



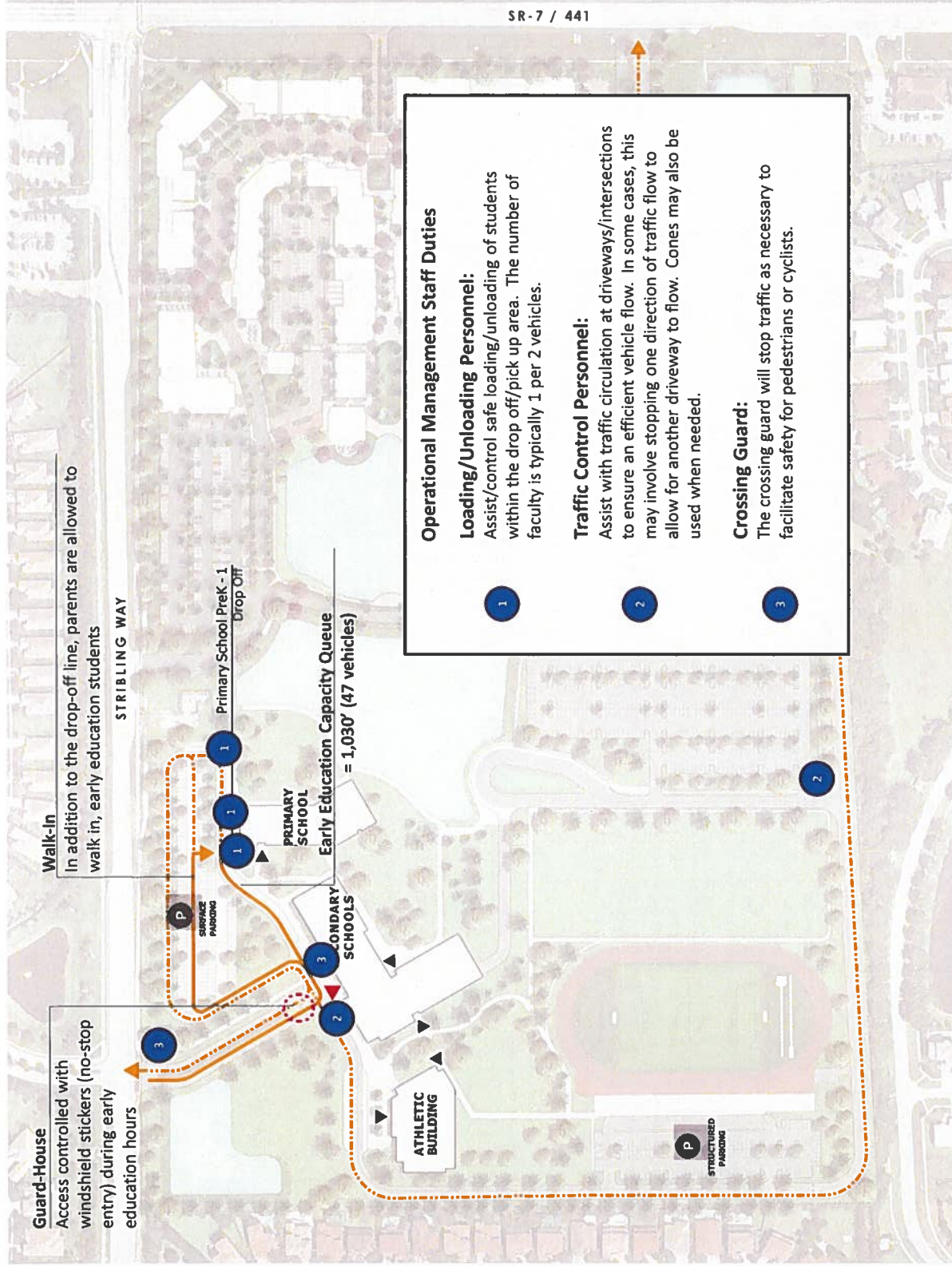
Full Build Site Circulation | Staff and Student Parking



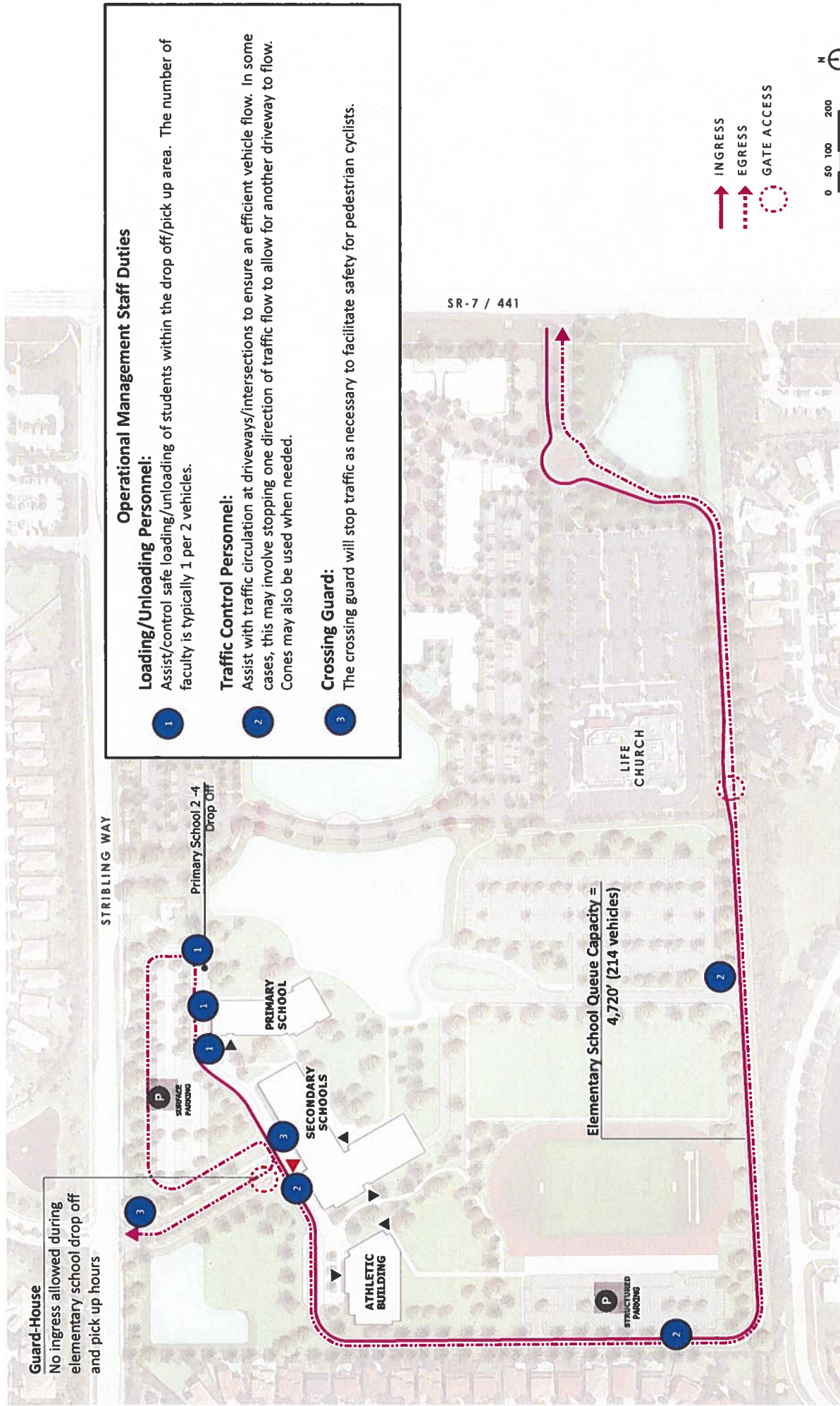
Full Build Site Circulation | Bus Drop-off



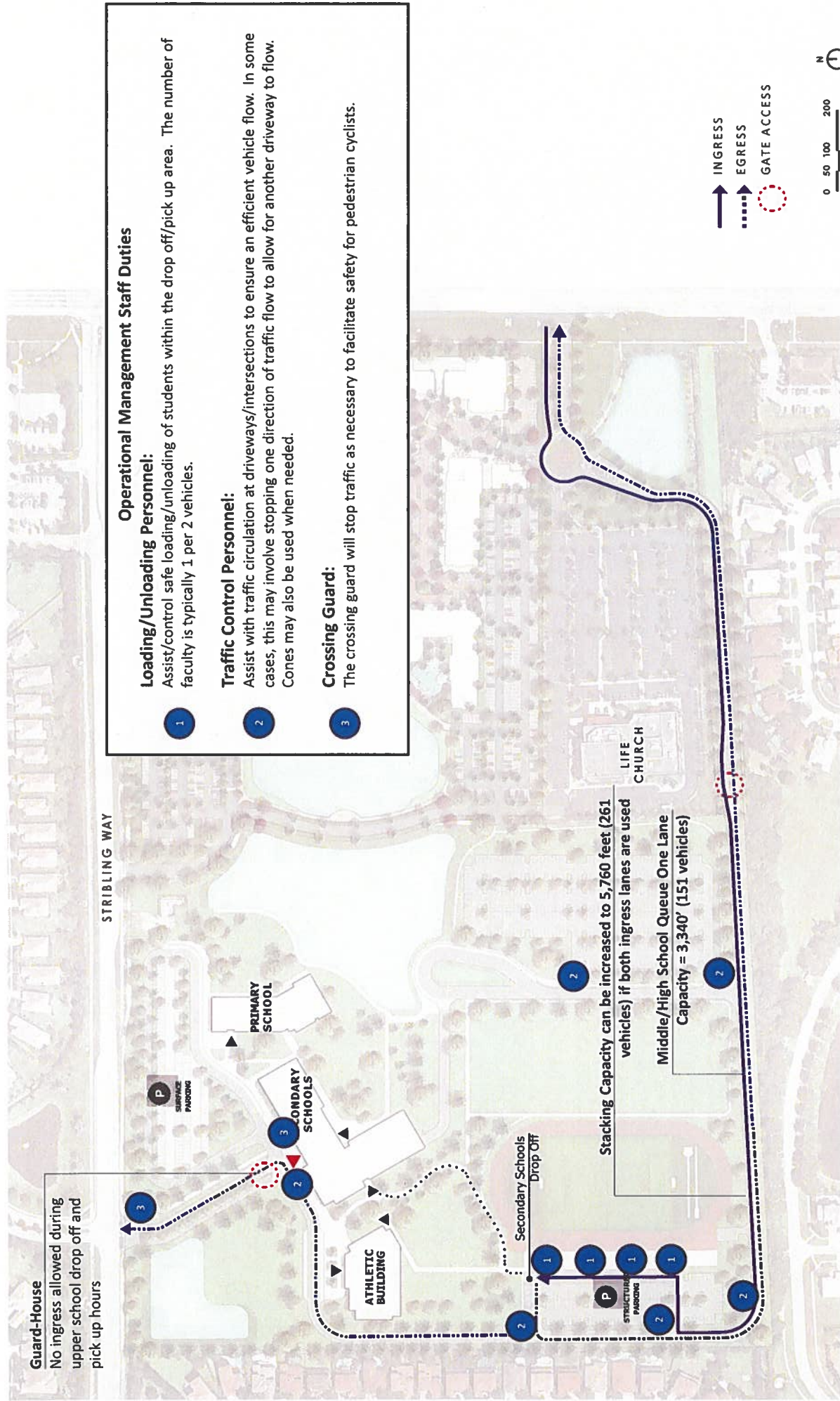
OPERATIONAL PLAN – PHASE 1



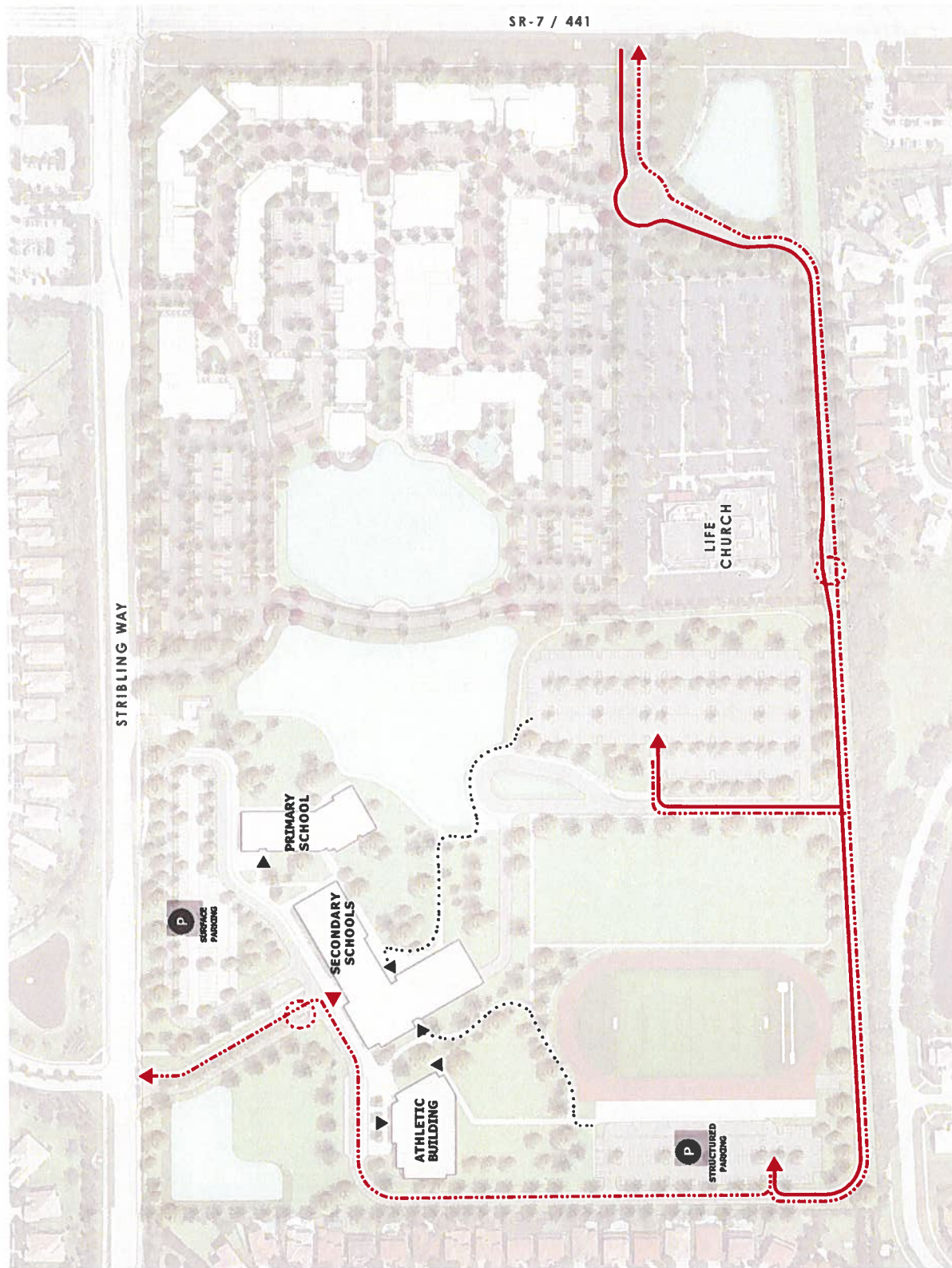
Phase 1 Site Circulation | Primary School PreK - 1 Drop-off



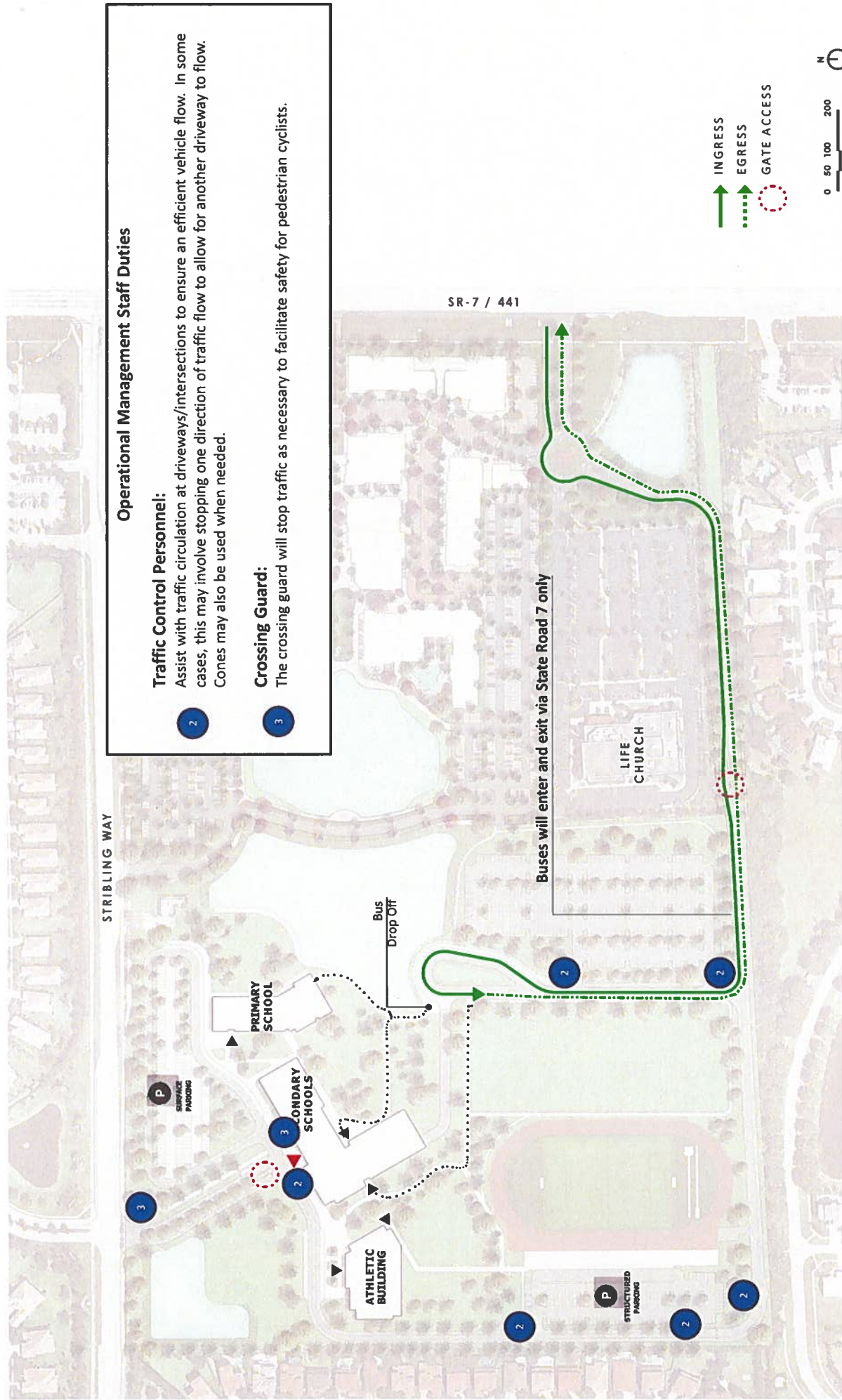
Phase 1 Site Circulation | Primary School 2 - 4 Drop-off



Phase 1 Site Circulation | Secondary Schools Drop-off



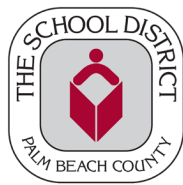
Phase 1 Site Circulation | Staff and Student Parking



Phase 1 Site Circulation | Bus Drop-off







THE SCHOOL DISTRICT OF
PALM BEACH COUNTY, FL

KRISTIN K. GARRISON, AICP
DIRECTOR

MICHAEL J. BURKE
SUPERINTENDENT

PLANNING & INTERGOVERNMENTAL RELATIONS
3661 INTERSTATE PARK ROAD NORTH, 200
RIVIERA BEACH, FL 33404

JOSEPH M. SANCHES, MBA
CHIEF OPERATING OFFICER

PHONE: 561-434-8020 / FAX: 561- 434-8942
WWW.PALMBEACHSCHOOLS.ORG

SCHOOL CAPACITY AVAILABILITY DETERMINATION (SCAD)

| | | | | |
|---|---|--|----------------------------|---------------------------|
| Application | Submittal Date | 11/21/2025 | | |
| | SCAD No. | 25112101Z/Rezoning and 25112101D/D. O. | | |
| | FLU /Rezoning/D.O. No. | 2025-0003-REZ/2025-0001-MP – Village of Wellington | | |
| | PCN No. / Address | 73-42-43-27-05-026-0011; 73-41-44-24-06-003-0000/ 10400 Stribling Way | | |
| | Development Name | K-Park MUPD - PH 1 | | |
| | Owner / Agent Name | ACME Improvement District / Urban Design Studio | | |
| | SAC No. | 179D | | |
| | Proposed Rezoning Proposed D. O. | Maximum 356 Residential Units (PH 1 and PH 2) 15 Apartment Units | | |
| Impact Review | | Panther Run Elementary School | Polo Park Middle School | Wellington High School |
| | New Students Generated | 3 | 2 | 2 |
| | Capacity Available | 187 | 273 | -118 |
| | Projected Utilization | 81% | 81% | 104% |
| School District Staff's Recommendation | <p>Based on the findings and evaluation of the proposed development, there will be a negative impact on the public-school system. Given the recent increases in school impact fees, effective January 1, 2023, much of these impacts will be mitigated. The impact fees, however, will not fully cover impacts to the school system. Therefore, if the proposed development is approved by Village Council and if the Developer voluntarily agrees, School District staff recommends the following condition to mitigate such impacts.</p> <p>In order to address the school capacity deficiency generated by this proposed development at the District high school level, the property owner shall contribute \$31,346.00 to the School District of Palm Beach County prior to the issuance of first building permit.</p> <p>This voluntary school capacity contribution is intended to supplement the required school impact fee (impact fee credit has already been applied).</p> <p>Please note that the school impact fee credit is calculated based on the Net Impact Cost per Student, as calculated in the County's latest Impact Fee Ordinance, which was adopted on September 13, 2022.</p> | | | |
| Validation Period | <p>1) This determination is valid from 12/11/2025 to 12/10/2026 or the expiration date of the site-specific development order approved during the validation period.</p> <p>2) A copy of the approved D.O. must be submitted to the School District Planning Dept. prior to 12/10/2026 or this determination will expire automatically on 12/10/2026.</p> | | | |
| Notice | <p>School age children may not necessarily be assigned to the public school closest to their residences. Students in Palm Beach County are assigned annually to schools under the authority of the School Board and by direction of the Superintendent, public school attendance zones are subject to change.</p> | | | |

Joyce Cai

School District Representative Signature

Joyce C. Cai, Senior Planner

Print Name & Title

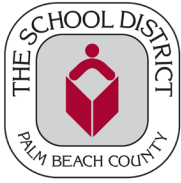
December 11, 2025

Date

joyce.cai@palmbeachschools.org

Email Address

CC: Kelly Ferraiolo, Senior Planner, Village of Wellington
Joyell Shaw, PIR Manager, School District of Palm Beach County



THE SCHOOL DISTRICT OF
PALM BEACH COUNTY, FL

KRISTIN K. GARRISON, AICP
DIRECTOR

MICHAEL J. BURKE
SUPERINTENDENT

PLANNING & INTERGOVERNMENTAL RELATIONS
3661 INTERSTATE PARK ROAD NORTH, 200
RIVIERA BEACH, FL 33404

JOSEPH M. SANCHES, MBA
CHIEF OPERATING OFFICER

PHONE: 561-434-8020 / FAX: 561- 434-8942
WWW.PALMBEACHSCHOOLS.ORG

SCHOOL CAPACITY AVAILABILITY DETERMINATION (SCAD)

| | | | | |
|---|---|---|----------------------------|---------------------------|
| Application | Submittal Date | 11/21/2025 | | |
| | SCAD No. | 25112102Z/Rezoning and 25112102D/D. O. | | |
| | FLU /Rezoning/D.O. No. | 2025-0003-REZ/2025-0001-MP – Village of Wellington | | |
| | PCN No. / Address | 73-42-43-27-05-026-0011; 73-41-44-24-06-003-0000/ 10400 Stribling Way | | |
| | Development Name | K-Park MUPD - PH 2 | | |
| | Owner / Agent Name | ACME Improvement District / Urban Design Studio | | |
| | SAC No. | 179D | | |
| | Proposed Rezoning Proposed D. O. | Maximum 356 Residential Units (PH 1 and PH 2) 200 Mid-Rise Apt/Condo Units | | |
| Impact Review | | Panther Run Elementary School | Polo Park Middle School | Wellington High School |
| | New Students Generated | 6 | 3 | 4 |
| | Capacity Available | 181 | 270 | -122 |
| | Projected Utilization | 82% | 81% | 105% |
| School District Staff's Recommendation | <p>Based on the findings and evaluation of the proposed development, there will be a negative impact on the public-school system. Given the recent increases in school impact fees, effective January 1, 2023, much of these impacts will be mitigated. The impact fees, however, will not fully cover impacts to the school system. Therefore, if the proposed development is approved by Village Council and if the Developer voluntarily agrees, School District staff recommends the following condition to mitigate such impacts.</p> <p>In order to address the school capacity deficiency generated by this proposed development at the District high school level, the property owner shall contribute \$62,692.00 to the School District of Palm Beach County prior to the issuance of first building permit.</p> <p>This voluntary school capacity contribution is intended to supplement the required school impact fee (impact fee credit has already been applied).</p> <p>Please note that the school impact fee credit is calculated based on the Net Impact Cost per Student, as calculated in the County's latest Impact Fee Ordinance, which was adopted on September 13, 2022.</p> | | | |
| Validation Period | <p>1) This determination is valid from 12/11/2025 to 12/10/2026 or the expiration date of the site-specific development order approved during the validation period.</p> <p>2) A copy of the approved D.O. must be submitted to the School District Planning Dept. prior to 12/10/2026 or this determination will expire automatically on 12/10/2026.</p> | | | |
| Notice | <p>School age children may not necessarily be assigned to the public school closest to their residences. Students in Palm Beach County are assigned annually to schools under the authority of the School Board and by direction of the Superintendent, public school attendance zones are subject to change.</p> | | | |

Joyce Cai

School District Representative Signature

Joyce C. Cai, Senior Planner

Print Name & Title

December 11, 2025

Date

joyce.cai@palmbeachschools.org

Email Address

CC: Kelly Ferraiolo, Senior Planner, Village of Wellington
Joyell Shaw, PIR Manager, School District of Palm Beach County

November 07, 2025

RE: Wellington Village - Approvals
LB Project № 0100062928
Building Logistics Analysis Support Letter

Dear Village of Wellington Review Team,

Lerch Bates has been working with the Elkus Manfredi Architects to complete a detailed assessment of the planned loading dock spaces and service areas for each zone / group of buildings within the development. The analysis includes all retail, food and beverage, residential, hotel, and event program components. The study evaluates anticipated operational demand, delivery frequencies, vehicle types, tenant mix, and back-of-house circulation needs. Based on the latest review, the proposed loading dock space / area quantities are sufficient to support daily operations without adverse impact to on-site circulation. As the project progresses and tenant requirements continue to develop, Lerch Bates will update the analysis and coordinate with the design team to confirm that the planned loading accommodations continue to meet capacity needs.

In addition, a development-wide waste generation analysis has been performed to confirm that projected municipal solid waste and cardboard volumes can be adequately managed within the localized waste storage and handling areas planned in each zone, in conjunction with a centralized waste collection strategy currently being evaluated. Under this approach, most municipal solid waste and cardboard generated throughout the village will be transported from localized storage areas to a central waste collection building, allowing waste haulers to access a single controlled location and minimizing waste truck movement throughout the site. A few zones—specifically those containing hotel, event, or grocery uses—are anticipated to require direct hauler access due to higher waste generation rates. This analysis will also be updated as the project progresses, and the program of each building / tenant is updated.

As summarized in the attached documentation, the proposed loading dock space /area quantities support safe and efficient operations. The waste volume and container quantity analysis based on the program received in October is also included. We respectfully submit this information in support of the proposed loading dock plan and waste management strategy.

Best Regards,

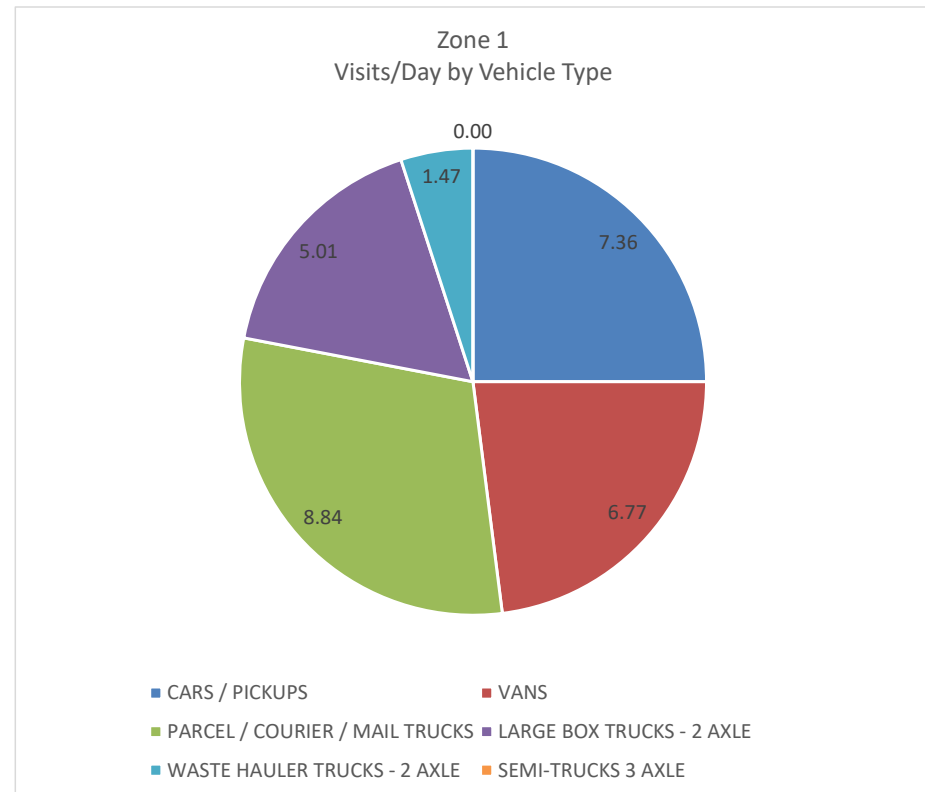


Prepared by:

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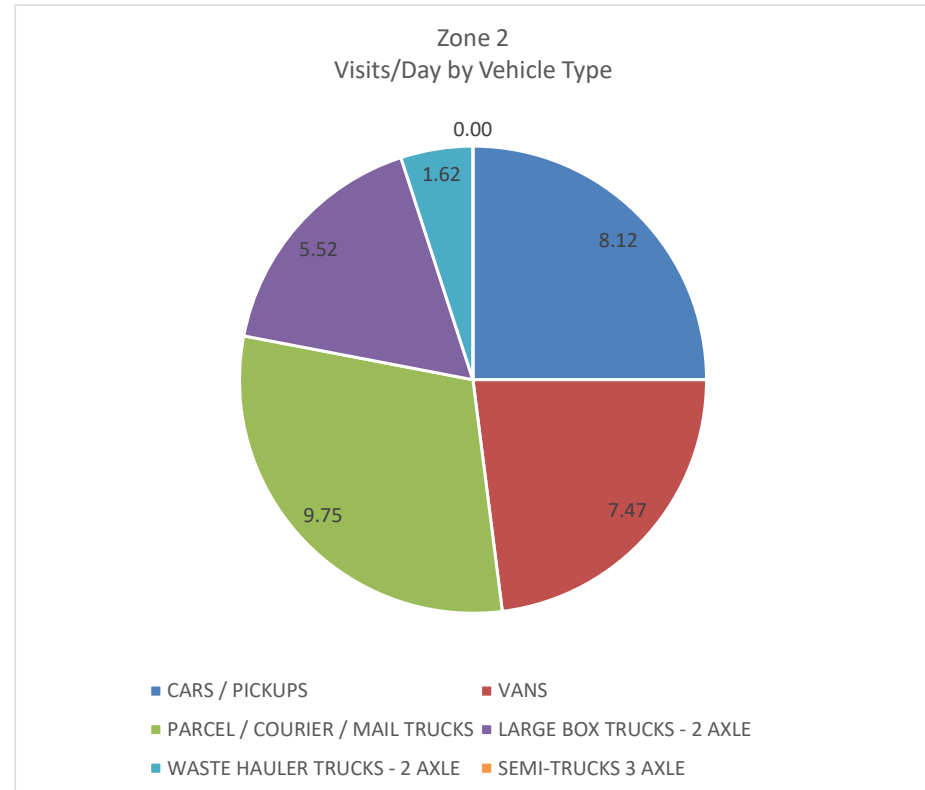
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 25% | CARS / PICKUPS | 7.36 |
| 23% | VANS | 6.77 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 8.84 |
| 17% | LARGE BOX TRUCKS - 2 AXLE | 5.01 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.47 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 29.45 |

| ZONE 1 | |
|-------------------|----|
| TOTAL BAYS | 2 |
| TOTAL TRIPS / DAY | 30 |



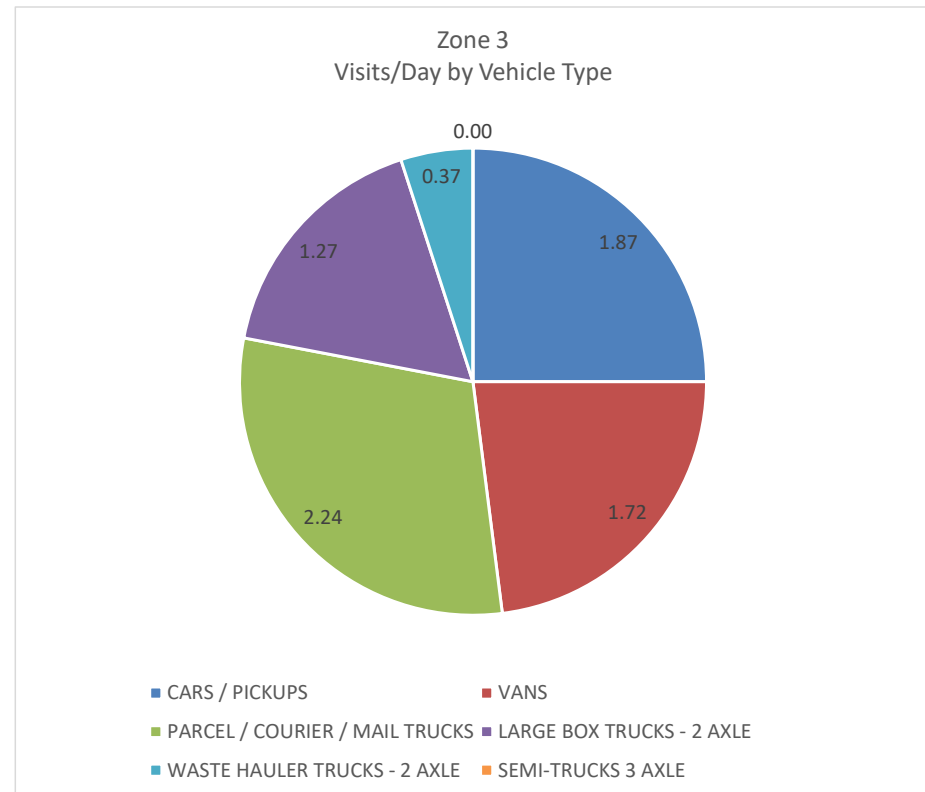
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 25% | CARS / PICKUPS | 8.12 |
| 23% | VANS | 7.47 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 9.75 |
| 17% | LARGE BOX TRUCKS - 2 AXLE | 5.52 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.62 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 32.49 |

| ZONE 2 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 33 |



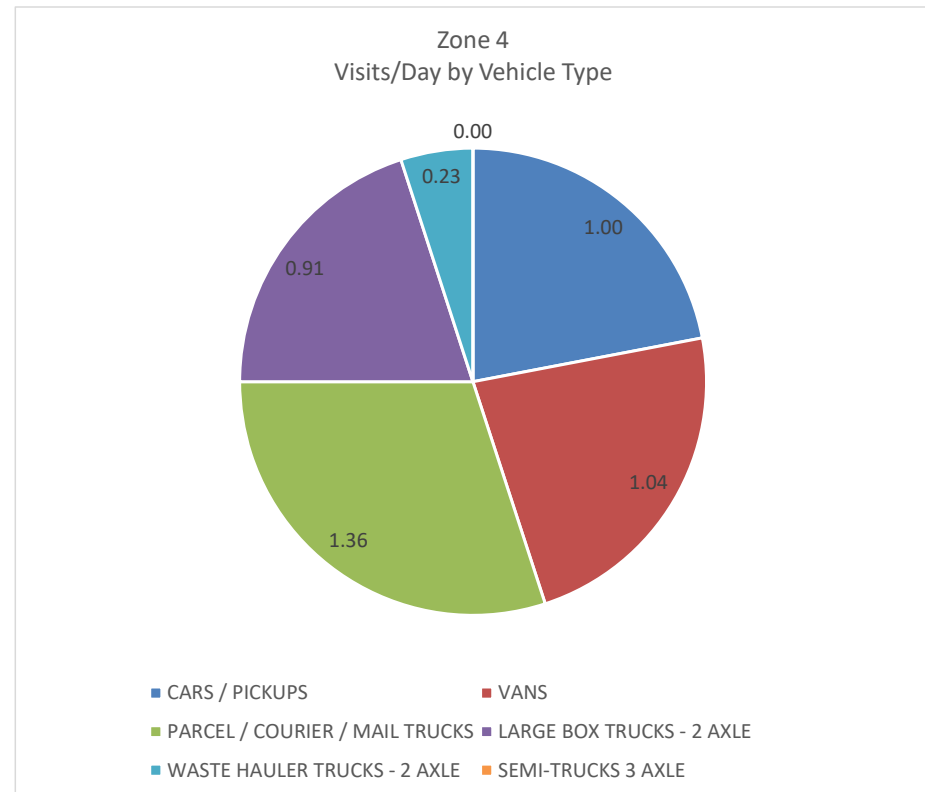
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 25% | CARS / PICKUPS | 1.87 |
| 23% | VANS | 1.72 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 2.24 |
| 17% | LARGE BOX TRUCKS - 2 AXLE | 1.27 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 0.37 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 7.48 |

| ZONE 3 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 8 |



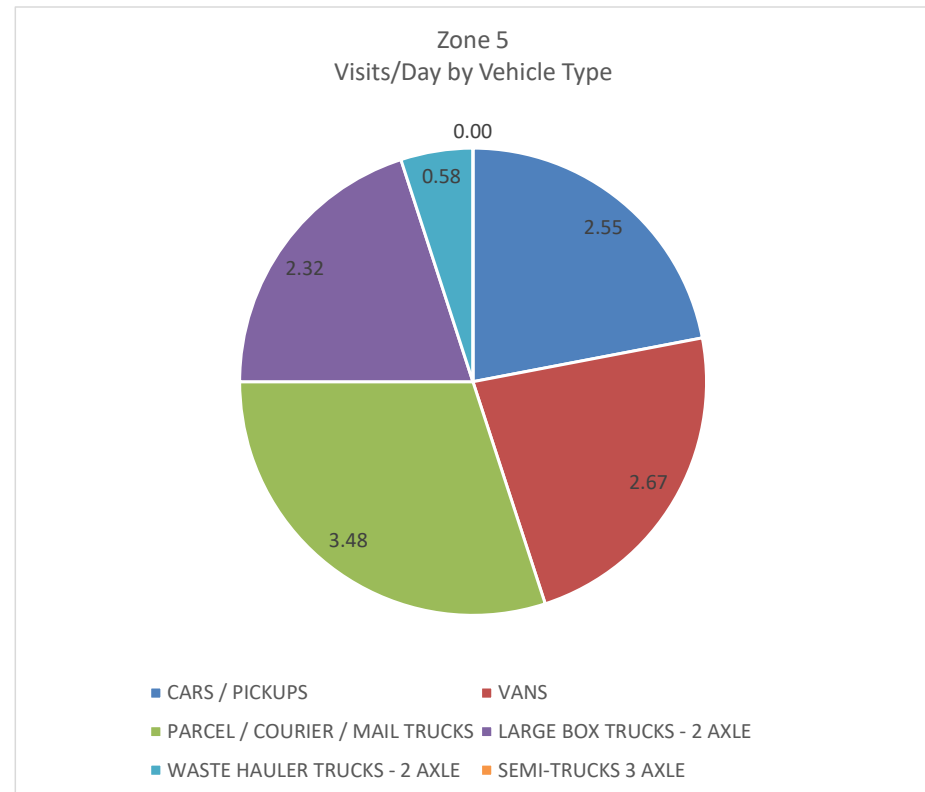
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 1.00 |
| 23% | VANS | 1.04 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 1.36 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 0.91 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 0.23 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 4.54 |

| ZONE 4 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 5 |



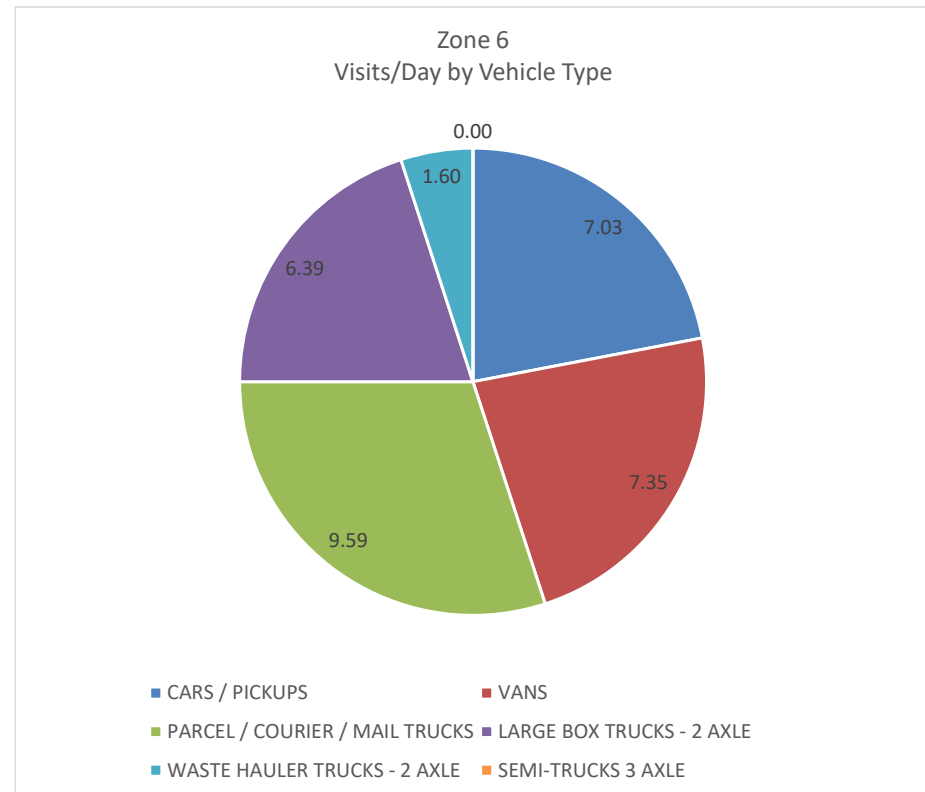
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 2.55 |
| 23% | VANS | 2.67 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 3.48 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 2.32 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 0.58 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 11.61 |

| ZONE 5 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 12 |



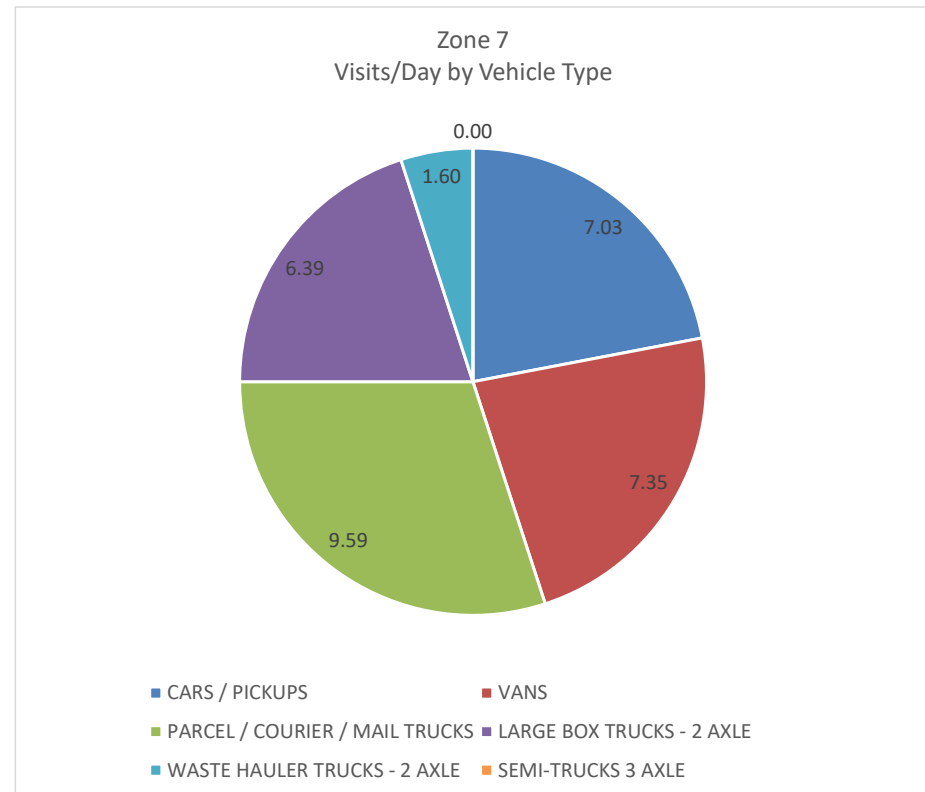
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 7.03 |
| 23% | VANS | 7.35 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 9.59 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 6.39 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.60 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 31.96 |

| ZONE 6 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 32 |



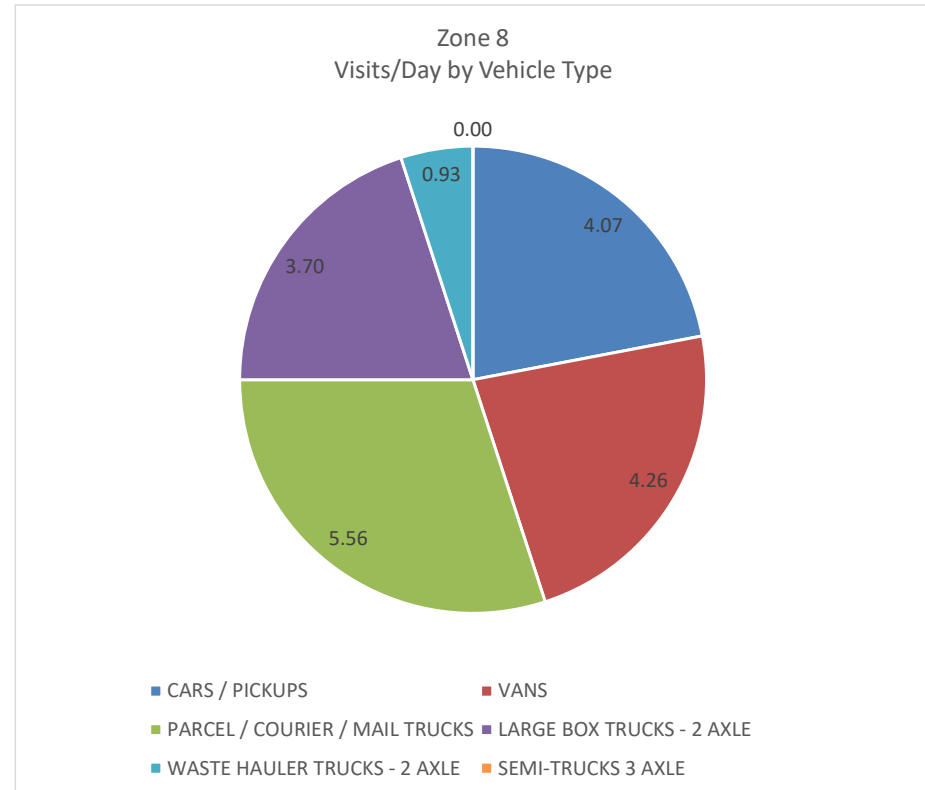
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 7.03 |
| 23% | VANS | 7.35 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 9.59 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 6.39 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.60 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 31.96 |

| ZONE 7 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 32 |



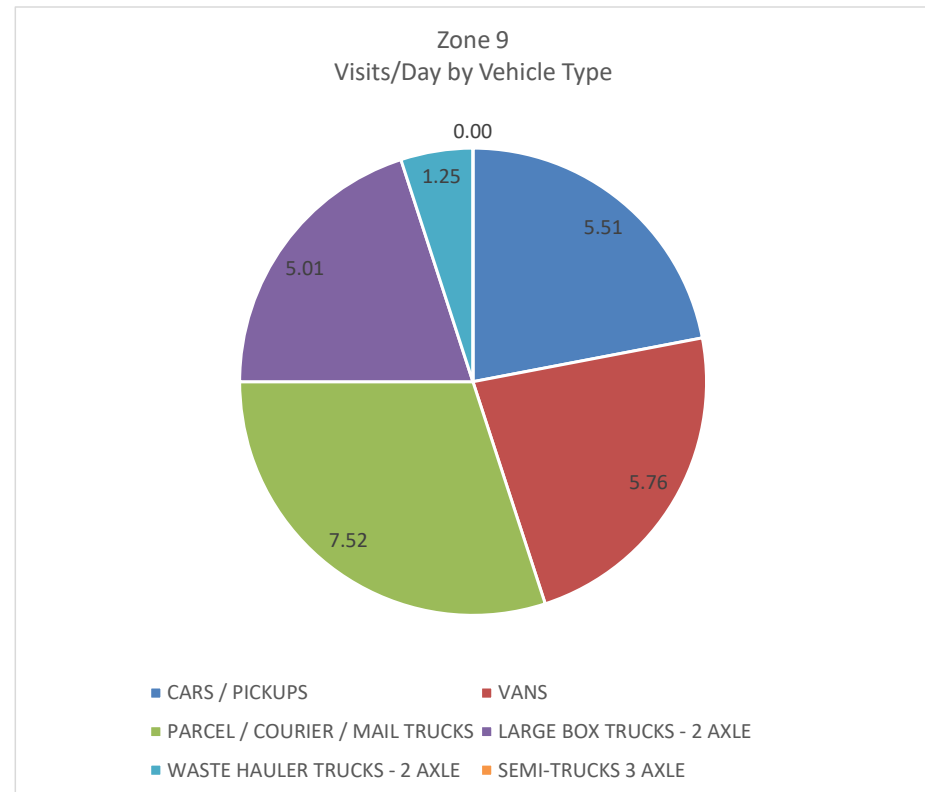
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 4.07 |
| 23% | VANS | 4.26 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 5.56 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 3.70 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 0.93 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 18.52 |

| ZONE 8 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 19 |



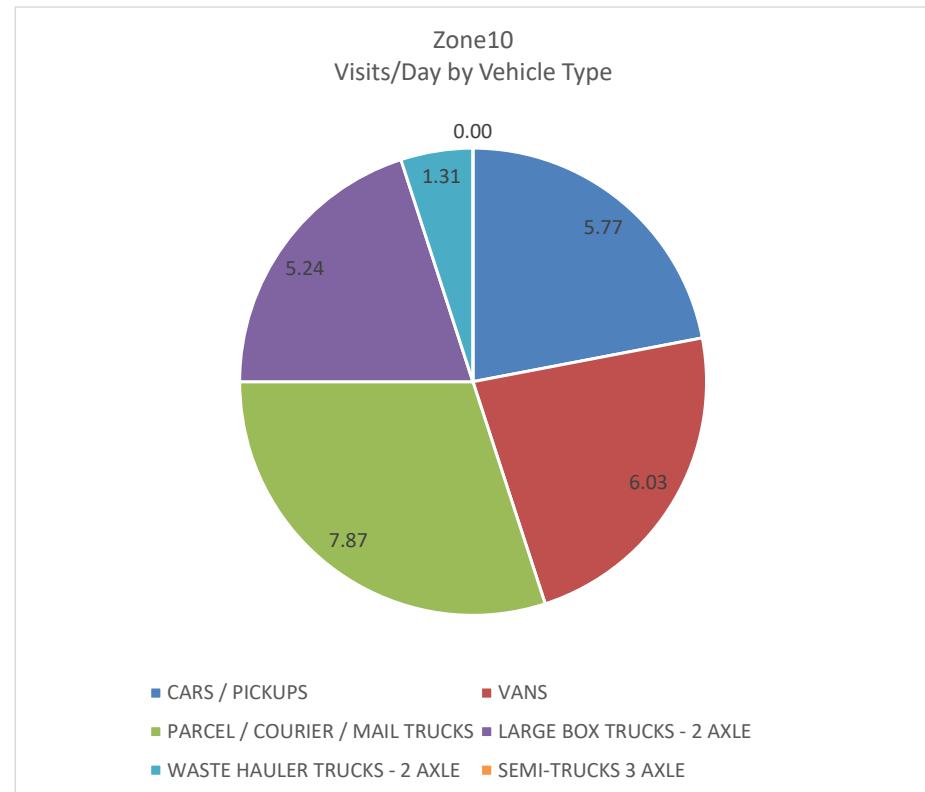
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 5.51 |
| 23% | VANS | 5.76 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 7.52 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 5.01 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.25 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 25.05 |

| ZONE 9 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 26 |



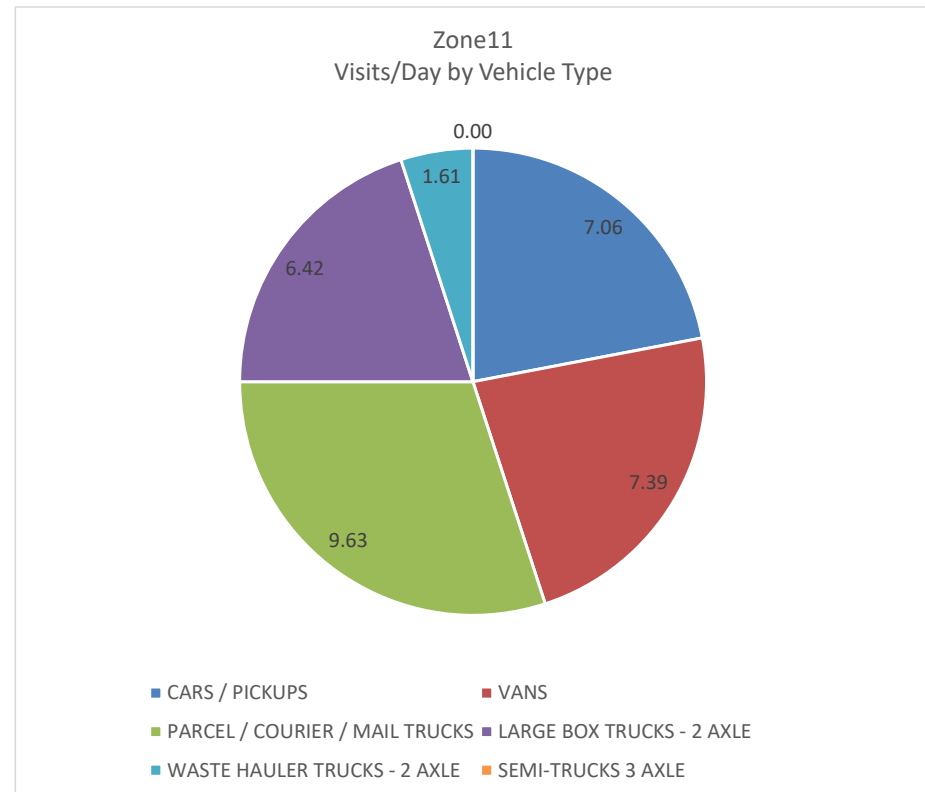
| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 5.77 |
| 23% | VANS | 6.03 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 7.87 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 5.24 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.31 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 26.22 |

| ZONE 10 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 27 |



| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 7.06 |
| 23% | VANS | 7.39 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 9.63 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 6.42 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 1.61 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 32.11 |

| ZONE 11 | |
|-------------------|------|
| TOTAL BAYS | 2.00 |
| TOTAL TRIPS / DAY | 33 |

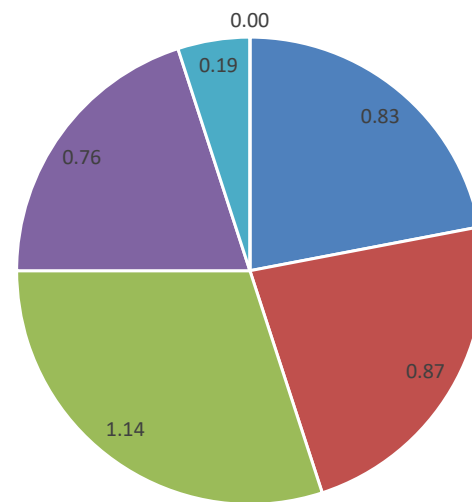


| SERVICE VEHICLE ACTIVITY BY VEHICLE TYPE | | |
|--|--------------------------------|-------------|
| % / DAY | VEHICLE TYPE | VISITS/DAY |
| 22% | CARS / PICKUPS | 0.83 |
| 23% | VANS | 0.87 |
| 30% | PARCEL / COURIER / MAIL TRUCKS | 1.14 |
| 20% | LARGE BOX TRUCKS - 2 AXLE | 0.76 |
| 5% | WASTE HAULER TRUCKS - 2 AXLE | 0.19 |
| 0% | SEMI-TRUCKS 3 AXLE | 0.00 |
| TOTAL | | 3.78 |

100%

| ZONE 12 | |
|-------------------|-------------|
| TOTAL BAYS | 1.00 |
| TOTAL TRIPS / DAY | 4 |

Zone 12
Visits/Day by Vehicle Type



■ CARS / PICKUPS ■ VANS
 ■ PARCEL / COURIER / MAIL TRUCKS ■ LARGE BOX TRUCKS - 2 AXLE
 ■ WASTE HAULER TRUCKS - 2 AXLE ■ SEMI-TRUCKS 3 AXLE

WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|-----------------|-----------------------|-----------------|-----------------------|
| Zone 1 | 1,279.75 | 924.93 | lbs / day | 347.86 | lbs / day | 6.96 | lbs / day |
| Zone 2 | 1,458.02 | 1,103.81 | lbs / day | 346.85 | lbs / day | 7.36 | lbs / day |
| Zone 3 | 1,256.17 | 961.81 | lbs / day | 287.95 | lbs / day | 6.41 | lbs / day |
| Zone 4 | 798.42 | 595.84 | lbs / day | 198.61 | lbs / day | 3.97 | lbs / day |
| Zone 5 | 1,116.86 | 870.24 | lbs / day | 240.82 | lbs / day | 5.80 | lbs / day |
| Zone 6 | 896.29 | 668.87 | lbs / day | 222.96 | lbs / day | 4.46 | lbs / day |
| Zone 7 | 816.91 | 609.64 | lbs / day | 203.21 | lbs / day | 4.06 | lbs / day |
| Zone 8 | 1,076.06 | 816.87 | lbs / day | 253.74 | lbs / day | 5.45 | lbs / day |
| Zone 9 | 1,394.53 | 1,040.70 | lbs / day | 346.90 | lbs / day | 6.94 | lbs / day |
| Zone 10 | 889.35 | 663.69 | lbs / day | 221.23 | lbs / day | 4.42 | lbs / day |
| Zone 11 | 1,083.87 | 808.85 | lbs / day | 269.62 | lbs / day | 5.39 | lbs / day |
| Zone 12 | 566.85 | 408.49 | lbs / day | 155.25 | lbs / day | 3.10 | lbs / day |
| TOTAL POUNDS / DAY | 11,353.32 | 8,548.81 | lbs / day | 2,747.14 | lbs / day | 57.37 | lbs / day |
| TOTAL CUBIC YARDS | 6.07 | 4.39 | loose cy / day | 1.65 | loose cy / day | 0.03 | loose cy / day |

| ZONE 1 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY | MUNICIPAL SOLID WASTE | CARDBOARD | UNIVERSAL WASTE |
|--|--------------------------|--------------------------|---------------------|
| LOOSE OR COMPACTED? / UNPROCESSED / PROCESSED | COMPACTED | COMPACTED | LOOSE |
| COMPACTOR / EQUIPMENT REQUIRED: | VERTICAL COMPACTOR | VERTICAL COMPACTOR | NONE |
| CONTAINER TYPE | SELF-CONTAINED COMPACTOR | SELF-CONTAINED COMPACTOR | GAYLORD BOX |
| CONTAINER SIZE: | 34.00 CY | 34.00 CY | 1.50 CY |
| CONTAINER QUANTITY: | 2.00 | 1.00 | 1.00 |
| FREQUENCY OF PICKUP: | 3 TIMES PER WEEK | 3.00 TIMES PER WEEK | 1.00 TIMES PER YEAR |

**ZONE 1 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)**

| ZONE 1 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|----------------------------------|------------------------------|-----------------------|------------------|-----------------------|------------------------|-----------------------|
| Zone 1 | 1,279.75 | 924.93 | lbs / day | 347.86 | lbs / day | 6.96 | lbs / day |
| TOTAL POUNDS / DAY | 1,279.75 | 924.93 | lbs / day | 347.86 | lbs / day | 6.96 | lbs / day |
| TOTAL CUBIC YARDS | 6.07 | 4.39 | loose cy / day | 1.65 | loose cy / day | 0.03 | loose cy / day |

| ZONE 1 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|------------------------------|----------------|------------------|----------------|------------------------|----------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 7.00 | | 3.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

**Zone 2 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)**

| Zone 2 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 2 | 1,458.02 | 1,103.81 | lbs / day | 346.85 | lbs / day | 7.36 | lbs / day |
| TOTAL POUNDS / DAY | 1,458.02 | 1,103.81 | lbs / day | 346.85 | lbs / day | 7.36 | lbs / day |
| TOTAL CUBIC YARDS | 6.91 | 5.23 | loose cy / day | 1.64 | loose cy / day | 0.03 | loose cy / day |

| Zone 2 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 8.00 | | 3.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 3 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 3 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 3 | 1,256.17 | 961.81 | lbs / day | 287.95 | lbs / day | 6.41 | lbs / day |
| TOTAL POUNDS / DAY | 1,256.17 | 961.81 | lbs / day | 287.95 | lbs / day | 6.41 | lbs / day |
| TOTAL CUBIC YARDS | 5.96 | 4.56 | loose cy / day | 1.37 | loose cy / day | 0.03 | loose cy / day |

| Zone 3 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 7.00 | | 3.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 4 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 4 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 4 | 798.42 | 595.84 | lbs / day | 198.61 | lbs / day | 3.97 | lbs / day |
| TOTAL POUNDS / DAY | 798.42 | 595.84 | lbs / day | 198.61 | lbs / day | 3.97 | lbs / day |
| TOTAL CUBIC YARDS | 3.79 | 2.83 | loose cy / day | 0.94 | loose cy / day | 0.02 | loose cy / day |

| Zone 4 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 5.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 5 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 5 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 5 | 1,116.86 | 870.24 | lbs / day | 240.82 | lbs / day | 5.80 | lbs / day |
| TOTAL POUNDS / DAY | 1,116.86 | 870.24 | lbs / day | 240.82 | lbs / day | 5.80 | lbs / day |
| TOTAL CUBIC YARDS | 5.30 | 4.13 | loose cy / day | 1.14 | loose cy / day | 0.03 | loose cy / day |

| Zone 5 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 7.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 6 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 6 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 6 | 896.29 | 668.87 | lbs / day | 222.96 | lbs / day | 4.46 | lbs / day |
| TOTAL POUNDS / DAY | 896.29 | 668.87 | lbs / day | 222.96 | lbs / day | 4.46 | lbs / day |
| TOTAL CUBIC YARDS | 4.25 | 3.17 | loose cy / day | 1.06 | loose cy / day | 0.02 | loose cy / day |

| Zone 6 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 5.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

**Zone 7 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)**

| Zone 7 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 7 | 816.91 | 609.64 | lbs / day | 203.21 | lbs / day | 4.06 | lbs / day |
| TOTAL POUNDS / DAY | 816.91 | 609.64 | lbs / day | 203.21 | lbs / day | 4.06 | lbs / day |
| TOTAL CUBIC YARDS | 3.87 | 2.89 | loose cy / day | 0.96 | loose cy / day | 0.02 | loose cy / day |

| Zone 7 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 5.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 8 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 8 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 8 | 1,076.06 | 816.87 | lbs / day | 253.74 | lbs / day | 5.45 | lbs / day |
| TOTAL POUNDS / DAY | 1,076.06 | 816.87 | lbs / day | 253.74 | lbs / day | 5.45 | lbs / day |
| TOTAL CUBIC YARDS | 5.10 | 3.87 | loose cy / day | 1.20 | loose cy / day | 0.03 | loose cy / day |

| Zone 8 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 6.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 9 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 9 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 9 | 1,394.53 | 1,040.70 | lbs / day | 346.90 | lbs / day | 6.94 | lbs / day |
| TOTAL POUNDS / DAY | 1,394.53 | 1,040.70 | lbs / day | 346.90 | lbs / day | 6.94 | lbs / day |
| TOTAL CUBIC YARDS | 6.61 | 4.94 | loose cy / day | 1.65 | loose cy / day | 0.03 | loose cy / day |

| Zone 9 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|-----------------------|-------------------|------------|-------------------|-----------------|-------------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 8.00 | | 3.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

**Zone 10 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)**

| Zone 10 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|----------------------------------|------------------------------|-----------------------|------------------|-----------------------|------------------------|-----------------------|
| Zone 10 | 889.35 | 663.69 | lbs / day | 221.23 | lbs / day | 4.42 | lbs / day |
| TOTAL POUNDS / DAY | 889.35 | 663.69 | lbs / day | 221.23 | lbs / day | 4.42 | lbs / day |
| TOTAL CUBIC YARDS | 4.22 | 3.15 | loose cy / day | 1.05 | loose cy / day | 0.02 | loose cy / day |

| Zone 10 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|------------------------------|----------------|------------------|----------------|------------------------|----------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 5.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 11 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 11 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 11 | 1,083.87 | 808.85 | lbs / day | 269.62 | lbs / day | 5.39 | lbs / day |
| TOTAL POUNDS / DAY | 1,083.87 | 808.85 | lbs / day | 269.62 | lbs / day | 5.39 | lbs / day |
| TOTAL CUBIC YARDS | 5.14 | 3.84 | loose cy / day | 1.28 | loose cy / day | 0.03 | loose cy / day |

| Zone 11 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|-----------------------|----------------|------------|----------------|-----------------|----------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 6.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |

Zone 12 OF WELLINGTON VILLAGE
3 DAY HOLDING STUDY (PICKUP FREQUENCY = 3 / WEEK)

| Zone 12 OF WELLINGTON VILLAGE - DAILY WASTE PROJECTIONS (LOOSE) | TOTAL WASTE (LBS/DAY) | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|---|--------------------------|-----------------------|-----------------------|---------------|-----------------------|-----------------|-----------------------|
| Zone 12 | 566.85 | 408.49 | lbs / day | 155.25 | lbs / day | 3.10 | lbs / day |
| TOTAL POUNDS / DAY | 566.85 | 408.49 | lbs / day | 155.25 | lbs / day | 3.10 | lbs / day |
| TOTAL CUBIC YARDS | 2.69 | 1.94 | loose cy / day | 0.74 | loose cy / day | 0.01 | loose cy / day |

| Zone 12 OF WELLINGTON VILLAGE - WASTE MANAGEMENT CONTAINER SUMMARY AT LOCAL COLLECTION AREA. | MUNICIPAL SOLID WASTE | | CARDBOARD | | UNIVERSAL WASTE | |
|--|-----------------------|----------------|------------|----------------|-----------------|----------------|
| CONTAINER TYPE | TILT TRUCK | | TILT TRUCK | | GAYLORD BOX | |
| CONTAINER SIZE: | 2.00 | CY | 2.00 | CY | 1.50 | CY |
| CONTAINER QUANTITY: | 3.00 | | 2.00 | | 1.00 | |
| FREQUENCY OF PICKUP: | 3 | TIMES PER WEEK | 3.00 | TIMES PER WEEK | 1.00 | TIMES PER YEAR |



Technical Excellence
Practical Experience
Client Responsiveness

September 29, 2025

**Drainage Statement for
K-Park MUPD
10400 Stribling Way
Village of Wellington, Palm Beach County, FL
Langan Project No.: 341020902**

Site Description

The project site is located on approximately 71.27 acres along the south side of Stribling Way at the southwest corner of the intersection of Stribling Way and US 441/SR 7, in the Village of Wellington located within Palm Beach County, Florida. The site consists of vacant, agricultural land and is bounded on the south and west by residential parcels, on the north by Stribling Way, and on the east by US 441/SR 7. A portion of the project site is also adjacent to an existing church property which is located near the southeast corner of the site. The Palm Beach County Property Appraiser's Office describes the property by Folio Numbers 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000.

The project site lies in South Florida Water Management District's C-51 drainage basin. The existing site elevations range from approximately 15.0-ft NAVD to 19-ft NAVD. The site is located in a FEMA Flood Zone "X" and existing on-site lake portion in Zone AE Elevation 15.4-ft NAVD.

Site Drainage

The project site is located within ACME Improvement District Basin A in the Village of Wellington. There are existing interconnected on-site ditches which drain the agricultural rows. The on-site ditches currently have a connection to the Lake Worth Drainage District (LWDD) E-1 canal.

With the proposed development, stormwater will be collected via a network of inlets and piping connected to the buildings, parking areas, and roadways, and piped to the onsite system of lakes, dry detention areas, and exfiltration trench. Legal positive outfall to the ACME Improvement District canal system is proposed via the Stribling Way drainage infrastructure. The on-site system will not have a connection to the LWDD E-1 canal.

The proposed drainage design will address the following criteria:

- Parking lot elevations will be set at or above the 5-year, 1-day flood stage
- No off-site discharge up to the 25-year, 3-day flood stage except through an approved control structure. The on-site drainage basin will have a control structure with a bleed down orifice for recovery
- Legal positive outfall to the Stribling Way drainage infrastructure (existing 36" RCP)
- Tailwater elevations at outfall to be per Village of Wellington ordinance 2010-14
- Building finished floor elevations set at or above the 100-year, 3-day (zero discharge) flood stage
- Water Quality Treatment volume in accordance with SFWMD and ACME Improvement District criteria
- Allowable discharge per the ACME Improvement District criteria
- Drainage review and permits will be obtained from SFWMD, ACME Improvement District and the Village of Wellington prior to construction
- All construction will be performed in accordance with SFWMD permit and other permits to be obtained for the project

Drainage Statement for
K-Park MUPD
Village of Wellington, Palm Beach County, FL
Langan Project No.: 341020902

September 29, 2025
Page 2 of 2

Site Drainage for Life Church

The existing stormwater management system for the Life Church property (3061 State Road 7, Wellington, FL; PCN 73-41-44-24-10-001-0000) is being modified to accommodate the project's proposed shared access from US 441/SR7. The project proposes to reconfigure the site's onsite lake, relocate and increase the site's dry detention area, and relocates the site's existing outfall to Stribling Way. The proposed outfall pipe serving the Life Church property will be rerouted within a proposed drainage easement, before connecting to the offsite drainage infrastructure. After the construction of the new outfall pipe, the existing 50-foot drainage easement (O.R.B. 25941, Pg. 0325) will be vacated.

If you have any questions regarding this application or drainage statement, feel free to contact me at (561) 473-8318 or via email at samritt@langan.com.

Sincerely,

Langan Engineering and Environmental Services, LLC.



This item has been digitally signed and sealed by Sarah Helga Amritt, PE on the date adjacent to the seal.

Signature must be verified on any electronic copies.

Sarah H Amritt

Digitally signed by Sarah H Amritt
DN: cn=Sarah H Amritt,
o=Langan Engineering and Environmental
Services, Inc., ou=US
Date: 2025.09.29 14:33:38-0400

Sarah H. Amritt, P.E., ENV SP
Professional Engineer License No. 76127
Associate Principal

SHA:sha/me
Enclosure(s):

cc: Richard Reikenis (Langan)
Marckley Etienne (Langan)

FBPE Registry No. 6601

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September 29, 2025

Narrative for Life Church Water & Sewer Utility Relocations
K-Park MUPD
10400 Stribling Way
Village of Wellington, Palm Beach County, FL
Langan Project No.: 341020902

Description of Existing Water & Sewer Utilities to be Relocated

The project site proposes modifications to the existing roadway access to the Life Church property from US 441/SR 7. The proposed modifications require reconfiguration of the on-site lake and relocation of the following utilities that currently run east along the property's driveway:

- 8" DIP water main within a 12-ft water easement (W.E.)
- 2" HDPE private force main

Proposed Water & Sewer Utility Relocations and Easements

The project proposes the following water main and force main relocations as shown on sheet C-512:

- The 8" DIP water main will be relocated to run east along the new driveway on US 441/SR 7, within a new 12-ft water easement within private property.
- The 2" HDPE force main will be relocated to run east along the new driveway on US 441/SR 7.

Phasing of Water & Sewer Utility Construction for Life Church

The project proposes to relocate the water and sewer utilities while keeping uninterrupted service to the Life Church property. The anticipated phasing is as follows (details subject to change based on contractor means and methods):

1. Construction of new 8" DIP water main, as shown on sheet C-512. The water main will have a wet tap to the existing onsite 8" water main upstream of the segment to be relocated, and will have a wet tap to the existing 12" water main on US 441/SR 7.
2. Construction of new 2" HDPE force main, as shown on sheet C-512. The force main will connect to the existing onsite 2" HDPE force main upstream of the segment to be relocated, and will connect to the existing 4" force main on US 441/SR 7.
3. Once the new segments of 8" DIP water main and 2" HDPE force main are constructed and certified, the existing water and sewer main segments in conflict with the modified lake will be cut, capped and removed.

4. The 12' water easement (P.B. 115 PG. 44) will be vacated after the new segment of water main is constructed and existing segment of water main is removed. A new water easement will be recorded for the relocated water main.

If you have any questions regarding this application, feel free to contact me at (561) 473-8318 or via email at samritt@langan.com.

Sincerely,
Langan Engineering and Environmental Services, LLC.



Sarah H. Amritt, P.E., ENV SP
Associate Principal

SHA:sha/me
Enclosure(s):

cc: Richard Reikenis, Marckley Etienne (Langan)
FBPE Registry No. 6601

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K-PARK MUPD REZONING JUSTIFICATION STATEMENT

Request: Rezoning

Original Submittal: May 16, 2025

Resubmittal: August 18, 2025

Resubmittal: September 29, 2025

Resubmittal: November 19, 2025



Urban Design
Land Planning
Landscape Architecture

OVERVIEW OF REQUEST & PROPERTY INTRODUCTION

On behalf of the Applicants, Wellington Property Owner LLC and Founders Acreage Wellington, LLC, Urban Design Studio (UDS) requests a Rezoning from Community Commercial (CC) and Community Facilities (CF) to Multiple Use Planned Unit Development (MUPD) consistent with the concurrent Master Plan application for the K Park MUPD Project. A concurrent application to approve a Conditional Use approval for a Hotel, and a School, Secondary or Primary as part of the MUPD are also being submitted. The subject property is comprised of a 71.27-acre property of which approximately 5.3 acres are currently zoned Community Commercial (CC), and approximately 65.98 acres designated as Community Facilities (CF). The site is located at 10400 Stribling Way, and is generally located at the southwest intersection of Stribling Way and State Road 7, referenced by parcel control numbers (PCN) 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000

The purpose of this application is to allow for the development of an MUPD consisting of a 405,000 SF / 1,750 student School, 215 Residential Units, 210,000 SF of retail, 75,000 SF of office, an 180-room hotel, and 105,000 SF of restaurant uses plus up to 20,000 SF of outdoor dining. A Master Site Plan is included for the MUPD.

SURROUNDING USES

The Subject Property is situated along the eastern boundary of the Village at the southwest corner of Stribling Way and State Road 7 (US 441). Within the Village, this portion of the State Road 7 corridor has been developed with primarily non-residential uses at a larger scale including the Palomino Park Medical Center and Marketplace at Wycliffe to the south as well as the Village Green Center, the Mall at Wellington Green, and Wellington Regional Medical Center to the north. Similarly, the Stribling Way corridor provides for a number of nearby institutional uses including the adjacent Life Church Wellington, Emerald Cove Middle School and Olympia Park to the east, and the Village Park Athletics Complex to the west. In addition to the considerable non-residential uses in the surrounding area, properties to the west, north, and south are developed with single-family residential uses on lots of one-fifth to one-quarter of an acre and separated from the site by road right-of-way or substantial landscape buffers. The Subject Property is strategically positioned to provide for the proposed mix of uses contemplated for the K-Park MUPD, including the proposed school use on the western portion of the site.

Please refer to the following summary table with information on the land use and zoning of adjacent properties in closest proximity to the Subject Property.

| | Existing Use | Land Use | Zoning |
|------------------|--|--------------------------------------|----------------------------------|
| Northwest | SF Residential – Castellina | Residential C | Residential PUD |
| Northeast | Commercial – Village Green Center | Community Commercial (CC) | Community Commercial (CC) |
| South | SF Residential – Farmington Estates (PBC) | Residential Low Density (PBC) | Residential PUD (PBC) |
| Southeast | Life Church Wellington | Community Facilities (CF) | Community Facilities (CF) |
| East | State Rd 7 ROW - Olympia Park | Community Facilities (CF) | Community Facilities (CF) |
| West | SF Residential – Oakmont Estates | Residential C | Residential PUD |

BACKGROUND & HISTORY

On September 9, 2003, the Acme Improvement District (Acme) acquired 66.27 acres for of the subject property. The additional acreage for the 71.27-acre property were formerly road right-of-way south of Stribling Way and a strip of land providing access to State Road 7 on the south side of the property.

In February 2006, Wellington entered into a contract with Sunrise Development, Inc. to sell 5.3 acres of the site along State Road 7 to develop an assisted living facility for seniors. As a part of that project, Wellington approved a Comprehensive Plan amendment for the intended site in October 2006, changing the designations from Community Facilities to Community Commercial. However, the buyer elected not to proceed with the project and withdrew their offer to purchase on November 17, 2006.

In December 2010, Wellington began leasing the property to J. Alderman Farms, Inc. on an annual basis. Currently, Alderman leases the property for approximately \$55,000 per year.

In January 2025, Wellington executed two Purchase and Sale Agreements with Wellington Property Owner, LLC and Founders Acreage Wellington, LLC to purchase the property upon receiving entitlements for a mixed-use development. The first phase of the entitlements involved a Village-initiated amendment of the Future Land Use Map of Wellington’s Comprehensive Plan. Previously, 5.3 acres of the property were designated Commercial (C), and 65.98 acres were designated Community Facilities (CF). The proposed land use change was processed to support the development of the property as a comprehensive mixed-use project with multifamily residential, retail, hotel, restaurant, and a school. On March 11, 2025, the Village Council adopted Ordinance No. 2025-04 to approve the Comprehensive Plan Map Amendment redesignating the entirety of the Subject Property with the Mixed-Use (MU) Future Land Use.

REZONING

The applicant is proposing rezoning to the MUPD district to allow for the creation of a master planned, mixed-use development under the recently assigned Mixed-Use (MU) FLU designation. Per Table LU&CD 1-1 of the Village Comprehensive Plan, the MU FLU designation allows a maximum density of 10 dwelling units/acre and a maximum FAR of 0.8. However, the Village adopted Future Land Use Amendment per Ordinance 2025-04, included a Traffic Analysis establishing a maximum density and intensity for the 71.3-acre site of 8.42 dwelling units an acre based on 600 multifamily dwelling units

and a maximum FAR of 0.5 which allows for an overall maximum development intensity of 1,552,478 SF.

The proposed MUPD remains within both the established Comprehensive Plan and Future Land Use Amendment Traffic Analysis limits for density and intensity under the MU FLU designation. The proposed development program is designed with up to 215 multifamily residential dwelling units with a proposed density of 3.02 du/ac and a proposed FAR of 0.42 with up to 1,300,000 SF of gross floor area.

The proposed rezoning of the 71.27-acre Village-owned vacant site to the Multiple Use Planned Development District (MUPD) aligns with the broader urban development goals of the Village, provides a premier education option for the Village, and offers a regional mixed-use retail center including shopping, dining, hotel, office and recreation for the benefit of the Village residents.

Some of the benefits proposed in the MUPD to support the rezoning request include, but are not limited to:

- Traffic calming & roadway features such as on-street parking and chicanes for slowing traffic.
- Public gathering & recreation spaces including pedestrian promenades, plazas, parklets, pathways, wide sidewalks, and gardens to enhance community interaction.
- Street trees, decorative landscape planters.
- Pedestrian-friendly light fixtures, shading, trash receptacles, and benches.
- Structured parking to allow more open and green spaces.
- Pedestrian-focused events (e.g., farmer's markets, live music, street fairs, holiday celebrations, and seasonal programming).
- Enhanced architectural details such as detailing on all sides to prevent blank walls, and see of breaks in roof planes, building planes, and massing for visual interest.
- Incorporation of varied materials, colors, and textures for unique storefronts.

REZONING STANDARDS

Pursuant to the Village's Development Review Manual, the following decision-making standards apply to any request for Rezoning. Please consider the Applicant's responses to all applicable standards as listed below.

A. *Whether the proposed amendment is consistent with all elements of the comprehensive plan.*

Response: The proposed rezoning of the 71.27-acre site from Community Commercial (CC) and Community Facilities (CF) to MUPD is consistent with all elements of the Village of Wellington Comprehensive Plan, specifically the Future Land Use & Community Design Element (LU&CD). The MUPD zoning designation allows for the contextually appropriate large-scale mixed-use developments along the State Road 7 corridor and aligns with the community's long-term planning principles. This fulfills Objective LU&CD 2.5, which promotes the corridor's evolution into a mixed-use destination with housing, businesses, and employment opportunities.

The proposed development is consistent with Goal LU&CD 1, which emphasizes enhancing the character and quality of Wellington's neighborhoods, commercial districts, and corridors. The proposed rezoning achieves this by requiring and providing a mix of five (5) land uses inclusive of School, Residential, Commercial, Open Space, and Office. The site is also greater than 60 acres, consistent with Objective LU&CD 1.4 and Policy LU&CD 1.4.1 for Large-Scale Mixed-Use (LSMU) designations. It satisfies the required balance of uses by dedicating more than 60% of the project area to non-residential functions, including commercial and institutional components, while also providing residential density at a level compatible with the maximum Planned Development density of 10 DU/AC permitted

in the Mixed Use (MU) Land Use category. This mixed-use character supports the regional orientation of the site and contributes to a sustainable and walkable development pattern.

The school, hotel, retail, and dining components which will be permitted as part of the MUPD zoning will also encourage job creation and community interaction, consistent with Policies LU&CD 2.5.3 and 2.5.4.

The proposed rezoning to MUPD will also require that the project be designed with integrated vehicular and pedestrian circulation in accordance with Policy LU&CD 1.3.2. The MUPD will include landscaping, buffering, and open space features that enhance the site. These features will also enhance the transition between uses and preserve the character of adjacent neighborhoods.

Further supporting the Comprehensive Plan's vision, the location of the site along State Road 7 fulfills Objective LU&CD 2.5, which promotes the corridor's evolution into a mixed-use destination with housing, businesses, and employment opportunities. The school, hotel, retail, and dining components encourage job creation and community interaction, consistent with Policies LU&CD 2.5.3 and 2.5.4, while the inclusion of the additional 215 multifamily housing units supports Policy LU&CD 2.5.5 by providing residential options for those that work within the MUPD and for potential residents of the Village of Wellington. The project also contributes to Wellington's strategic land use pattern by providing a mix of vertically and horizontally integrated uses, reinforcing the corridor's vitality and reducing development pressure on more suburban and rural areas.

The project has also been designed with integrated vehicular and pedestrian circulation networks in accordance with Policy LU&CD 1.3.2, and will include landscaping, buffering, and open space features that enhance the site cohesion. These features will enhance the transition between uses and preserve the character of adjacent neighborhoods.

The project complies with Policy MB 1.1.3 as the MUPD proposes a widening of the existing sidewalk within Stribling Way, a comprehensive sidewalk network along the storefront and plaza areas of Pod B, and an 8' minimum lake path surrounding the Pod "O" Common Open Space Lake. The site is also designed to ensure adequate circulation aisles, turning radii and parking spaces for comfortable vehicular circulation, demonstrating compliance with Policy MB 2.1.2.

The project will comply with Policy CI 1.3.3 and 1.4.1 of the Capital Improvements Element as it relates to Proportionate Share cost of expanded facilities and infrastructure, and impact fees required by the proposed development. The Applicant will commit to providing the required proportionate share and impact fees generated by the new development as coordinated with Village staff throughout the development process. Proposed improvements associated with the project will increase the efficiency of public utility infrastructure at the expense of the Applicant in direct support of the CI Element policies.

The project also complies with Policy ED 1.4.2 of the Education Element of the Comprehensive Plan with the provision of the school proposed in Pod A of the MUPD. The proposed school will provide a comprehensive curriculum focused on academic excellence, global citizenship, and holistic development. Instruction will be delivered in a nurturing, inclusive environment that fosters critical thinking, creativity, and lifelong learning further enhancing the educational opportunities in Wellington.

The project complies with Policy H&N 2.1.3 of the Housing Element of the Comprehensive Plan through the accommodation of mixed-use development and vertically integrated housing and commercial with the Pod B design. The proposed mixed-use pod is also adjacent to State Road 7 which is a major transportation corridor appropriate for the mix of uses.

The project complies with Policy PF 1.1.4 of the Public Facilities Element of the Comprehensive Plan which prescribes that adequate water supplies and required infrastructure are available to serve new development no later than the date of the issuance of the first certificate of occupancy. The proposed MUPD will be designed to provide the necessary water supplies and infrastructure for the Pod A School and Pod B Mixed-Use Village prior to the first Certificate of Occupancy.

The project will also comply with Policy CSR 3.1.1 of the Conservation, Sustainability, & Resiliency Element. The proposed construction for Pod A and Pod B will be designed to achieve LEED silver certification or similar green building ratings on the architecture proposed.

In summary, the proposed amendment to establish this MUPD is fully consistent with the goals, objectives, and policies of the Village of Wellington's Comprehensive Plan. It advances the community's vision for sustainable, integrated, and economically vibrant development within the State Road 7 corridor and complements the surrounding land use context in a balanced and forward-thinking manner.

- B. Whether the proposed request is not in conflict with any portion of Wellington's LDR and is consistent with the purpose and intent of the LDR.*

Response: The proposed rezoning to MUPD (Multiple Use Planned Development) is consistent with the Wellington Land Development Regulations (LDRs) and aligns with the stated purpose and intent of applicable regulations. It is compatible with the underlying Mixed-Use (MU) Future Land Use designation and the specific limitations established by Ordinance No. 2025-04.

The MUPD zoning meets the minimum lot size and dimensional requirements, and complies with the maximum allowable residential density of 10 dwelling units per acre and a Floor Area Ratio (FAR) of 0.8, as specified in Table LU&CD 1-1 of the Village Comprehensive Plan.

The proposed development includes approximately 215 residential units, resulting in a density of 3.02 dwelling units per acre. It also includes up to 1,300,000 SF square feet of gross floor area, including the residential development, yielding an FAR of 0.42. Both figures fall within the limits set by the Comprehensive Plan and Ordinance No. 2025-04.

Additionally, the MUPD meets the requirement for a minimum number of use types on sites exceeding 30 acres. The K-Park MUPD will include a mix of School, Residential, Commercial, Open Space, and Office.

- C. Whether the proposed request is compatible and consistent with existing uses and the zoning surrounding the subject land and is the appropriate zoning district for the land.*

Response: The proposed rezoning of the 71.27-acre from Community Facilities (CF) and Community Commercial (CC) to Multiple Use Planned Development (MUPD) is compatible with and complementary to the existing development patterns and land uses in the surrounding area. The immediate vicinity includes a diverse mix of commercial, residential, and institutional uses, including the Village Green Shopping Center to the north, Emerald Cove Middle School and Olympia Park to the east, and Single-Family residential to the west and south. The integration of multifamily residential, retail, office, dining, hotel, and school uses on the site aligns with this existing mix and enhances the area's functionality by providing additional amenities and services that can be utilized by nearby residents and businesses.

The location of the site along State Road 7, a major north-south arterial, further supports the appropriateness of the MUPD zoning. The MUPD is designed to propose the higher intensity hotel, retail, and dining uses within Pod B adjacent to State Road 7 and further away from the neighboring

single-family residential to the west and south of the property. Instead, the Pod A school is designed to be adjacent to these uses to allow for compatibility with the surrounding area.

The MUPD also provides multifamily residential uses which will strengthen housing diversity while maintaining compatibility through appropriate buffering, design standards, and connectivity. The proposed Rezoning enhances land use efficiency and contributes to a balanced community by integrating living, working, shopping, education, and hospitality in a cohesive environment. The proposed development supports long-term growth and vitality in the region.

D. Whether there exist changed conditions which require a rezoning.

Response: The proposed rezoning of the 71.27-acre site is warranted due to a series of changed conditions that render the existing Community Facilities (CF) and Community Commercial (CC) zoning no longer suitable as the vacant land represents an underutilized site for the Village and surplus to necessary operations. This is further supported by the Purchase and Sale Agreements approved between the Village and Applicant team indicating the desire to redevelop the site in the future. The site no longer aligns with the current development patterns or land use needs of the area. Over time, the surrounding corridor has experienced substantial growth and transformation, with significant increases in commercial, residential, and mixed-use development activity. This evolution has repositioned the area as a prominent regional node, making it more appropriate for a higher-intensity, mixed-use designation.

Furthermore, regional infrastructure improvements—particularly along State Road 7—have enhanced accessibility and mobility, increasing the site's suitability for integrated development. Market demand within Wellington has also shifted toward more diverse land use offerings, including multi-family residential, educational facilities, retail, office, and hospitality services. These uses are not only compatible with each other, but also respond to community needs and economic trends. The proposed Multiple Use Planned Development (MUPD) zoning supports a flexible land use framework that encourages infill development and promotes a walkable, sustainable community.

E. Whether and the extent to which the proposed amendment would result in significantly adverse impacts on the natural environment.

Response: The proposed rezoning from CF to MUPD will not result in significantly adverse impacts on the natural environment. The site is currently used for low-intensity agricultural purposes and has been cleared and, as such, there are no environmentally sensitive areas, protected species, or wetlands existing on or adjacent to the property that would be adversely affected by the proposed development. The site layout seeks to preserve existing mature vegetation where feasible and incorporates native landscaping to reduce irrigation needs. Stormwater is managed on-site through a system designed to meet or exceed South Florida Water Management District and Village requirements, thereby preventing off-site discharge and protecting water quality. Sustainable building practices will further reduce air and water impacts associated with construction and operation.

F. That the proposed request would result in a logical and orderly development pattern.

Response: The proposed rezoning to MUPD results in a logical and orderly development pattern by converting a large, underutilized agricultural tract into a mixed-use community with a complimentary selection of uses. By clustering commercial uses along a major transportation corridor and placing institutional and residential uses internally with appropriate transitions, the plan avoids land use conflicts and supports efficient access to goods, services, schools, and employment. The internal circulation network, combined with external connectivity to Stribling Way and State Road 7, reflects thoughtful

coordination with the broader mobility and land use system. This approach promotes long-term viability, land use balance, and community cohesion.

G. That the proposed request is consistent with applicable neighborhood plans.

Response: No neighborhood plans have been identified for the subject site area.

H. Whether the proposed complies with article 2, Concurrency Management,

Response: The proposed rezoning to MUPD will be consistent with Article 2 Concurrency Management. The Applicant is in coordination with the Village of Wellington Utility Department as well as the dry public utility providers in order to ensure that existing infrastructure can accommodate the proposed school without diminishing service levels to surrounding properties. Emergency services access has been incorporated into the site plan, and the development complies with applicable fire and life safety codes.

CONCLUSION

On behalf of the applicants, Urban Design Studio requests favorable review and consideration of this request to approve the proposed Rezoning. The Project Managers at Urban Design Studio are Ken Tuma and Lentzy Jean-Louis who can be reached at 561-366-1100. Please feel free to contact with any questions or for additional information in support of this application.

K-PARK MUPD POD B - HOTEL CONDITIONAL USE JUSTIFICATION STATEMENT

Request: Conditional Use – Hotel
Original Submittal: August 18, 2025
Resubmittal: September 29, 2025
Resubmittal: November 19, 2025
Resubmittal: December 17, 2025



Urban Design
Land Planning
Landscape Architecture

OVERVIEW OF REQUEST

On behalf of the Applicant, Wellington Property Owner, LLC, Urban Design Studio (UDS) and Elkus Manfredi Architects, as Agents, have prepared and hereby respectfully submit this application for approval of a Conditional Use to allow a Hotel within Pod B of the K-Park MUPD. The Applicant proposes to establish a hotel use that will serve as a high-quality hospitality option within the project designed to meet the needs of visitors in the Village of Wellington (Village) and surrounding communities.

The proposed hotel is designed as a family-friendly extended stay hotel integrated within the heart of the mixed-use pod operated by a brand known for setting global hospitality standards. The suite-style accommodations are complemented by the extensive site amenities including a pool, outdoor kitchen, and more to provide guests with the comfort of home while maintaining the services and amenities of a hotel. The hotel is designed to offer upgraded architectural treatment and, as the tallest structure integrated within the mixed-use village, is located as a central element for passive wayfinding. This proposed hospitality use is a thoughtful inclusion within the mixed-use K-Park MUPD as a perfect complement to the restaurants, retail, and service businesses offered within the balance of the project. Further, the hotel will support visitors to the Village who contribute to local commerce, particularly in the equestrian, cultural, and retail sectors that define the Village's identity.

CONCURRENT REQUESTS

This application is submitted concurrently with a Rezoning request from Community Facilities (CF) to Multiple Use Planned Unit Development (MUPD), a Master Plan application for the K-Park MUPD, and a Site Plan application for Pod B within the K-Park MUPD within which the proposed hotel is located. A separate application will be made at a later date to approve the height of the proposed Hotel which is anticipated to be above 35'. The proposed height request and building elevations will be subject to ARB and Village Council approval.

PROPERTY INTRODUCTION

The K-Park MUPD property comprises ± 71.27 -acres generally located at the southwest corner of Stribling Way and State Road 7 (US 441). The site is currently addressed 10400 Stribling Way and can be more specifically referenced by parcel control numbers (PCNs) 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000 ("Subject Property"). The hotel site is integrated with the balance of the mixed-use development planned to occupy the eastern portion of the Subject Property.

Please refer to the associated Rezoning and Master Plan applications for additional details on the overall project. This narrative serves to address the specific Conditional Use criteria in relation to the proposed hotel use located within Pod B as depicted on the K-Park MUPD Master Plan.

BACKGROUND & HISTORY

On September 9, 2003, the Acme Improvement District (Acme) acquired 66.27 acres of the subject property. The additional acreage for the 71.27-acre property was formerly road right-of-way south of Stribling Way and a strip of land providing access to State Road 7 on the south side of the property.

In February 2006, Wellington entered into a contract with Sunrise Development, Inc. to sell 5.3 acres of the site along State Road 7 to develop an assisted living facility for seniors. As a part of that project, Wellington approved a Comprehensive Plan amendment for the intended site in October 2006, changing the designations from Community Facilities to Community Commercial. However, the buyer elected not to proceed with the project and withdrew their offer to purchase on November 17, 2006. In December 2010, Wellington began leasing the property to J. Alderman Farms, Inc. on an annual basis. Currently, Alderman leases the property for approximately \$55,000 per year.

In January 2025, Wellington executed two Purchase and Sale Agreements with Wellington Property Owner, LLC and Founders Acreage Wellington, LLC to purchase the property upon receiving entitlements for a mixed-use development. On March 11, 2025, the Village Council adopted Ordinance No. 2025-04 to approve the Comprehensive Plan Map Amendment redesignating the entirety of the Subject Property with the Mixed-Use (MU) Future Land Use. The proposed land use change was processed to support the development of the property as a comprehensive mixed-use project offering retail, services, hotel, restaurant, offices, residential, and school uses.

SURROUNDING USES

The Subject Property is situated along the eastern boundary of the Village at the southwest corner of Stribling Way and State Road 7 (US 441). Within the Village, this portion of the State Road 7 corridor has been developed with primarily non-residential uses at a larger scale including the Palomino Park Medical Center and Marketplace at Wycliffe to the south as well as the Village Green Center, the Mall at Wellington Green, and Wellington Regional Medical Center to the north. Similarly, the Stribling Way corridor provides for a number of nearby institutional uses including the adjacent Life Church Wellington, Emerald Cove Middle School and Olympia Park to the east, and the Village Park Athletics Complex to the west. In addition to the non-residential uses in the surrounding area, properties to the west, north, and south are developed with single-family residential uses on lots of one-fifth to one-quarter of an acre and separated from the site by road right-of-way or substantial landscape buffers. The Subject Property is strategically positioned to provide for the proposed mix of uses contemplated for the K-Park MUPD, including the proposed hotel use on the southwestern portion of the site.

Please refer to the following summary table with information on the land use and zoning of adjacent properties in closest proximity to the Subject Property.

| | Existing Use | Land Use | Zoning |
|-----------|-----------------------------|---------------|-----------------|
| Northwest | SF Residential – Castellina | Residential C | Residential PUD |

| | | | |
|------------------|--|--------------------------------------|----------------------------------|
| Northeast | Commercial – Village Green Center | Community Commercial (CC) | Community Commercial (CC) |
| South | SF Residential – Farmington Estates (PBC) | Residential Low Density (PBC) | Residential PUD (PBC) |
| Southeast | Life Church Wellington | Community Facilities (CF) | Community Facilities (CF) |
| East | State Rd 7 ROW - Olympia Park | Community Facilities (CF) | Community Facilities (CF) |
| West | SF Residential – Oakmont Estates | Residential C | Residential PUD |

SITE DESIGN

The proposed hotel is envisioned as an integral component of a walkable, mixed-use development that blends hospitality, retail, dining, residential, office, and public space into a cohesive “main street” environment. The hotel is not conceived as a stand-alone building but as a seamless anchor within the larger development framework, supporting both residents and visitors while contributing to the vibrancy of the district. Designed to integrate with the overall main street aesthetic, the hotel incorporates traditional village design elements such as articulated façades, varied rooflines, transitional massing, and pedestrian-scaled treatments. The hotel shares surface parking in balance with the mixture of uses throughout this area of the project. A comprehensive network of pedestrian pathways connect the hotel to surrounding office, retail, personal service, and dining uses in a highly walkable configuration weaving guests through public plazas and green spaces. The location of the hotel and integration with the balance of the mixed-use village component creates a synergistic relationship between the uses and within the project design.

Please refer to the Master Preliminary Site Plan, Master Circulation Plan, and Conceptual Landscape Plans prepared by Urban Design Studio included with this conditional use application for depictions of the proposed hotel within the mixed-use site design as described above and in further detail below.

BUILDING HEIGHT & SETBACKS

Pursuant to Section 6.3.1.G.4 of the Land Development Regulations, developments within the State Road 7 corridor including uses such as the proposed hotel, may be allowed additional building height subject to Conditional Use approval of the Wellington Council. A separate Conditional Use application will be made at a later date to approve the height of the proposed Hotel which will exceed 35’ in height. Per Ordinance 2025-30, The Village of Wellington has updated the ULDC to amend the following

- Amend Article 5, Table 5.1.2-1, Development Application Approvals, Related To Building Height Application Types;
- Add 5 Section 5.3.11 Relating To Building Height;
- Amend Article 6, Section 6.3.1.F.5, Related To Building Height Applications In Excess Of 35 Feet;

As outlined in the amended code language, buildings shall provide one foot of additional setback beyond the minimum requirement for every one foot in height above 35 feet. The hotel as currently designed achieves approximately 86 feet of building height requiring an additional 50 feet of setback. The hotel building is located on the site to comply with this requirement for additional setback. The hotel is setback more than 130 feet from the closest property line to the south (rear setback) and more than 600 feet

from the property lines to the east abutting State Road 7 or north abutting Stribling Way (front and side setbacks). The proposed location of the hotel ensures the structure is located more than 600 feet from the nearest existing property development with residential uses.

As the tallest structure in the project, the building is strategically located within the core of the site and will serve as a visual focal point while functioning as an easily located passive wayfinding and orienting feature from any portion of the site. The hotel's architecture will be designed to offer transitional massing which breaks up the structure into smaller forms to avoid a monolithic appearance. The building will utilize materiality, fenestration, articulation and other façade treatments to offer a structure that seamlessly blends into the surrounding buildings and uses. Landscaping in foundation plantings and surrounding planting areas along the pedestrian pathways, plazas, and streets will complement the architecture and contribute to a softening of the building's massing and height as experienced from the surrounding pedestrian realm. The proposed hotel design utilizes a variety of innovative design strategies including façade articulation, massing modulation, roofline variation, landscape buffers, increased setbacks, and more to ensure compatibility in the context of the surrounding development pattern.

In support of this proposed height, the hotel provides innovative architectural design, increased setbacks, integration within a larger mixed-use development pattern, and a cohesive landscape palette with landscape plantings thoughtfully distributed along the main street, hotel, and surrounding plazas within the mixed-use project area. These standards will be further detailed in a forthcoming application for Conditional Use Height approval.

ACCESS

The hotel will be accessed through the planned internal street network of the mixed-use development, which provides safe and efficient vehicular and pedestrian circulation. Adequate surface parking is provided proximate to the hotel that will be shared with nearby uses in the mixed-use pod. A drop-off loop is provided for the hotel along with a dedicated valet service offered to best serve guests, guarantee efficient use of parking lots, and ensure smooth flow of traffic through the site. The project incorporates pedestrian connections and bicycle accommodations consistent with Wellington's mobility objectives and the goal of creating a compact, walkable mixed-use main street character within the project.

Please refer to the Traffic Operational Management Plan prepared by Simmons & White included with this application for more specific details on the hotel's operation as it related to access and circulation.

PARKING & LOADING

Pursuant to Section 7.5.1 of the Land Development Regulations, Table 7.5-1. Minimum Off-Street Parking Standards, the minimum off-street parking requirement for the hotel use is established as follows:

- 1.25 spaces per guest room
- 1 space per employee (at max shift)
- Accessory use areas over 1,500 net square feet are to be calculated separately

The hotel is designed to accommodate 180 guest rooms with as many as 25 employees on shift at one time. Based on the Village's requirements, the hotel is required to provide a total of 250 spaces to serve the guest rooms and employees with any accessory use areas over 1,500 net square feet to require additional parking. It is important to note the hotel use is a component of the larger K-Park MUPD and will utilize shared parking areas serving adjacent restaurant, retail, and personal service uses throughout the eastern portion of the site. Shared parking is viable for a development of this nature as different uses have different hours of operation and peak demand. Additionally, the compact, pedestrian friendly mixture of uses creates an ideal scenario for internal capture between uses.

As detailed in the provided Shared Parking Study prepared by Simmons & White, the combined maximum demand for parking in the mixed-use project area is 867 spaces during the height of weekend demand. As depicted on the Master Preliminary Site Plan, Pod B offers 776 parking spaces. With the use of valet and shared parking programs, the project can provide an additional 247 parking spaces for a total of 1,023 parking spaces. The total of 776 spaces does not include any of the valet parking or off-site parking that could potentially be utilized during peak demand nor does it account for the increased efficiency in the use of parking areas resulting from professional valet services.

LANDSCAPE BUFFERS

Pursuant to Chapter 8 of the Village's Land Development Regulations, specific landscape requirements are prescribed for various uses based on the proposed use of the site, applicable zoning district, adjacent uses, and specific site design characteristics. Further, Section 6.5.4.E.3 prescribes additional landscape standards for an MUPD. In connection with the Village's requirements for perimeter landscape buffers, the following perimeter buffer types are proposed for Pod B which includes the hotel:

- Stribling Way (North Property Line): 20-foot Type C buffer, with 5' overlap of a 10' general utility easement
- State Road 7 (East Property Line): 20-foot Type C buffer, with 5' overlap of a 10' general utility easement
- Abutting Life Church Property (South Property Line): Modified 10-foot Type A/F buffer with proposed elimination of required fence

Please refer to the Master Plan – Regulating Buffer Details (MP-2), the Conceptual Landscape Plan, and the Pod B Site Plan included with concurrent applications for specific information related to the design of landscape buffers and plantings proposed for Pod B in relation to the hotel. Please also refer to the concurrent rezoning, master plan, and site plan applications for K-Park MUPD with attention to the Project Standards Manual (PSM) for additional details on deviations and proposed landscaping for the project.

CONDITIONAL USE STANDARDS

Pursuant to the Village's Development Review Manual, the following decision-making standards apply to any request for Conditional Use or Compatibility Determination. Please consider the Applicant's responses to all applicable standards as listed below.

- a) *That the proposed request is consistent with all elements of the Comprehensive Plan.*

Response: The proposed hotel use is consistent with the goals, objectives, and policies of the Village of Wellington Comprehensive Plan, which encourages balanced growth, economic development, and the integration of residential, commercial, and hospitality uses within mixed-use districts along primary transportation corridors. By locating the hotel within a mixed-use development, the project supports land use policies that promote compact development patterns, efficient infrastructure use, and the creation of vibrant, pedestrian-oriented centers. Further, additional hospitality offerings within the Village will serve to support priority industries such as medical services and equestrian competition consistent with stated goals within the Comprehensive Plan.

The introduction of a vibrant and active mixed-use development is dependent, in part, on the proper mixture of uses at varying scales to create diversity, sustainability, and synergy within the project. The inclusion of a hospitality anchor contributes to this proper blend of uses in a manner directly supporting Objective LU&CD 1.4 of the Land Use & Community Design

(LU&CD) Element of the Comprehensive Plan. The hotel use within the mixed-use Pod B further supports the more specific vision for such development to occur along State Road 7 as stated in Objective LU&CD 2.5, which promotes the corridor's evolution into a mixed-use destination with housing, businesses, and employment opportunities. Similarly, the use serves as an additional amenity to enhance the Village as the Winter Equestrian Capital of the World as stated in Objective LU&CS 2.6. The hotel use encourages job creation, community interaction and hospitality offerings in the Village consistent with Policies LU&CD 2.5.3 and 2.5.4. The project also contributes to Wellington's strategic land use pattern by providing a mix of vertically and horizontally integrated uses, reinforcing the corridor's vitality and reducing development pressure on more suburban and rural areas.

The project has also been designed with integrated vehicular and pedestrian circulation networks in accordance with Policy LU&CD 1.3.2, and will include landscaping, buffering, and open space features that enhance the site cohesion. These features will enhance the transition between uses and preserve the character of adjacent neighborhoods. The project complies with Policy MB 1.1.3 as the MUPD proposed a meandering pedestrian pathway along Stribling Way as well as providing a comprehensive sidewalk network along the storefront and plaza areas of Pod B, as well as providing an 8' minimum lake path surrounding the Pod "O" Common Open Space Lake. The site is also designed to ensure adequate circulation aisles, turning radii and parking spaces for comfortable vehicular circulation, demonstrating compliance with Policy MB 2.1.2. Finally, additional hotel rooms will offer support to guests visiting the Village in relation to the equestrian competition industry in alignment with Goal EQ.3 in the Equestrian Element of the Comprehensive Plan.

The proposed hotel use directly supports the various Comprehensive Plan Goals, Objectives, and Policies listed above and indirectly supports dozens more.

- b) *That the proposed request is in compliance with Article 6 of the LDR (Supplementary Regulations).*

Response: Pursuant to Section 6.2.2.B.11 of the Village's Unified Land Development Code (ULDC), the following supplementary standards apply to the principal use of a Hotel. Specific responses are provided for each supplementary standard demonstrating compliance with the applicable standards.

- a. *A hotel may have an accessory lounge not to exceed to 25 percent of the gross floor area of the hotel, exclusive of parking.*

Response: The proposed 7-story hotel is designed with a gross floor area of approximately 175,000 square feet. In accordance with the above provision, the hotel may include an accessory lounge of up to 43,750 square feet. Floor plans including the specific floor area dedicated to the accessory lounge, lobby, and other amenities will be provided at time of Architectural Review Board (ARB) application for Pod B. The hotel's accessory lounge and other ancillary areas will comply with Sec. 6.2.2 Supplementary standards for principal uses and other requirements of the Land Development Regulations.

- b. *Must be located on a minimum of two acres.*

Response: The hotel use is proposed as one of several principal uses within the mixed-use Pod B which occupies 24.05-acres of the Subject Property before accounting for right-of-way dedications and required buffers. With a net site area of 22.22-acres, the Pod B site area far exceeds the minimum requirement for two acres.

- c) *That the proposed request will ensure general compatibility with adjacent properties and other property in the district (use and character).*

Response: The hotel has been designed to integrate seamlessly into the larger mixed-use development, which includes complementary residential, retail, and office components. Hospitality is a compatible use in this context, as it provides lodging opportunities that support nearby commercial activity, restaurants, and cultural venues. Architectural design, buffering, and site planning ensure that the hotel will not create adverse impacts on adjacent residential neighborhoods and will enhance the overall character of the development. The global reputation of the hotel brand guarantees guests can expect the highest quality of site and building operation, quality of service, and quality of neighbor.

Increased setbacks resulting from the central location of the hotel building within the site ensure the structure will exceed the requirement to provide an additional foot of setback for each additional foot of building height over 35 feet. The building utilizes a variety of architectural techniques to soften and transition the massing and blend into surrounding open spaces and public plazas. The increased setback of the building and innovative building design work in unison with perimeter landscape buffers, dispersed landscaping throughout the site, and foundation plantings abutting the structure to ensure the hotel is fully integrated within the site and highly compatible with adjacent properties. For these reasons and others discussed throughout this narrative, the proposed use of a hotel will be a positive inclusion within the mixed-use development and enhances the project's degree of compatibility with adjacent properties along the State Road 7 corridor.

- d) *That the design of the proposed request will minimize adverse effects, including visual impact and intensity of the proposed use on adjacent lands.*

Response: The project has been designed to minimize potential adverse impacts on surrounding properties through thoughtful building orientation, landscaping, and screening. The hotel will adhere to Village design standards, ensuring a high level of architectural quality and compatibility with Wellington's character. Building massing is broken into smaller forms to avoid a monolithic appearance, with corner treatments and architectural detailing highlighting key intersections and public spaces. Materials such as brick, stone, stucco, or decorative metal will be selected to complement adjacent retail and residential structures, reinforcing the sense of place. The hotel use is consolidated on the site within a minimal footprint to accomplish an intensity necessary for high-quality hospitality operation with extensive site amenities that encourage guests to explore the balance of the mixed-use development. Noise, lighting, and operational impacts will be mitigated through best management practices and site design. Any potential visual, auditory, or other sensory impacts resulting from the hotel would be mitigated by the distance of the use from surrounding properties and the use of perimeter and internal landscaping which serve to buffer the use.

- e) *That required provisions have been made for public facilities.*

Response: The subject property is located within an urbanized area of the Village along the State Road 7 corridor and is served by existing public facilities and infrastructure, including utilities, water and sewer, and roadway access, all of which have adequate capacity to support the proposed hotel use within the larger mixed-use development. Fire, rescue, and police services are readily available to serve the site, ensuring that the development will not place an undue burden on municipal services.

- f) *That the design of the proposed request will minimize environmental impacts, including but not limited to water, air, stormwater management, wildlife, vegetation, wetlands and natural functioning of the environment.*

Response: Environmental stewardship has been a priority in the design process. The site layout seeks to preserve existing mature vegetation where feasible and incorporates native landscaping to reduce irrigation needs. Stormwater is managed on-site through a system designed to meet or exceed South Florida Water Management District and Village requirements, thereby preventing off-site discharge and protecting water quality. The project has been previously cleared and utilized for agriculture and, as such, there are no environmentally sensitive areas, protected species, or wetlands existing on or adjacent to the property that would be adversely affected by the proposed development. Sustainable building practices will further reduce air and water impacts associated with construction and operation.

- g) *That the proposed request will result in logical, timely and orderly development patterns.*

Response: The hotel contributes to an orderly pattern of development by concentrating higher-intensity uses in a planned, coordinated environment along the primary State Road 7 corridor. Its placement within the mixed-use project helps to balance daytime and nighttime activity, ensuring vitality and economic sustainability while reducing potential traffic congestion by encouraging multi-purpose trips and internal capture within the development.

- h) *That the proposed request complies with all Code standards for use, layout, function and general development characteristics.*

Response: The hotel is located on site and designed to be maximally compliant with all applicable Code standards. The proposal specifically complies with the supplemental use standards for hotels as stated above. The hotel use and associated structure work to comply with requirements for the layout, function, and dimensional regulations associated with the proposed building form and the larger mixed-use development within Pod B. As stated with the Project Standards Manual (PSM) associated with the K-Park MUPD rezoning and master plan applications, certain deviations are requested for the project as a whole with several allowing for flexibility within Pod B such as alternative locations for foundation plantings. On the whole, the hotel use and structure are compliant with the requirements of the Code and lend support to the innovative design efforts employed within the K-Park MUPD in the interest of best addressing the intent of the Code.

- i) *That the proposed request is not out of scale with the needs of the neighborhood or Wellington.*

Response: The addition of a hotel within the mixed-use development supports Wellington's economic base by attracting visitors who contribute to local commerce, particularly in the equestrian, cultural, and retail sectors that define Wellington's identity. The hotel is proportionate to both the mixed-use environment created within the project and the broader Village context. It supports local walkability and diversity of uses, aligning with the Village's goal to maintain a high quality of life through design and balanced development. The hotel will provide employment opportunities, increase the Village's tax base, and serve as an amenity for both residents and visitors. By situating lodging within walking distance of shops, dining, and entertainment, the hotel fosters a dynamic, sustainable community environment. The hotel functions as a hospitality anchor within the mixed-use project, bringing visitors who support local businesses, creating a steady flow of activity, and reinforcing the main street as a vibrant and active destination.

CONCLUSION

On behalf of the applicants, Urban Design Studio requests favorable review and consideration of this request to approve the proposed Conditional Use. The Project Managers at Urban Design Studio can be reached at 561-366-1100. Please feel free to contact the Agent with any questions or for additional information in support of this application.

K-PARK MUPD POD A - WINGROVE ACADEMY CONDITIONAL USE JUSTIFICATION STATEMENT

Request: Conditional Use – School
Original Submittal: May 16, 2025
Resubmittal: September 29, 2025
Resubmittal: December 17, 2025



Urban Design
Land Planning
Landscape Architecture

OVERVIEW OF REQUEST

On behalf of the Applicant, Founders Acreage Wellington, LLC, Urban Design Studio (UDS), as Agent, has prepared and hereby respectfully submit this application for approval of a Conditional Use to allow a School entitled "Wingrove Academy" within Pod A of the K-Park MUPD. The Applicant proposes to establish a new private school that will serve as a high-quality educational institution designed to meet the academic and developmental needs of students in the Village of Wellington (Village) and surrounding communities. The school will be operated by a world-class organization with an established reputation as an experienced and globally recognized provider of innovative and student-centered educational programs.

The campus is planned to accommodate approximately 1,750 students in grade levels from early childhood through high school, offering a comprehensive curriculum focused on academic excellence, global citizenship, and holistic development. Instruction will be delivered in a nurturing, inclusive environment that fosters critical thinking, creativity, and lifelong learning. The school facility will include age-appropriate classrooms, science and technology labs, art and music spaces, a library/media center, and recreational areas designed to support both academic instruction and extracurricular enrichment. The site design will incorporate sustainable practices, efficient traffic circulation, and landscaped buffers to ensure compatibility with surrounding residential uses and integrate within the overall MUPD. With a focus on community integration, the new school will not only provide a valuable educational option for families but also support the long-term vision of Wellington as a vibrant, family-oriented community with access to diverse learning opportunities. A concurrent site plan application has been submitted for Pod A of the K-Park MUPD to reflect the proposed design and specific improvement associated with the school conditional use.

CONCURRENT REQUESTS

This application is submitted concurrently with a Rezoning request from Community Facilities (CF) to Multiple Use Planned Unit Development (MUPD), a Master Plan application for the K-Park MUPD, and a Site Plan application for Pod A. A separate application will be made at a later date to approve the height of the proposed buildings on site which exceed 35'. The proposed height request and building elevations will be subject to ARB and Village Council approval.

PROPERTY INTRODUCTION

The K-Park MUPD property comprises ± 71.27 -acres generally located at the southwest corner of Stribling Way and State Road 7 (US 441). The site is currently addressed 10400 Stribling Way and can be more specifically referenced by parcel control numbers (PCNs) 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000 ("Subject Property"). The school site is planned to occupy approximately 43.76-

acres as the western portion of the K-Park MUPD and serves as one of five required uses within the MUPD.

Please refer to the associated Rezoning and Master Plan applications for additional details on the overall project. This narrative serves to address the specific Conditional Use criteria in relation to the proposed school use.

BACKGROUND & HISTORY

On September 9, 2003, the Acme Improvement District (Acme) acquired 66.27 acres of the subject property. The additional acreage for the 71.27-acre property was formerly road right-of-way south of Stribling Way and a strip of land providing access to State Road 7 on the south side of the property.

In February 2006, Wellington entered into a contract with Sunrise Development, Inc. to sell 5.3 acres of the site along State Road 7 to develop an assisted living facility for seniors. As a part of that project, Wellington approved a Comprehensive Plan amendment for the intended site in October 2006, changing the designations from Community Facilities to Community Commercial. However, the buyer elected not to proceed with the project and withdrew their offer to purchase on November 17, 2006. In December 2010, Wellington began leasing the property to J. Alderman Farms, Inc. on an annual basis. Currently, Alderman leases the property for approximately \$55,000 per year.

In January 2025, Wellington executed two Purchase and Sale Agreements with Wellington Property Owner, LLC and Founders Acreage Wellington, LLC to purchase the property upon receiving entitlements for a mixed-use development. The first phase of the entitlements involved a Village-initiated amendment of the Future Land Use Map of Wellington's Comprehensive Plan. Previously, 5.3 acres of the property were designated Commercial (C), and 65.98 acres were designated Community Facilities (CF). The proposed land use change was processed to support the development of the property as a comprehensive mixed-use project with multifamily residential, retail, hotel, restaurant, and a school. On March 11, 2025, the Village Council adopted Ordinance No. 2025-04 to approve the Comprehensive Plan Map Amendment redesignating the entirety of the Subject Property with the Mixed-Use (MU) Future Land Use.

SURROUNDING USES

The Subject Property is situated along the eastern boundary of the Village at the southwest corner of Stribling Way and State Road 7 (US 441). Within the Village, this portion of the State Road 7 corridor has been developed with primarily non-residential uses at a larger scale including the Palomino Park Medical Center and Marketplace at Wycliffe to the south as well as the Village Green Center, the Mall at Wellington Green, and Wellington Regional Medical Center to the north. Similarly, the Stribling Way corridor provides for a number of nearby institutional uses including the adjacent Life Church Wellington to the south, Emerald Cove Middle School and Olympia Park to the east, and the Village Park Athletics Complex to the west. In addition to the considerable non-residential uses in the surrounding area, properties to the west, north, and south are developed with single-family residential uses on lots of one-fifth to one-quarter of an acre and separated from the site by road right-of-way or substantial landscape buffers. The Subject Property is strategically positioned to provide for the proposed mix of uses contemplated for the K-Park MUPD, including the proposed school use on the western portion of the site.

Please refer to the following summary table with information on the land use and zoning of adjacent properties in closest proximity to the Subject Property.

| | Existing Use | Land Use | Zoning |
|------------------|--|--------------------------------------|----------------------------------|
| Northwest | SF Residential – Castellina | Residential C | Residential PUD |
| Northeast | Commercial – Village Green Center | Community Commercial (CC) | Community Commercial (CC) |
| South | SF Residential – Farmington Estates (PBC) | Residential Low Density (PBC) | Residential PUD (PBC) |
| Southeast | Life Church Wellington | Community Facilities (CF) | Community Facilities (CF) |
| East | State Rd 7 ROW - Olympia Park | Community Facilities (CF) | Community Facilities (CF) |
| West | SF Residential – Oakmont Estates | Residential C | Residential PUD |

SITE DESIGN

The school site, referred to as “Wingrove Academy,” occupies approximately 43.76 acres on the western portion of the larger K-Park MUPD. It features an innovative layout with high-quality architecture and thoughtfully designed site improvements. The campus is fully secured along all perimeters, ensuring separation from adjacent uses, while still integrating with the broader development through the centrally located lake, which serves as a unifying element. The school provides a series of four unique structures ranging from two to three stories for a total building area of approximately 405,000 square feet complimented by sports courts and fields, a two story parking garage, associated surface parking and circulation around the periphery serving to allow additional separation for active use areas and building setbacks from adjacent properties.

The primary (elementary) and secondary (middle and high) school buildings are designed as separate but complimentary structures to account for the necessary differences in the operation of each. The primary school structure is designed at two stories as the northernmost building with a dedicated parking lot and drop-off area along with a secured playground located centrally within the design of the building to effectively screen this active use area. The secondary school structure is designed at three stories and is centrally located within the school site with a large courtyard activating the internal portion of the site. The design utilizes the adjacent lake area to frame the structures, enhance outdoor activity area, and create cohesion with other structures proposed. A two-story theater building is proposed at the northwestern portion of the site, framing the western side of the entry drive.

A final two-story sports and aquatic center with significant canopy sits as the westernmost structure positioned south of the two-story theater.

The remaining active sports areas are located towards the southern portion of the site and are clustered together to provide a compatible and comfortable functionality of such uses. Exterior amenities include two practice fields, multiple tennis courts, a baseball field, a softball field, a soccer field, and track to allow the school to offer the full gamut of Olympic sporting events.

A perimeter drive lane is proposed within the school site to allow functional access from adjacent roadways and to serve parking lots distributed throughout the site. This driveway and perimeter parking lots work to increase the separation of active uses and setbacks of proposed structures from adjacent properties. As a fully secured site, guardhouses are proposed at the northern driveway from Stribling Way and along the southern driveway providing connection to the shared access points along State Road 7. These gates are setback sufficiently to ensure extensive queueing within the property.

As described later in this narrative, perimeter landscape buffers are proposed along the north, west, and south property line with additional planting areas throughout the school site to allow for shaded pedestrian circulation, planted vehicular use areas, and foundation plantings. Further, security features including fences and walls are proposed to ensure safe integration of the amenitized lake features.

Please refer to the Master Preliminary Site Plan, Master Circulation Plan, and Conceptual Landscape Plans prepared by Urban Design Studio included with this Conditional Use application for depictions of the proposed school site design as described above and in further detail below.

BUILDING HEIGHT & SETBACKS

Per Ordinance 2025-30, The Village of Wellington has updated the ULDC to amend the following

- Amend Article 5, Table 5.1.2-1, Development Application Approvals, Related To Building Height Application Types;
- Add 5 Section 5.3.11 Relating To Building Height;
- Amend Article 6, Section 6.3.1.F.5, Related To Building Height Applications In Excess Of 35 Feet;

As outlined in the amended code language, all buildings above 35' in height are subject to a Conditional Use Height approval by Village Council. A separate application will be made at a later date to approve the height of the Pod A school buildings which are anticipated to be above 35'. The proposed height request and building elevations will be subject to ARB and Village Council approval. Also prescribed in the approved code revisions and currently prescribed in Section 6.3.1.G.5 is the requirement that all developments within the State Road 7 corridor featuring non-retail uses with 100 or more employees, such as the proposed school use, may be allowed building heights up to a maximum height of 72 feet subject to approval of the Wellington Council. Such buildings shall provide one foot of additional setback beyond the minimum requirement for every one foot in height above 35 feet. All structures proposed within the school site that exceed the base height of 35 feet will, at a minimum, comply with the requirement to provide one additional foot of setback for each additional foot of building height of 35 feet.

The school site proposes five structures consisting of a two-story theater, a two-story sports & aquatic center, a two-story primary school, a three-story secondary school, and a two-story parking garage. The architecture of the buildings and design utilizing increased floor to ceiling heights will result in a maximum building within 72' as currently permitted subject to Council approval in Section 6.3.1.G.5. With a base setback of 30 feet as established in the Project Standards Manual (PSM) for the K-Park MUPD, a building height of 72 would require a minimum setback of 67 feet. The centrally located secondary school structure, expected to be the tallest structure on the school site, exceeds the minimum requirement of a 67' setback from the north, west, and south property line. This additional setback significantly exceeds the minimum additional setback required to allow the increased building height with an increase of more than 200 feet above the minimum setback of 30 feet or more than 5 feet of additional setback for each additional foot of building height proposed over the base height of 35 feet.

The southernmost structure, a parking garage for the school uses, proposes a minimum rear setback of 101.9 feet from the south property line as the minimum building setback on the school site providing an additional 71.9 feet of setback above the minimum rear setback of 30 feet. Along the western property line, adjacent to the Oakmont Estates residential community, the Parking Garage is designed with a side of setback 55 feet from the property line. The proposed code revisions expected to be adopted in the first week of December prescribes that buildings adjacent to single-family residential properties shall provide two (2) feet of additional setback beyond the minimum requirement for every one (1) foot in height for all portions of the building that exceed 35 feet on the side of the building adjacent to the single-family properties. Based on the required side setback of 30' established for Pod A, the proposed parking garage structure can be built at a maximum of 47' in height, which the Applicant does not anticipate to be exceeded. As mentioned earlier in this narrative, a separate Condition Use application for height will be submitted at a later date for all buildings exceeding 35' and will be subject to ARB and Village Council approval.

The western setback is mitigated by several existing and proposed buffers. Oakmont Estates provides an existing 20-foot buffer, which is supplemented by a 10-foot landscape buffer along the western perimeter of Pod A. Additionally, there is a 45-foot separation area that includes a 30-foot driveway and landscaping. Together, these elements result in a total separation of 75 feet from the rear property lines of the adjacent single-family homes.

Similarly, the southern setback is mitigated by an existing 25-foot buffer tract within the Farmington Estates community. The 101.9-foot setback to the parking garage includes a 10-foot Type A/F buffer, a 30-foot circulation driveway, and additional landscaping north of the drive to further screen the garage from the residential area.

ACCESS

Vehicular access to the serve the school has been strategically planned to minimize any impact on the surrounding roadway system and create the safest and most efficient access and circulation pattern for the school operation. The access and circulation for the school have been analyzed in the context of existing traffic patterns and wholistically in the context of the overall mix of uses for the K-Park MUPD.

Vehicular access serving the school is proposed via the following driveway connections to the adjacent roadway network:

- Full access driveway on Stribling Way approximately 2,300 feet west of State Road 7;
- Full access signalized driveway connection to State Road 7 approximately 1,100 feet south of Stribling Way, located at the adjacent Life Church Wellington property.

The Applicant has devised a comprehensive operational management plan to create a safe, efficient, and minimally impactful access and circulation plan utilizing a variety of best practices supported by extensive experience operating in urban areas and backed by contextual data analysis and the professional input of local traffic engineers. These strategies include but are not limited to the following: staggered class start/end times, extensive on-site queuing, traffic control personnel, parent education programs, student parking, controlled delivery schedules, pedestrian and bicycle infrastructure, and traffic monitoring.

Significant vehicular stacking is provided to avoid any potential spillover of school traffic on adjacent roadways. Staggered start times for various grade levels will serve to distribute the school traffic arrivals and departures to further minimize any potential for impact to traffic accessing the project or navigating surrounding roadways during peak hours. The school will conduct parental and student education programs to ensure those driving into the site are informed as to the best practices for access and

circulation which will be complimented by trained school staff and traffic control personnel. Further, on-site parking for upper-level high school students and controlled delivery schedules serve to enhance the efficiency of traffic management. The Applicant is committed to ongoing traffic monitoring as the school operation comes online and ramps up in the early years of operation towards full enrollment.

Please refer to the Traffic Operational Management Plan prepared by Simmons & White included with this application for more specific details on the school's operation as it related to access and circulation.

PARKING & LOADING

Pursuant to Section 7.5.1 of the Land Development Regulations, Table 7.5-1. Minimum Off-Street Parking Standards, the minimum off-street parking requirement for schools is established as follows:

- Elementary: 1 space per classroom, plus 1 space per 200 net square feet of building not accounted for in classroom or hallway areas
- Secondary: 0.25 spaces per student, plus 1 space per 200 net square feet of building not accounted for in classroom or hallway areas

At this time, the details of interior buildout for the primary and secondary educational buildings are not finalized to verify the number of classrooms or tabulate the net square footage of building area outside of classrooms and hallways. Further, the school use is proposed as Pod A of the K-Park MUPD which will include a specific Project Standards Manual (PSM) establishing the unique parking requirements for the development. Based on initial estimates during conceptual planning, it is expected the school demand for parking as calculated within the PSM will result in a more appropriate parking requirement than the standard rate referenced above. The Applicant is proposing to calculate required parking for the primary and secondary school and accessory uses based on the following:

Proposed Parking Metrics

- 1 space for each faculty and staff member (426 faculty/staff members) = 426
- 1 visitor space for every 50 students (1750 Students) = 35
- 1 space for every 10 students in Grade 11 (168 Students) = 17
- 1 space for every student in Grade 12 (168 Students) = 168
- Total Parking Required = 646 Spaces

As shown above, the required parking based on the proposed parking metrics is 646 spaces. The school is currently proposing a total of 646 parking spaces. Note the number of proposed parking spaces may change slightly during the development process but will maintain at least 646 parking spaces. The majority of the parking will be located in the structured parking garage and the surface parking lot to the north of the school buildings.

It is important to note the school use is a component of the larger K-Park MUPD and, while not currently contemplated, it is feasible that opportunities for shared parking could be identified within other portions of the MUPD should it be necessary in the future. In consideration of the above, the proposed school site design more than accommodates the expected demand for parking associated with all elements of the school use.

PHASING

The Pod A School will be developed in two (2) phases. Phase 1 includes construction of the primary school, secondary school, a two-story sports and aquatic center, and the soccer field and track located south of the academic structures. Phase 1 will also provide the surface parking lot north of the primary school and a temporary surface parking lot in the areas planned for the future parking structure, along with the baseball and softball fields.

Phase 2 represents full build-out of the campus and will include the two-story theater, an expansion of the two-story sports and aquatic center with additional outdoor swimming facilities, an expansion of the secondary school, the parking garage structure at the southwestern corner of the site, the two (2) practice fields, the tennis courts, and the baseball and softball fields.

The proposed Phasing Plan will be reflected in the Pod A Site Plan application and incorporated into the Project Standards Manual.

LANDSCAPE BUFFERS

Pursuant to Chapter 8 of the Village's Land Development Regulations, specific landscape requirements are prescribed for various uses based on the proposed use of the site, applicable zoning district, adjacent uses, and specific site design characteristics. Further, Section 6.5.4.E.3 prescribes additional landscape standards for an MUPD. In connection with the Village's requirements for perimeter landscape buffers, the following perimeter buffer types are proposed for the school site:

- Stribling Way (North Property Line): 20-foot Type C buffer
- West Property Line: 10-foot Type A/F buffer with minimum 6' high masonry wall at property line
- Abutting Life Church Property (East and Northeast Property Lines): 10-foot Type A buffer
- South Property Line: Modified 10-foot Type A/F buffer with minimum 6' high masonry wall terminating at southern gate access into Pod A School

Please also see below for additional information relating the minimum code requirement for buffers for the school site consistent with the Project Standards Manual (PSM) and Master Buffer Plan for K-Park MUPD:

- Buffer Type C.1 – Pod A (North adjacent to Stribling ROW)
 - A 20' Type C perimeter buffer plus a 10' Utility Easement is proposed along the north side of Pod A adjacent to Stribling Way. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the Applicant proposes to maintain the width of the existing sidewalk within the Stribling Way ROW at 8' with locations to vary. The portion of the 8' sidewalk that lands within the subject site shall be dedicated to the Village of Wellington via Sidewalk Easement.
- Buffer Type AF.1 – Pod A (West & south adjacent to residential)
 - Whereas a 10' perimeter buffer is required for a Type A/F buffer, a modified 10' perimeter buffer is provided along the west & south side of Pod A adjacent to the residential neighborhoods of Oakmont Estates and Farmington Estates. The modified buffer retains the existing fence along the western property line to remain and proposes a Masonry Wall at a minimum 6' in height at the property line to further secure the Pod A School. Pod A also includes a proposed 6-foot-tall masonry wall along the southern property line, terminating at the gate access leading into the Pod A School. Accordingly, a deviation is requested to eliminate the eastern portion of the required wall along this southern boundary. An additional deviation is requested to allow the wall to be placed directly on the property line where the adjacent properties to the west and south already provide an existing buffer, rather than following the standard Type A/F buffer requirement that landscaping be located between the barrier and the adjacent property or right-of-way.
- Buffer Type A.1
 - Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer is provided along the east side of Pod A, adjacent to the church property.

- Buffer Type A.2
 - Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer consisting of a 100% overlap of the buffer in an existing 15' wide FPL utility easement is proposed for Pod A along the north side of the access easement adjacent to the Church. This will result in an elimination of code required buffer trees due to existing FPL overhead utilities & the proposed easement overlap. Native ornamental grasses and wildflowers may be provided to satisfy code required hedge & shrub requirements.

Please refer to the Site Plan and Landscape Plan prepared by Urban Design Studio included with this application for specific information related to the design of landscape buffers and plantings proposed within the school site. Please also refer to the concurrent rezoning and master plan applications for K-Park MUPD with attention to the Project Standards Manual (PSM) and Master Buffer Plan for additional details on deviations and proposed landscaping for the project.

CONDITIONAL USE STANDARDS

Pursuant to the Village's Development Review Manual, the following decision-making standards apply to any request for Conditional Use or Compatibility Determination. Please consider the Applicant's responses to all applicable standards as listed below.

- a) *That the proposed request is consistent with all elements of the Comprehensive Plan.*

Response: The proposed school use is consistent with the goals, objectives, and policies of the Village of Wellington Comprehensive Plan, particularly those promoting access to educational facilities, orderly growth, and compatibility of land uses. The Land Use & Community Design Element, specifically Policy 1.2.5, encourages the integration of limited non-residential uses, such as schools, in proximity to residential areas to support community needs and reduce travel distances. The Transportation Element supports development that utilizes existing road networks efficiently, and the proposed school aligns with this by providing safe and accessible connections for students, staff, and visitors. Additionally, Goal 4 of the Public Facilities Element encourages the connection of schools to the larger network of multi-modal circulation infrastructure within the Village and the proposed development proposes robust internal pedestrian and cyclist infrastructure connecting to the larger established network in furtherance of this goal.

In addition to the above, the Village Comprehensive Plan contains a dedicated Education Element with multiple goals, policies, and objectives directly supported by the proposed project as a high-quality contributor to the educational and cultural fabric of the Village. The proposed use will contribute to maintaining and increasing the superior quality of life and family-focus encouraged by this element of the Comprehensive Plan with further diversification of the educational opportunities in the Village. The Education Element emphasizes the importance of coordination between the Village and school providers to ensure adequate educational infrastructure. The proposed request supports all of the above directives and other related stated goals within the Village's Comprehensive Plan while maintaining consistency with the underlying land use designation and enhancing the overall livability of the area.

- b) *That the proposed request is in compliance with Article 6 of the LDR (Supplementary Regulations).*

Response: Pursuant to Section 6.2.2.F.5 of the Village's Unified Land Development Code (ULDC), the following supplementary standards apply to the principal use of a School,

Secondary or Primary. Specific responses are provided for each supplementary standard demonstrating compliance with the applicable standards.

- a. *For primary schools, the proposed site shall have direct access to at least a minor collector road. Outdoor recreational facilities and similar support facilities shall be located and buffered on the proposed site to minimize impacts on the adjacent properties.*

Response: The school site provides for access from both Stribling Way, a minor collector road, and State Road 7, an urban principal arterial road, to comply with the above supplemental standard. All outdoor recreational facilities are adequately buffered from adjacent properties to minimize any impact to surrounding uses. A minimum separation of 66 feet is proposed from the south property line with multiple layers of on-site landscape and driveway proposed within this separation. Additionally, a 25-foot-wide landscaped buffer is existing on adjacent property beyond the southern property line where proximate to adjacent residential uses to create additional buffer and separation from outdoor recreation facilities. The Applicant will coordinate with the Village to ensure that any proposed outdoor activity is appropriately managed to minimize any potential impact on the use of adjacent property.

- b. *For secondary schools, the proposed location shall have direct access to at least a major collector road. Stadiums, outdoor recreational facilities, and similar support facilities shall be located and buffered on the proposed site to minimize impacts on adjacent properties.*

Response: The school campus provides for both primary and secondary education. Please refer to the above response confirming compliance with the access requirements and details related to the proposed buffering and separation to minimize any potential impact of recreation facilities on adjacent properties.

- c. *Prior to approval of a building permit, a pedestrian access/bike path and cross-walk plan shall be submitted by the applicant showing access to the school site from surrounding neighborhoods. This system shall be integrated with existing or proposed pedestrian/bike path systems in the area, and shall be subject to the approval by the Wellington Engineer.*

Response: The school site proposes pedestrian and bicycle circulation infrastructure as depicted on the Master Circulation Plan and as supplemented by the School Operational Plan with this application. Additional details on such pathways will be provided at time of site plan application with further construction details included at time of building permit. This system will integrate with pedestrian and cyclist infrastructure in the surrounding right-of-ways as coordinated with the Wellington Engineer.

- d. *Vehicular circulation shall be designed to provide an independent traffic flow for school employees, visitors, and deliveries from the bus loading area, and parent drop-off area, where applicable.*

Response: Please refer to the details included above under the Access section of this narrative as well as the Traffic Operational Management Plan prepared by Simmons & White included with this application.

- e. *Dumpster and trash receptacles shall be located a minimum of 100 feet from residential property and screened from view with a six-foot solid wood fence and hedge combination unless part of a pre-existing commercial development.*

Response: All dumpster and trash enclosures proposed on the site will be located a minimum of 100 feet from the west or south property lines where adjacent properties are developed with residential uses. Such areas will be adequately screened with a minimum six-foot fence and landscaping to effectively screen such areas from adjacent properties.

- f. *A six-foot-high security fence shall be installed around the entire perimeter of the outside activity area to limit access.*

Response: A minimum six-foot-high fence and/or wall is proposed around the entire perimeter of the school site to secure the property. Additional security barriers are proposed where the activity area is accessible to pedestrians on the site.

- g. *Outside activity areas shall be located away from adjacent residential areas, whenever possible. Outside activity areas located adjacent to developed residential properties because of site design constraints shall provide a 50-foot buffer. This landscape buffer shall be supplemented with a six-foot-high hedge or hedge/berm combination and a double row of native canopy trees, spaced an equivalent of one tree per 20 linear feet of landscape buffer.*

Response: All outdoor recreational facilities are adequately buffered from adjacent properties to minimize any impact to surrounding uses. A minimum separation of 55 feet is proposed from the south property line with multiple layers of on-site landscape and driveway proposed within this separation. A 10-foot Type A/F landscape buffer with a minimum 6-foot-high masonry wall is currently proposed along a portion of the south property line terminating at the southern gate providing access into Pod A with plantings both within this perimeter buffer and along the north side of the driveway. Additionally, a 25-foot-wide landscaped buffer is existing on adjacent property beyond the southern property line where proximate to adjacent residential uses to create additional buffer and separation from outdoor recreation facilities. The Applicant will coordinate with the Village to ensure that any proposed outdoor activity is appropriately managed to minimize any potential impact on the use of adjacent property.

- h. *Subject to site plan conditions, schools with 100 or fewer on-site students are a permitted use. Schools with 101 or more on-site students shall be considered a conditional use. All schools must demonstrate consistency with the existing site plan/conditions of approval if applicable.*

Response: The proposed school campus is planned to accommodate approximately 1,750 students in grade levels from early childhood through high school at the time of full enrollment. This application serves to request approval as a conditional use and provides supporting justification to address all applicable criteria for approval of the use and site design as a component of the K-Park MUPD.

- i. *Existing public schools are considered conforming uses and shall not require conditional use approval to expand or modify their facilities.*

Response: The proposed school will be operated as a private school and, as such, is subject to the requirement for conditional use approval. This application serves to request approval as a conditional use and provides supporting justification to address all applicable criteria for approval of the use and site design as a component of the K-Park MUPD.

- c) *That the proposed request will ensure general compatibility with adjacent properties and other property in the district (use and character).*

Response: The proposed conditional use request to allow a school use within the subject property ensures general compatibility with adjacent properties with respect to both use, operation, and character of the proposed development. The school use is consistent with the Village of Wellington's planning framework, which promotes institutional and community-serving uses in proximity to residential neighborhoods to enhance accessibility and support neighborhood cohesion.

The proposed development has been thoughtfully designed to integrate into the surrounding area with consideration for scale, architectural style, buffering, and traffic management. Site design includes increased building setbacks, perimeter landscaping, additional landscape plantings throughout the site, and screening to minimize any visual or operational impacts on adjoining residential properties. Furthermore, the operational characteristics of the school including, but not limited to, staggered start times, controlled access, internalized outdoor play areas, and noise mitigation efforts enhance the compatibility with the predominantly residential character of properties to the north and west.

Overall, the school will serve as a neighborhood asset to this corridor and the Village at large, providing educational services in a manner that complements and supports the existing community fabric without introducing adverse impacts to adjacent uses.

- d) *That the design of the proposed request will minimize adverse effects, including visual impact and intensity of the proposed use on adjacent lands.*

Response: The school has been sited and designed to mitigate any adverse impacts to adjacent properties through the use of landscape buffering, appropriate building orientation, and separation from residential uses. Architectural design elements reflect a scale and style consistent with surrounding development of the K-Park MUPD and is expected to set a new standard for high-quality design along this corridor. Extensive efforts will be employed to ensure elements such as exterior lighting and signage will be limited to prevent visual intrusion. Ingress and egress points are positioned appropriately and complemented by a comprehensive traffic management plan to minimize traffic congestion and conflict with local residential circulation patterns. Collectively, these design strategies ensure the school use will not adversely affect the visual or functional quality of adjacent lands.

- e) *That required provisions have been made for public facilities.*

Response: The proposed development provides adequate access to and capacity for all necessary public facilities, including transportation networks, potable water, sanitary sewer, stormwater management, and solid waste disposal. Coordination with the Village of Wellington and relevant utility providers confirms that existing infrastructure can accommodate the proposed school without diminishing service levels to surrounding properties. Emergency

services access has been incorporated into the site plan, and the development complies with applicable fire and life safety codes.

- f) *That the design of the proposed request will minimize environmental impacts, including but not limited to water, air, stormwater management, wildlife, vegetation, wetlands and natural functioning of the environment.*

Response: Environmental stewardship has been a priority in the design process. The site layout seeks to preserve existing mature vegetation where feasible and incorporates native landscaping to reduce irrigation needs. Stormwater is managed on-site through a system designed to meet or exceed South Florida Water Management District and Village requirements, thereby preventing off-site discharge and protecting water quality. The project has been previously cleared and utilized for agriculture and, as such, there are no environmentally sensitive areas, protected species, or wetlands existing on or adjacent to the property that would be adversely affected by the proposed development. Sustainable building practices will further reduce air and water impacts associated with construction and operation.

- g) *That the proposed request will result in logical, timely and orderly development patterns.*

Response: The school use represents an appropriate and beneficial addition to the area, supporting the Village's comprehensive planning goals and filling a community need in a timely manner. The location promotes orderly development by utilizing an infill site with existing access and infrastructure while ensuring compatibility with nearby residential, civic, and commercial uses. The project enhances connectivity, reinforces existing land use patterns, and contributes to a balanced and complete neighborhood framework. Innovative architecture and conscious site design ensure the physical improvements will be compatible with all surrounding uses and integrate seamlessly into the existing urban development pattern of this area of the Village.

- h) *That the proposed request complies with all Code standards for use, layout, function and general development characteristics.*

Response: The proposed school complies with the applicable provisions of the Village's Land Development Regulations as established within the Project Standards Manual (PSM) for the unique K-Park MUPD. The design is compliant with the proposed dimensional requirements for minimum landscape buffers, supplemental uses standards, setbacks, building height, parking, and traffic circulation. The layout reflects best practices in site planning, ensuring functionality, safety, and accessibility. Innovative design characteristics such as internalized playgrounds and courtyards and unique architecture along with operational structure including hours of operation, traffic management, and secured site access contribute to a thoughtful development plan designed to meet code and reduce potential conflicts with adjacent uses.

- i) *That the proposed request is not out of scale with the needs of the neighborhood or Wellington.*

Response: The scale and capacity of the proposed school are aligned with the demographic and service needs of the surrounding neighborhood and the broader Wellington community. The facility is designed to serve a defined student population and is not intended to function as a regional or large-scale institutional use. As such, it reflects a context-sensitive response to local educational demand, supporting community objectives without overwhelming the existing infrastructure or altering the character of the area. The scale of the proposed structures is appropriate in consideration of their unique and innovative design and configuration within the site. Lush landscape buffers, foundation plantings, and considerable setbacks further soften the experienced scale of the proposed structures located centrally within the school site.

CONCLUSION

On behalf of the applicants, Urban Design Studio requests favorable review and consideration of this request to approve the proposed Conditional Use. The Project Managers at Urban Design Studio can be reached at 561-366-1100. Please feel free to contact these Agents with any questions or for additional information in support of this application.

K-PARK MUPD

JUSTIFICATION STATEMENT

Request: Master Plan

Original Submittal: May 16, 2025

Resubmittal: August 18, 2025

Resubmittal: September 29, 2025

Resubmittal: November 19, 2025

Resubmittal: December 17, 2025



Urban Design
Land Planning
Landscape Architecture

OVERVIEW OF REQUEST & PROPERTY INTRODUCTION

On behalf of the Applicants, Wellington Property Owner, LLC and Founders Acreage Wellington, LLC, Urban Design Studio (UDS), as Agent, requests a Master Plan approval consistent with the concurrent Rezoning request to Multiple Use Planned Unit Development (MUPD) for the K-Park MUPD Project. Concurrent applications to approve a Conditional Use for a School, Secondary or Primary, and a Hotel are also being submitted as part of the MUPD. The subject property is comprised of a 71.27-acre property of which 5.30 acres of property is currently zoned Community Commercial (CC), and 65.74 acres designated as Community Facilities (CF). The site is located at 10400 Stribling Way, and is generally located at the southwest intersection of Stribling Way and State Road 7, referenced by parcel control numbers (PCN) 73-42-43-27-05-026-0011 and 73-41-44-24-06-003-0000

The purpose of this application is to allow for the development of an MUPD consisting of a 405,000 SF / 1,750 student School, 215 Residential Units, 210,000 SF of retail, 75,000 SF of office, an 180-room hotel, and 105,000 SF of restaurant uses plus up to 20,000 SF of outdoor dining. An overall conceptual Site Plan is included with this request for the MUPD.

SURROUNDING USES

The Subject Property is situated along the eastern boundary of the Village at the southwest corner of Stribling Way and State Road 7 (US 441). Within the Village, this portion of the State Road 7 corridor has been developed with primarily non-residential uses at a larger scale including the Palomino Park Medical Center and Marketplace at Wycliffe to the south as well as the Village Green Center, the Mall at Wellington Green, and Wellington Regional Medical Center to the north. Similarly, the Stribling Way corridor provides for a number of nearby institutional uses including the adjacent Life Church Wellington, Emerald Cove Middle School and Olympia Park to the east, and the Village Park Athletics Complex to the west. In addition to the considerable non-residential uses in the surrounding area, properties to the west, north, and south are developed with single-family residential uses on lots of one-fifth to one-quarter of an acre and separated from the site by road right-of-way or substantial landscape buffers. The Subject Property is strategically positioned to provide for the proposed mix of uses contemplated for the K-Park MUPD, including the proposed school use on the western portion of the site.

Please refer to the following summary table with information on the land use and zoning of adjacent properties in closest proximity to the Subject Property.

| | Existing Use | Land Use | Zoning |
|-----------|---|-------------------------------|---------------------------|
| Northwest | SF Residential – Castellina | Residential C | Residential PUD |
| Northeast | Commercial – Village Green Center | Community Commercial (CC) | Community Commercial (CC) |
| South | SF Residential – Farmington Estates (PBC) | Residential Low Density (PBC) | Residential PUD (PBC) |
| Southeast | Life Church Wellington | Community Facilities (CF) | Community Facilities (CF) |
| East | State Rd 7 ROW - Olympia Park | Community Facilities (CF) | Community Facilities (CF) |
| West | SF Residential – Oakmont Estates | Residential C | Residential PUD |

BACKGROUND & HISTORY

On September 9, 2003, the Acme Improvement District (Acme) acquired 66.27 acres for of the subject property. The additional acreage for the 71.28-acre property was formerly road right-of-way south of Stribling Way and a strip of land providing access to State Road 7 on the south side of the property.

In February 2006, Wellington entered into a contract with Sunrise Development, Inc. to sell 5.3 acres of the site along State Road 7 to develop an assisted living facility for seniors. As a part of that project, Wellington approved a Comprehensive Plan amendment for the intended site in October 2006, changing the designations from Community Facilities to Community Commercial. However, the buyer elected not to proceed with the project and withdrew their offer to purchase on November 17, 2006.

In December 2010, Wellington began leasing the property to J. Alderman Farms, Inc. on an annual basis. Currently, Alderman leases the property for approximately \$55,000 per year.

In January 2025, Wellington executed two Purchase and Sale Agreements with Wellington Property Owner, LLC and Founders Acreage Wellington, LLC to purchase the property upon receiving entitlements for a mixed-use development. The first phase of the entitlements involved a Village-initiated amendment of the Future Land Use Map of Wellington's Comprehensive Plan. Previously, 5.3 acres of the property were designated Commercial (C), and 65.98 acres were designated Community Facilities (CF). The proposed land use change was processed to support the development of the property as a comprehensive mixed-use project with multifamily residential, retail, hotel, restaurant, and a school. On March 11, 2025, the Village Council adopted Ordinance No. 2025-04 to approve the Comprehensive Plan Map Amendment redesignating the entirety of the Subject Property with the Mixed-Use (MU) Future Land Use.

PROPOSED DESIGN

Site Design

The Master Plan is thoughtfully divided into distinct yet interconnected development pods—Pod A (School), Pod B (Mixed-Use), and Pod O (Common Open Space), and a municipal pod which includes an existing public lift-station.

Pod A

The school site, occupies approximately 43.76 acres on the western portion of the larger K-Park MUPD. Of the 43.76-acres of the total Pod A area, ~0.31 acres is allocated to proposed ROW dedication for Stribling Way while ~1.96 acres are allocated to the perimeter buffer areas. The School Pod is planned to feature an innovative layout with high-quality architecture and thoughtfully designed site improvements. The campus will be fully secured along all perimeters, ensuring separation from adjacent uses, while still integrating with the broader development through the centrally located lake, which serves as a unifying element. The school program will provide a total building area of approximately 405,000 square feet complimented by sports courts and fields.

Pod O

Positioned between Pods A and B is the Pod “O” Common Open Space, totaling 3.23 acres. This area includes a 2.5-acre lake with a minimum 8-foot walking path that extends around its perimeter. Pod O lies east of the spine road which provides access into the interior of Pod B from Stribling Way. On the west side of this spine road, within Pod A, is a 3.31-acre lake. Together, these features create an elevated entry experience when utilizing the centralized access from Stribling Way as lake views are provided along both sides. Further details are regarding Pod O are provided in the Project Standards Manual included in the submittal package.

Pod B

Pod B totals 24.05 acres and serves as the mixed-use and social hub of the project with a mix of horizontally and vertically integrated uses offering a regional destination for the residents of the Village of Wellington to eat, work, and play. Of the 24.05-acres of the total Pod B area, 0.25 acres is allocated to the proposed ROW dedication while 1.58 acres are allocated to the perimeter buffer areas. Pod B is designed as a Mixed-Use Village with storefronts and buildings oriented around an organic network of public spaces such as pedestrian promenades, plazas, parklets, pathways, wide sidewalks and gardens serving as key focal points for the development.

The proposed mixed-use center is planned to include approximately 210,000 SF of retail space, 105,000 SF of restaurant gross square footage plus up to 20,000 SF of outdoor dining, 75,000 SF of office space, and a 175,000 SF / 180 room hotel for which a concurrent Conditional Use Application has been submitted.

Municipal Pod

The municipal Pod within the MUPD totals 0.19 acres excluding ROW dedication & buffer area. The municipal Pod is carved out to allow for an existing public lift-station located on Stribling Way

to remain. The access and configuration of the municipal Pod is further detailed in the Project Standards Manual for the MUPD.

Residential Phasing

As part of the requested Master Plan approval for the K-Park MUPD, a total of 215 multifamily residential units are proposed. The development of these units will occur in phases: 15 vertically integrated units will be constructed in the first phase alongside the Mixed-Use Village, while up to 200 remaining units are planned for a second phase within the surface parking areas located on the north and south sides of the Pod O open-space lake, as illustrated and hatched on the Master Site Plan. At the time the Phase 2 units are developed, it is anticipated that a separate Site Plan Amendment application will be required. This application will provide detailed information regarding the proposed residential units, their configuration, parking, and all associated data on the Master Site Plan.

Access & Circulation

Access to the MUPD is proposed through three driveway connections located along Stribling Way and one driveway connection along State Road 7. An additional off-site driveway is intended to provide cross access for the MPUD via the driveway connection to the adjacent Life Church property located just south of the site on State Road 7. Along Stribling Way, Driveway #1 at the western end of School Pod A is proposed as a full-access driveway with signal with considerations of a potential roundabout proposed. Driveway #2 along Stribling is proposed as a full-access driveway into Pod B. The eastern most Driveway #3 along Stribling is proposed as a right-in, right-out driveway to Pod B positioned closer to the intersection of Stribling Way and State Road 7. Along State Road 7, Driveway #4 is proposed as a right-in, right-out driveway located approximately 500 feet south of the intersection of Stribling Way and State Road 7 for Pod B. The aforementioned off-site driveway serving the adjacent Life Church property is proposed to be a modification to the Church's existing driveway and is proposed to be full-access signalized driveway located approximately 1,100 feet south of the intersection with Stribling Way.

The proposed improvements to the adjacent Church access are currently being coordinated with the adjacent Church property owners, FDOT and the Village. The off-site full-access traffic signal with improvements located at the Life Church property along State Road 7 is contingent upon FDOT permitting and agreements with the neighboring Church property which are anticipated to be provided prior to Master Plan approval from the Village Council. This proposed access allows for the best solution to consolidate primary project access serving all pods while also providing a new access point for Life Church with cross-access provisions and a roundabout design that optimizes traffic flow. Life Church benefits from the full access traffic signal as the church currently experiences high attendance numbers with six services each Sunday. This access would provide safe ingress and egress during their peak arrival and dismissal times. As part of the proposed MUPD development, changes to the existing access shown on the approved Site Plan for the Church site are expected. The applicant acknowledges that a formal Site Plan amendment application for the Church site will be processed administratively at a later date to depict the proposed improvements that affected the Church site.

The MUPD will be developed with a robust and comprehensive pedestrian circulation system. The Project maintains the 8' sidewalk within the Stribling Way ROW for the northern perimeter of the MUPD along a lushly planted Type C landscape buffer. The pedestrian circulation design is further enhanced by a series of sidewalks within the MUPD, and a minimum 8' wide walking path surrounding the Pod O Common Open Space Lake, and a pedestrian network within the Pod B Mixed-Use Village which will be designed to maintain a minimum 8' clear path along storefronts and the proposed Spine Roads.

Parking

A deviation is requested to the code required parking for each development Pod as outlined by the parking studies prepared by Simmons & White included in the current site plan applications. The parking analysis considered parking rates from the ITE Parking Generation, 6th Edition and the Urban Land Institute's (ULI) Shared Parking, 3rd Edition publication.

Buffering & Landscape

As part of the requested MUPD, a master buffer plan has been provided for the MUPD site perimeter, assessing the use and surrounding property for each pod. Section 6.5.4.E.3, which prescribes that landscape standards shall exceed the Article 7 landscape standards of the LDR by a minimum of 30 percent of the general standard, will be met through a combination of 30% increases in size or 30% increases in quantity as applicable for each pod. The 30% increases will be further detailed on the conceptual landscape plans provided at time of Site Plan approval or Conditional Use approval.

The following buffers required for the MUPD are listed below and are specified by each applicable buffer type as indicated on the K-Park MUPD Master Buffer Regulating Plan (Refer to sheet MP-2).

- Buffer Type C.1 – Pod A (North adjacent to Stribling ROW)
 - A 20' Type C perimeter buffer plus a 10' Utility Easement is proposed along the north side of Pod A adjacent to Stribling Way. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the Applicant proposes to maintain the width of the existing sidewalk within the Stribling Way ROW at 8' with locations to vary. Any portion of the 8' sidewalk that lands within the subject site shall be dedicated to the Village of Wellington via Sidewalk Easement.
- Buffer Type C.3 – Pod B (North adjacent to Stribling ROW)
 - A 20' Type C buffer with a 5' utility easement overlap is provided along the north side of Pod B adjacent to the Stribling Way ROW. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the Applicant proposes to maintain the width of the existing sidewalk within the Stribling Way ROW at 8' with locations to vary. The portion of the 8' sidewalk that lands within the subject site shall be dedicated to the Village of Wellington via Sidewalk Easement.
- Buffer Type C.4 - Pod B (East adjacent to State Road 7 ROW)

- A 20' Type C buffer with a 5' utility easement overlap within the buffer is provided along the east side of Pod B adjacent to the State Road 7 ROW. In lieu of the additional 10' added to the minimum width of the perimeter landscape area for a pedestrian path, the existing sidewalk within the ROW will connect into Pod B where a minimum 8' wide publicly accessible sidewalk shall be provided along the storefronts of the commercial buildings.
- Buffer Type AF.1 – Pod A (West & south adjacent to residential)
 - Whereas a 10' perimeter buffer is required for a Type A/F buffer, a modified 10' perimeter buffer is provided along the west & south side of Pod A adjacent to the residential neighborhoods of Oakmont Estates and Farmington Estates. The modified buffer retains the existing fence along the western property line to remain and proposes a Masonry Wall at a minimum 6' in height at the property line to further secure the Pod A School. Pod A also includes a proposed minimum 6-foot-tall masonry wall along the southern property line, terminating at the southern gate access leading into the Pod A School. Accordingly, a deviation is requested to eliminate the eastern portion of the required wall along this southern boundary. An additional deviation is requested to allow the wall to be placed directly on the property line where the adjacent properties to the west and south already provide an existing buffer, rather than following the standard Type A/F buffer requirement that landscaping be located between the barrier and the adjacent property or right-of-way.
- Buffer Type AF.2 -Pod B (South adjacent to church parcel)
 - Whereas a 10' perimeter buffer is required for a Type A/F buffer, a 10' perimeter buffer is located at the shared property line is provided along the south side of Pod B adjacent to the church property. A deviation to eliminate the required fence within the buffer is requested in order to allow integration between the Mixed-Use Village and the Church property.
- Buffer Type A.1 – Pod A (East adjacent to church parcel)
 - Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer is provided along the east side of Pod A, adjacent to the church property.
- Buffer Type A.2 – Pod A (South access easement, north side adjacent to church parcel)
 - Whereas a 10' perimeter buffer is required for a Type A buffer, a 10' perimeter buffer consisting of a 100% overlap of an existing FPL utility easement is proposed for Pod A along the north side of the access easement adjacent to the Church. This will result in an elimination of code required buffer trees due to existing overhead utilities & the proposed easement overlap. Native ornamental grasses and wildflowers may be provided to satisfy code required hedge & shrub requirements.

An alternative landscape plan (ALP) has been prepared demonstrating the intent of the landscape standards established in the ULDC as prescribed in Section 7.8.6.B. by meeting the following eight (8) design guidelines and principles:

1. Demonstrates an innovative use of plant materials and design techniques in response to site characteristics;

Response: The proposed landscape design demonstrates an innovative use of plant materials and design techniques that respond to the site's unique design characteristics. In Pod A, contextual landscape plantings are provided to complement the building configuration, athletic facilities, courtyards, and pedestrian pathways throughout the site. As illustrated in the alternative foundation planting design, the intent of the code is met through the provision of equivalent square footage of landscaping.

Similarly, Pod B is designed with integrated and innovative landscaping that frames walkable paths and storefronts within the mixed-use village, creating a lush and inviting pedestrian environment. Multiple open space plazas are accentuated with landscape plantings that define and enhance the pedestrian realm. The Pod B foundation planting design also satisfies code intent by providing equivalent landscape square footage in lieu of planting directly adjacent to the buildings.

2. Uses a variety of plant material, including plants of color, plants of form and plants of texture;

Response: The landscape design incorporates a variety of plant materials that provide differences in color, form, and texture. Buffer areas are planted with Southern Live Oaks, Silver Buttonwoods, Gumbo Limbo trees, and Trumpet Trees to create a layered and visually diverse landscape edge. Vehicular areas between both pods are framed with trees such as Royal Palms, Gumbo Limbos and Pigeon Plums, which provide visual interest along drive aisles and circulation routes. Understory planting provides color and different textures in shrub leaf(s), providing a unique design throughout the MUPD.

3. Incorporates naturalistic design principles, such as natural variations in topography, meandering or curvilinear buffer plantings and groupings of dominant plant materials including trees and large shrubs in a manner consistent with existing native vegetation;

Response: The proposed landscape design incorporates naturalistic design principles through the provision of groupings of dominant plant materials as depicted by the Gumbo Limbo and Southern Live Oak tree groupings in the Pod A perimeter buffer and by the groupings of Trumpet Trees and Silver Buttonwood Trees within the Pod B perimeter buffer.

4. Integrates landscaping and pedestrian facilities in a manner compatible with the location in which the development is located;

Response: The MUPD is designed to integrate landscaping and pedestrian facilities through the strategic placement of shade trees along sidewalk paths and throughout the courtyard and elementary playground within Pod A. Similarly,

the ALP for Pod B incorporates integrated landscape and pedestrian features by providing planters along the opposite side of the buildings adjacent to the minimum 8-foot-wide pedestrian walkway, helping to frame the walkway. Additional landscape plantings are provided to define and enhance outdoor plazas and seating areas throughout the pod.

5. Illustrate that 50 percent of the required trees exceed minimum height requirements;

Response: At least 50% of the trees installed shall exceed minimum height requirements as depicted on the ALP. Of the 744 shade trees required, 484 shade trees will be proposed at 18' in height, whereas the code minimum establishes 16' in height for buildings in between 26-35' in height. Buildings above 35' in height will be subject to separate Conditional Use Height approval, with the associated landscape requirements to be assessed at that time.

6. Creates greater compatibility with abutting properties and is consistent with the design principles and guidelines of this chapter and the DRM;

Response: The proposed ALP creates compatibility with abutting properties by providing shade trees and buffer plantings that not only screen the subject site, but also compliment the existing buffer material provided on the sites adjacent to the south and west. The proposed landscape design provides an extensive landscape palette with significant canopy and shade, and thoughtful placement of trees and shrubs that soften and enhance the proposed architecture on site, demonstrating compliance with the guidelines of the Chapter 7 Landscape regulations.

7. Provides a consistent aesthetic appearance from lot perimeters and adjacent roadways;

Response: A consistent aesthetic appearance from lot perimeters and adjacent roadways will be provided through the provision of shade trees such as Gumbo Limbo, and Southern Live Oak trees within the Pod A perimeter buffers, and Trumpet Trees and Silver Buttonwood Trees within the Pod B Perimeter buffers. Creating a cohesive design throughout the MUPD.

8. Uses water-efficient irrigation systems and Florida Friendly landscaping principles at appropriate locations; and

Response: The proposed landscaping has been designed to place landscape material in groupings to water efficiently with an irrigation system as well as using Florida Friendly plant material.

Recreation

Pursuant to Section 6.5.2 for Planned Development Districts, proposed development program must consist of five acres of property for park and recreational facilities and one acre of civic

facilities, each 1,000 of the population proposed for the project. Section 6.5.5 prescribes that private recreation areas shall be provided at a minimum of 110 SF of area/person.

As mentioned earlier in the narrative, the MUPD is proposed to developed in two residential phases, with 15 units proposed to be constructed alongside the Mixed-Use Village, while up to 200 remaining units are to be constructed in a second phase. As such – the required land dedication calculations have been provided for each respective phase: Phase 1 consisting of 15 units, and Phase 2 consisting of up to 200 units.

The proposed development program for Phase 1 amounts to an estimated population of 42 persons based on the target unit yield of 15 units, and the provided persons per dwelling unit count generated by the U.S. Census of 2.81 persons per dwelling unit.

The private recreation requirement for Phase 1 based on the Village provided formula amounts to 0.11 acres. The Applicant exceeds the 0.11-acre private recreation requirement by providing a total of 1.26 acres of private recreational amenities, consisting of:

- 0.67 acres for the fitness center in Pod B
 - 0.25 acres for the hotel event space in Pod B
 - 0.34 acres for the hotel pool in Pod B
- Total private recreation provided: 1.26 acres

The Civic requirement based on the Village formula provided results in a total of 0.042 acres, and the public recreation requirement based on the Village formula provided results in a total of 0.21 acres. Combined, the Project totals .252 acres of required public park and civic land dedication for Phase 1. The Applicant satisfies the 0.252-acre public park and civic land dedication requirement by activating the 2.9-acre lake in Pod O with a lakeside park and walking trail and requesting a 50% credit, which yields an effective credit of 1.45 acres. The Applicant also provides a series of interconnected open spaces throughout the site consisting of the main street streetscape, promenade, various plazas, and the central green adjacent to the east side of the Pod O lake. This area totals 5.07 acres. The Applicant is requesting a 75% credit for these areas, yielding an effective credit of 3.8 acres. The .23-acre municipal pod is also included at 100% credit in order to satisfy the civic/public-park land-dedication requirement. As a weighted average, the developer provided public-recreation areas amount to a 67% credit requested across the lake, streetscape & plaza, and municipal parcel areas provided.

The proposed development program for the full 215-unit buildout of Phase 2 amounts to an estimated population of up to 604 persons based on the target unit yield of up to an additional 200 units.

The private recreation requirement for full residential build-out for Phase 2 based on the Village provided formula amounts to 1.53 acres. In addition to the private recreation planned for Phase 1, it is anticipated that at the time of the development of the Phase 2 residential units, an additional .26 acres of residential amenity in each of the proposed residential expansion zones may be added. As such, The Applicant will exceed the 0.11-acre private recreation requirement by providing a total of 1.55 acres of private recreational amenities, consisting of:

- 0.67 acres for the fitness center in Pod B
- 0.25 acres for the hotel event space in Pod B
- 0.34 acres for the hotel pool in Pod B

- 0.26 acres for the future planned residential amenities in Pod B
Total private recreation provided: 1.55 acres

The Civic requirement for the full residential build-out for Phase 2 based on the Village formula provided results in a total of 0.604 acres, and the public recreation requirement for the full residential build-out for Phase 2 based on the Village formula provided results in a total of 3.02 acres. Combined, the Project totals 3.62 acres of required public park and civic land dedication for the full residential build-out for Phase 2. The 3.62 public recreation requirement is met through the 5.48-acres of Pod O Lake activation, the public open spaces and streetscapes throughout Pod B and the Utility Parcel. The Applicant would like to retain flexibility to reallocate some of the provided area to satisfy the civic/public park dedication requirements in order to accommodate potential program or site layout changes that might arise after the initial approval process.

Allowing the requested flexibility will allow the Applicant to best react to market conditions and changed demand to truly create the most effective and vibrant mixed-use village for the Village to benefit from. This flexibility also supports the project's phased buildout. Although up to 215 residential units are contemplated in the future, they will not all be constructed or occupied on day one. The more stringent public park and civic space dedication requirements are calculated based on the residential population at full buildout. By tying dedication obligations to the actual phased delivery of residential units rather than to the maximum theoretical buildout, the Applicant can proceed efficiently while ensuring that adequate public amenities are provided as the project grows and the population materializes.

As such, The Applicant would like to propose the following condition to allow the requested flexibility in the civic land dedication requirements:

The developer/owner is required to provide public land dedication for parks and recreation facilities as well as civic facilities for up to 215 DU of residential development within the project. The public land dedication required for parks and recreation is 3.02 acres. The civic land dedication required is .604 acres. Required land dedication (3.62-acre total) shall be satisfied per the following:

a. A 67% (approximately 5.48 AC) credit is approved for the developer provided public and private recreation areas with improvements and amenities as shown on the project's approved master plan, PSM and site plan (including landscape plan). The credit will be given towards the total 3.62-acre required for the Public Recreation and Civic land dedication requirement.

b. The exact credited acreage may be adjusted at final site plan approval to reflect the actual area constructed as building placement or minor site design refinements occur, provided that the credited acreage shall not be less than 3.62 acres. The Applicant may pull from the pool of 5.48 acres of public amenities provided to meet the 3.62-acre requirement. The credited area may be relocated within the project boundaries so long as the relocated area continues to meet the intent and functional requirements of the Village's civic and recreation standards, any such adjustments or relocations shall be reviewed and approved by the Village through the applicable site plan.

PSM Deviations

As a proposed MUPD, The Project design proposes flexible regulations to encourage distinctive design that results in the efficient use of land, parking, landscaping, open space, access, architectural compatibility, and adaptable space for future uses and sustainability. The Project also is designed to meet the intent to provide infill development, deter urban sprawl, and lessen the need for additional vehicular trips through the internalization of trips within the project. As such, the requested flexible regulations and identified deviations from the Wellington ULDC at this time are detailed below and also listed in the Project Standards Manual (PSM) of the site.

POD A DEVIATIONS

1. Required Parking

- As part of the K-Park MUPD PSM, a deviation to the school parking requirements as listed in Table 7.5.1 is requested. In lieu of the code requirement to park Elementary Schools at one space per classroom, plus one space per 200 NSF of building not accounted for in class or hall areas, and Secondary Schools at 0.25 per student, plus one per 200 NSF of building not accounted for in class or hall areas, a deviation is requested to the required parking as outlined by the School Operational Plan prepared by Simmons & White included in the concurrent site plan application for Pod A. The parking analysis considered parking rates from the ITE Parking Generation, 6th Edition and the Urban Land Institute's (ULI) Shared Parking, 3rd Edition publication.

The Applicant is proposing to calculate required parking for the primary and secondary school and accessory uses based on the following:

Proposed Parking Metrics

- 1 space for each faculty and staff member (426 faculty/staff members) = 426
- 1 visitor space for every 50 students (1750 Students) = 35
- 1 space for every 10 students in Grade 11 (168 Students) = 17
- 1 space for every student in Grade 12 (168 Students) = 168
- Total Parking Required = 646 Spaces

As shown above, the required parking based on the proposed parking metrics is 646 spaces. The school is currently proposing a total of 646 parking spaces at full build-out.

2. Required Bicycle Parking

- As part of the K-Park MUPD PSM, a deviation from the bicycle parking requirement is requested. Section 7.5.1.D. requires one bicycle parking space per 2,000 square feet of gross floor area. Based on the school's approximately 405,000 square feet, a total of 203 bicycle parking spaces would be required by code.

The Applicant proposes a reduced and more appropriate standard equal to 10 percent of the anticipated middle and high school student population of 1,072 students, resulting in 108 bicycle parking spaces. This approach reflects a realistic assessment of anticipated bicycle use and limits the calculation to middle and high school students who are more likely to bicycle to campus and have the ability to safely navigate traffic conditions, as opposed to elementary-age

students, where parents may not feel comfortable letting their children bike to school alone due to safety concerns. The proposed bicycle parking will be located in convenient and accessible areas for student use.

3. Parking Stall Width

- As part of the K-Park MUPD PSM, a deviation to the required parking stall width for Pod A is requested. As prescribed in Table 7.5.2 – required parking stall width shall be 9.5'. The proposed K-Park MUPD PSM establishes a proposed regulation of 9' minimum width for the Pod A School Parking Garage and for the east and west surface parking lots that are temporary in Phase 1. At full build out, the proposed modification is limited only to the parking garage, while the north surface parking lot spaces will meet the standard 9.5-foot width requirement. This approach preserves compliance where maneuverability is most critical and applies the reduction only in areas where it can be safely accommodated.

The garage is intended primarily for faculty, staff, and high-school student drivers, all of whom are regular daily users familiar with the campus circulation pattern. Concentrating parking vertically with 9' spaces within the garage also reduces the need for additional impervious surface area, thereby decreasing stormwater impacts and enhancing the site's landscape and recreational areas. The requested modification maintains functionality, safety, and accessibility of parking areas while advancing the pedestrian-friendly character of the development.

4. 90 Degree Parking Drive Aisle Width

- As part of the K-Park MUPD PSM, a deviation to the required drive aisle width for 90-degree parking spaces for Pod A is requested. As prescribed in Table 7.5.2 – the required drive aisle width for 90-degree parking spaces shall be 25'. The proposed K-Park MUPD PSM establishes a proposed regulation of 24' minimum width.

The minor 1' reduction in drive aisle width for the 90-degree spaces is consistent with accepted engineering and planning practice and remains adequate to accommodate safe and efficient two-way vehicular circulation. The modification allows for more efficient site planning, enabling the preservation of pedestrian-oriented open spaces and landscaping without compromising the maneuverability of passenger vehicles or the functionality of parking areas. The reduced drive-aisle also serves as a traffic calming technique to ensure that vehicles traversing along the drive-aisle do not speed and to also emphasize the safety and importance of the pedestrian experience within the pod.

5. Loading Space Quantity & Loading Space Dimensions

- As part of the K-Park MUPD PSM, a deviation to the required loading space quantity for Pod A is requested. As prescribed in Section 7.5.3 – the required loading regulations is one space for every 15,000 square feet of gross floor area (GFA) up to 100,000 square feet; and one space for every 50,000 square feet of GFA over 100,000 square feet. Additionally, Table 7.5.3. prescribes a minimum loading space size of 15' x 55'.

The required loading space requirements and dimensions and proposed loading deviations for the School use development program is as outlined below

| School – 405,000 GSF | | | |
|---|-----------------------|----------------------|-----------------------|
| Zoning | Required Count | Proposed Size | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' x 55' @ 1/50,000 SF OVER 100K | 6.1 | | |
| TOTAL | 13 | 15'x50' | 3 |

The proposed reduction in loading space and loading quantity are directed by Applicant coordination with internal operations teams and consultants who have established day-to-day input and direct experience managing comparable school campuses. The proposed loading deviations proposed are a minimum can sufficiently accommodate operations without impairing functionality.

Additionally, the Applicant has relied on established use patterns and knowledge of standard delivery truck sizes that regularly serve these types of tenants. The 15' x 50' are consistent with industry practice for accommodating box trucks, service vans, and smaller delivery vehicles that make up the majority of trips anticipated for the school.

6. Fence/Wall Location within Landscape Buffers

- As part of the K-Park MUPD PSM, a deviation from fence location within a landscape buffer is requested. As prescribed in Section 7.8.5.C.7. – If a fence or wall is desired or required, the landscaping shall be located between the barrier and the adjacent property or ROW. The proposed K-Park MUPD PSM proposes that the fence or wall may be located at the property line where the adjacent property has an existing buffer for the west and south property lines for Pod A. Landscape buffers along the western and southern property lines of Pod A exist today for the Oakmont Estates and Farmington Estates communities. As detailed in buffer type AF.1 – the Applicant proposes to provide a masonry wall a minimum of 6' in height at the western property line, and to provide a masonry wall a minimum of 6' in height terminating at the Pod A access gate at the southern property line. A fence or wall will also be proposed at the property line along the eastern perimeter of Pod A adjacent to the church site.

7. Fence/Wall Height

- As part of the K-Park MUPD PSM, a deviation from the 6-foot maximum fence height permitted adjacent to a Major Thoroughfare is requested. The Applicant proposes a minor 1-foot increase to allow a 7-foot-high fence along Stribling Way, which is classified as a collector road and identified as a Major Thoroughfare on the Wellington Functional Road Classifications Map within the Comprehensive Plan. This minor increase provides enhanced security for the school campus and allows fencing throughout the site to remain consistent at 7 feet in height, avoiding an isolated reduction to 6 feet along the Stribling Way frontage.

8. Outside Activity Areas Buffer

- As part of the K-Park MUPD PSM, a deviation from Section 6.2.2.F.5 to the required outside activity area buffer adjacent to residential of 50' for School uses is requested. Athletic facilities anticipated in Pod A may be situated towards the southern portion of the site; however, a minimum separation of 50 feet shall be provided proposed from the south property line with multiple layers of on-site landscape and driveway proposed within this separation. A 10-foot Type A/F landscape buffer is currently proposed along the entirety of the south property line with plantings both within this perimeter buffer and along the north side of the driveway. Additionally, a 25-foot-wide landscaped buffer is existing on adjacent property beyond the southern property line where proximate to adjacent residential uses to create additional buffer and separation from outdoor recreation facilities. As such – the intent of the outside activity area buffer requirement is will be met through the site design.

9. Type A Buffer (North adjacent to Church parcel)

- As part of the K-Park MUPD PSM, a landscape buffer deviation for tree planting for the Type A buffer along the northern property line of Pod A is requested. As prescribed in Section 7.8.5.A.1., Type A buffers shall contain one shade tree per 30 linear feet of buffer. A modified Type A buffer is provided along the north side of the Pod A access easement south of the Church site with native ornamental grasses and wildflowers to satisfy the shrub and hedge requirements. The modified buffer eliminates required tree plantings due to the existing overhead utility lines along the access strip which impacts the ability to properly plant shade trees at the required height established in the ULDC. FPL imposes “Right Tree, Right Place” requirements which limit the type of species and height that are planted in areas that are beneath the overhead lines. As such, the proposed deviation requested allows compliance with FPL requirements. There is also existing buffer material on the church property that further screens the access drive within this area.

10. Type A/F Buffer (South property line)

- As part of the K-Park MUPD PSM, a landscape buffer deviation is requested to eliminate the required fence within Type A/F buffer for the eastern portion of the southern property line between the proposed control access gates and SR 7. A modified Type A/F buffer is provided along the south side of Pod A, consisting of a proposed masonry wall at a minimum of 6' in height located at the property line and terminating at the southern gate access into Pod A. The eastern portion of the southern property line, where a wall is not proposed, does not contain any building square footage or active uses; it consists solely of a driveway that provides access to the school site from the roundabout located on the Church property and landscape open space consisting of a dry detention swale. Additionally, an existing landscape buffer along the northern property line of Farmington Estates, the residential community to the south, provides further screening for the areas where a wall is not proposed. The Applicant is also proposing a masonry wall in lieu of the required fence, providing a higher-quality aesthetic and improved site security compared to the fence that is typically required in a Type A/F buffer.

11. Landscape Buffer Perimeter Path

- As part of the K-Park MUPD PSM, a deviation from the required landscape buffer circulation path is requested. As prescribed in Section 6.5.4.7., A minimum width of ten feet shall be added to the minimum width of a perimeter landscape area to accommodate the path's construction. This requirement would be applicable to the north and eastern perimeter of the MUPD where adjacent to Stribling Way and State Road 7. The Pod A buffer adjacent to Stribling Way proposes a code compliant buffer width of 20' and a 10' perimeter general utility easement. In lieu of an additional 10' added to the code required 20' buffer to accommodate a circulation path the project proposes to maintain the existing sidewalk widths within the adjacent ROW's, specifically for Stribling Way at 8' in width.

12. Foundation Planting

- As part of the K-Park MUPD PSM, a deviation for the required foundation planting area for Pod A is requested. As prescribed in Section 7.8.2.B.2., Foundation plantings shall be provided along the front, side and rear facades of non-residential and multiple-family structures with a minimum depth of five feet for each building story or a maximum of 25 feet. The combined length of the required foundation planting shall be no less than 40 percent of the total length of the applicable side of the structure. The proposed K-Park MUPD PSM proposes a deviation to provide the equivalent square footage of planting area as required provided at or near vicinity of building.

The requested deviation allows for a interconnected network of walkways, courtyards, and circulation for the site to accomplish the "campus" feel while still providing the equivalent plantings from a square footage standpoint, to meet the intent of the foundation planting provision. Please refer to the conceptual landscape plans included in the submittal package detailing how each building proposed meets the equivalent square footage of foundation planting otherwise required through innovative design that complements the building configuration, athletic facilities, courtyards, and pathways throughout the site.

13. Internal Lot Trees

- As part of the K-Park MUPD PSM, a deviation from the required internal lot tree requirement is requested. As prescribed in Section 7.8.2.B.1., One tree and three shrubs shall be planted, or preserved, for every 1,500 square feet of a non-residential lot, or fraction thereof, excluding wetland areas and areas with listed species. This standard does not include trees, shrubs or hedges required to be planted in perimeter buffers and vehicular areas. The area subject to the planting requirement also excludes perimeter buffer acreage, lake acreage, and acreage dedicated to sports fields within the MUPD. This results in a requirement of 1,403 trees. The K-Park MUPD proposes 1,050 trees throughout the site resulting in a 25% reduction of the requirement. The shrubs proposed meet the requirement based on the revised regulation.

While the intent of this standard is to ensure adequate tree canopy and landscape integration across large sites, the application of this ratio to a compact, mixed-use project becomes excessive when considered alongside the building massing, interconnected open spaces, and pedestrian-oriented design features. The scale and intensity of development inherently limit the amount of available planting area,

and enforcing this threshold would concentrate trees in a manner that is not functionally or spatially consistent with the site's mixed-use layout.

Instead, the project satisfies the code's purpose through a balanced and comprehensive landscape program that integrates tree canopy with plazas, streetscapes, walkable corridors, and open spaces in a manner that directly enhances the walkable environments for Pods A and B. Planting the proposed trees per the ULDC code requirement would also not allow for the necessary root growth and spacing needed to support mature canopy trees overtime.

POD B DEVIATIONS

1. Required Parking

- As part of the K-Park MUPD PSM, a deviation to the required parking for Pod B is requested. Per the Village Code, both the retail and office parking rates are based on net square feet and not gross square feet. The preliminary building program anticipates a total of 360,000 of net square feet (plus 20,000 SF of outdoor dining) for the retail and office uses. Village Code allows for parking to be calculated at 1 space per 500 SF for all area above 80,000 SF within a MUPD.

The proposed uses, sizes and code required parking for Phase 1 of the approved development may be summarized as follows:

- Retail/Office Up To 80,000 Net S.F - (1 Space / 250 SF)
- Retail/Office Over 300,000 Net S.F - (1 Space / 500 SF)
- Hotel 180 Rooms 250 Spaces (1.25 Spaces / Room + 1 per employee)
- Residential – 15 Units – 34 Spaces (2.25 Spaces per Unit)
- TOTAL = 1,204 Spaces

A deviation is requested to the required parking depicted as outlined by the parking study prepared by Simmons & White included in this submittal. The anticipated maximum parking demand for the proposed plan of development is 867 parking spaces without of a 10% buffer, and 954 spaces with the 10% buffer based on industry standard rates as established in the ITE Parking Generation, 6th Edition and the Urban Land Institute's (ULI) Shared Parking, 3rd Edition publication.

Pod B is designed to provided 1,023 parking spaces, inclusive of 147 shared spaces located within the LifeChurch parking lot south of Pod B, and 100 valet spaces dispersed throughout the lot north of the Pod O lake, south of the Pod O lake, and within the LifeChurch parking lot. As such, the parking requirement is met based on the Shared Parking Study requirement of 954 spaces.

The reduction in required parking ensures a more efficient use of land while also allowing for more possibilities for thoughtfully designed open space as opposed to an overabundance of surface parking.

Should it be determined that the Shared-Parking Study isn't effectively providing required parking to serve the visitor demand, the Applicant has

prepared two (2) contingency plans to provide the ULDC code prescribed minimum parking of 1,204 spaces. The first contingency plan consists of providing 181 shared parking on the Pod A School site where the baseball and softball fields are located to meet the requirement. The second contingency plan consists of providing two (2) potential parking garages located at the lot north of the Pod O lake and south of the Pod O lake within Pod B.

2. Parking Stall Width

- As part of the K-Park MUPD PSM, a deviation to the required parking stall width for Pod B is requested. As prescribed in Table 7.5.2 – required parking stall width shall be 9.5'. The proposed K-Park MUPD PSM establishes a proposed regulation of 9' minimum width in the parking areas which are not considered high-turnover areas. The areas considered in the "High-Turnover Zone" are anticipated to be the most utilized and desirable parking spaces for the Mixed-Use Village and are located along the Main Street corridor and the central parking lots closest to retail storefronts and anticipated foot traffic. The parking lots west of Building A1 and south of the Hotel building are located in non "High-Turnover Zones" and are the subject of the deviation request to allow parking spaces 9' in width. These spaces are less valued and allow for the primary traffic to accommodate larger vehicles.

Pod B will be designed as a compact, pedestrian-oriented destination with a thoughtfully integrated mix of retail, residential, restaurant, hotel, and office uses. This reduction of parking stall width in non "High-Turnover Zones" is consistent with contemporary industry standards, supports efficient land utilization, and allows the project to prioritize high-quality open spaces, wider pedestrian corridors. The requested modification maintains functionality, safety, and accessibility of parking areas while advancing the pedestrian-friendly character of the development.

3. Required Bicycle Parking

- As part of the K-Park MUPD PSM, a deviation from the bicycle parking requirement is requested. Section 7.5.1.D. requires one bicycle parking space per 2,000 square feet of gross floor area. Based on the 565,000 square feet of non-residential floor area within Pod B, a total of 283 bicycle parking spaces would be required. The Applicant proposes a deviation to provide 200 bicycle parking spaces to serve the non-residential uses, excluding the 175,000 square foot hotel.

The bicycle parking requirement of one space per 2,000 square feet of gross floor area is not appropriate when applied to hotel uses, as hotel floor area is not directly correlated to bicycle demand. Hotel guests typically arrive by vehicle, rideshare, or shuttle, and bicycle use is incidental. Applying a square-footage-based standard would overstate demand and result in excess bicycle parking that is unlikely to be utilized. When the requirement is applied to the remaining 390,000 square feet of non-residential uses, the resulting requirement is 195

bicycle parking spaces, which is satisfied by the 200 spaces proposed within Pod B.

4. Parallel Parking Stall Width

- A deviation to the required parallel parking stall width within Pod B of the K-Park MUPD PSM is requested. Section 7.5.4 requires parallel parking spaces to be 10 feet in width; however, the proposed PSM establishes a minimum width of 9 feet. Parallel parking spaces are proposed along both sides of the driveway extending from the State Road 7 access, as well as along the driveway providing access to the hotel.

A reduction from a 10-foot to a 9-foot parallel parking space is appropriate within the mixed-use village context due to the pedestrian-oriented, low-speed design of the internal paved streets. The narrower stall width remains functional and safe given the slower travel speeds, traffic-calmed environment, and shorter vehicle maneuvering distances typical of a village-scale street network. Additionally, reducing the width to 9 feet allows for a more efficient allocation of right-of-way, supporting wider sidewalks, enhanced streetscape features, and improved pedestrian connectivity. The requested modification maintains operational functionality while furthering the compact, walkable block structure envisioned for the development.

5. 90 Degree Parking Drive Aisles & Angled Parking Drive Aisles

- As part of the K-Park MUPD PSM, a deviation to the required drive aisle width for 90-degree parking spaces and angled parking spaces for Pod B is requested. As prescribed in Table 7.5.2 – the required drive aisle width for 90-degree parking spaces shall be 25'. The proposed K-Park MUPD PSM establishes a proposed regulation of 24' minimum width. Similarly, as prescribed in Table 7.5.2 – the required drive aisle width for 60-degree parking spaces shall be 24 for a two-way drive-aisle'. The proposed K-Park MUPD PSM establishes a proposed regulation of 20' minimum width.

The minor 1' reduction in drive aisle width for the 90-degree spaces is consistent with accepted engineering and planning practice and remains adequate to accommodate safe and efficient two-way vehicular circulation. The modification allows for more efficient site planning, enabling the preservation of pedestrian-oriented open spaces and landscaping without compromising the maneuverability of passenger vehicles or the functionality of parking areas. Given that the proposed Mixed-Use Pod is intended to emphasize a walkable, compact, and pedestrian-friendly design, this adjustment strikes an appropriate balance between vehicular access and high-quality placemaking.

The deviation in drive-aisle width along angled parking spaces from 24 feet to 20 feet also allows for a more enhanced pedestrian experience. Angled spaces inherently require less maneuvering depth than 90-degree stalls, and industry standards recognize narrower aisles as appropriate in such configurations. The proposed 20-foot width provides sufficient clearance for safe entry and exit movements while creating a visually engaging and human-scaled experience for the pedestrian promenades in the retail Village. The reduced drive-aisle also serves as a traffic calming technique to ensure that vehicles traversing along the drive-aisle do not speed and to also emphasize the safety and importance of the

pedestrian experience within the pod. It is also worth noting that the 20' drive aisle width does not include 2' of valley curb on each side separating the parking spaces from the drive aisle. When incorporating the valley curb width into the overall drive aisle width along the angled spaces, the width is 24' which is in line with industry standards and the requested deviation for drive aisle widths along standard parking spaces.

6. Loading Space Quantity & Loading Space Dimensions

- As part of the K-Park MUPD PSM, a deviation to the required loading space quantity Pod B is requested. As prescribed in Section 7.5.3 – the required loading regulations are one space for every 15,000 square feet of gross floor area (GFA) up to 100,000 square feet; and one space for every 50,000 square feet of GFA over 100,000 square feet. Additionally, Table 7.5.3. prescribes a minimum loading space size of 15' x 55'.

The required loading space requirements and dimensions and proposed loading deviations between the hotel use and the balance of commercial uses is depicted below:

| HOTEL - 175,000 GSF | | | |
|------------------------------------|----------------|-----------|----------------|
| Zoning | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | | |
| 15' x 55' @ 1/50,000 SF OVER 100K | 2 | | |
| TOTAL HOTEL | 9 | 14' X 50' | 2 |

| RETAIL + F&B + OFFICE - 360,000 NSF | | | |
|-------------------------------------|----------------|-----------|----------------|
| ZONING | Required Count | Proposed | Proposed Count |
| 15' x 55' @ 1/15,000 SF UP TO 100K | 7 | 14' X 40' | 2 |
| 15' x 55' @ 1/50,000 SF OVER 100K | 5 | 14' X 50' | 9 |
| TOTAL RETAIL + F&B + OFFICE | 12 | | 13 |

| | | | |
|-----------------------------|-----------|-----------------------------|-----------|
| TOTAL REQUIRED POD B | 21 | TOTAL PROPOSED POD B | 13 |
|-----------------------------|-----------|-----------------------------|-----------|

The proposed reduction in loading space and loading quantity are directed by Applicant coordination with internal operations teams and consultants as supported in the Building Logistics Analysis Support Letter prepared by Lerch Bates, and as also depicted in the K-Park MUPD PSM.

Additionally, the Applicant has relied on established use patterns and knowledge of standard delivery truck sizes that regularly serve these types of tenants. The 14' x 40' and 14' x 50' loading spaces are consistent with industry practice for accommodating box trucks, service vans, and smaller delivery vehicles that make up the majority of trips anticipated for the hotel and commercial users. A portion of deliveries will also be made via parcel / courier / mail trucks / and vans that will make brief deliveries in the standard parking spaces offered on site. These dimensions, combined with the total number of proposed loading spaces, have

been calibrated to the actual operational needs of the project, ensuring efficient service access while avoiding the overbuilding of loading facilities, and avoiding intrusion on potential land area for pedestrian comfort and open space.

7. Buffer Type A/F (South)

- As part of the K-Park MUPD PSM, a deviation from the required fence within a Type A/F landscape buffer is requested. Section 7.8.5 requires a 10-foot-wide Type A/F buffer that includes a fence. The proposed PSM eliminates the required fence along the southern property line of Pod B, adjacent to the Life Church parcel. At the request of Life Church, the Applicant is proposing to remove the fence to promote a more integrated and connected transition between the church and the Mixed-Use Village to the north, while the existing landscape buffer continues to provide adequate screening and separation between the two sites.

8. Landscape Buffer Perimeter Path

- As part of the K-Park MUPD PSM, a deviation from the required landscape buffer circulation path is requested. As prescribed in Section 6.5.4.7., A minimum width of ten feet shall be added to the minimum width of a perimeter landscape area to accommodate the path's construction. The K-Park MUPD proposes the innovative and pedestrian friendly design of Pod B, providing a minimum 8' clear pedestrian path interior to the site along the storefronts of the mixed-use village buildings. The pedestrian circulation interior to Pod B is also designed to connect to the ROW for both State Road 7 and Stribling Way.

9. Foundation Planting

- As part of the K-Park MUPD PSM, a deviation for the required foundation planting area for Pod B is requested. As prescribed in Section 7.8.2.B.2., Foundation plantings shall be provided along the front, side and rear facades of non-residential and multiple-family structures with a minimum depth of five feet for each building story or a maximum of 25 feet. The combined length of the required foundation planting shall be no less than 40 percent of the total length of the applicable side of the structure. The proposed K-Park MUPD PSM proposes a deviation to provide the equivalent square footage of planting area as required provided at or near vicinity of building.

Pod B is designed as a highly walkable environment featuring 8-foot minimum sidewalks, plazas, outdoor dining areas, and pedestrian gathering spaces. Although foundation plantings are not located immediately adjacent to all Pod B buildings, the design incorporates planters along the opposite side of the pedestrian walkways while maintaining an 8-foot clear pedestrian path. These planters enhance the pedestrian experience by creating a landscaped separation between the travel lanes and the walkways.

The proposed planter configuration upholds the intent of the ULDC foundation planting requirements by softening building edges while also preserving storefront visibility and activation. The project provides an alternative landscape treatment that meets the regulation's intent and better supports the urban design goals of Pod B. Shade trees, decorative planters, outdoor seating, and architectural articulation collectively frame the pedestrian realm, providing comfort, shade, and visual interest. The Conceptual Landscape Plans submitted with the Site Plan

application further detail how each building achieves the equivalent square footage of planting area through this Alternative Design..

10. Internal Lot Trees

- As part of the K-Park MUPD PSM, a deviation from the required internal lot tree requirement is requested. As prescribed in Section 7.8.2.B.1., One tree and three shrubs shall be planted, or preserved, for every 1,500 square feet of a non-residential lot, or fraction thereof, excluding wetland areas and areas with listed species. This standard does not include trees, shrubs or hedges required to be planted in perimeter buffers and vehicular areas. Additional plantings are needed to meet buffer and vehicular area requirements. The area subject to the planting requirement also excludes perimeter buffer acreage, lake acreage, and acreage dedicated to sports fields within the MUPD. This results in a requirement of 1,403 trees. The K-Park MUPD proposes 1,050 trees throughout the site resulting in a 25% reduction of the requirement. The shrubs proposed meet the requirement based on the revised regulation.

While the intent of this standard is to ensure adequate tree canopy and landscape integration across large sites, the application of this ratio to a compact, mixed-use project becomes excessive when considered alongside the building massing, interconnected open spaces, and pedestrian-oriented design features. The scale and intensity of development inherently limit the amount of available planting area, and enforcing this threshold would concentrate trees in a manner that is not functionally or spatially consistent with the site's mixed-use layout.

Instead, the project satisfies the code's purpose through a balanced and comprehensive landscape program that integrates tree canopy with plazas, streetscapes, walkable corridors, and open spaces in a manner that directly enhances the walkable environments for Pods A and B. Planting the proposed trees per the ULDC code requirement would also not allow for the necessary root growth and spacing needed to support mature canopy trees overtime.

SECTION 6.5.6.H.1 STANDARDS

1. Council shall approve with the project master plan the percentage allocation of each use type based on the project design and efficient use of land, with a maximum percentage allocation for a single use not to exceed 60 percent of the total MUPD acreage. In consideration of the percentage allocation of uses. Council shall consider the following factors:

- i. A market study provided by the applicant/developer, showing the current demand of the use proposed for the project. The project should include one or more uses that is needed in the area as determined by Wellington.

Response: The findings of the Market Study prepared by the Village of Wellington for the State Road 7 Corridor supports the additional retail and residential uses proposed by this project.

ii. Furthers the intent of a mixed-use district and encourages development of mixed-use adjacent to other mixed-use developments, while providing enhanced inter-connectivity of both residential and non-residential uses within the project and with adjacent development(s), that is above any minimum required.

Response: The proposed K-Park MUPD further the intent of the mixed-use district with a thoughtful offering of institutional, commercial, office, and residential uses that will improve an underutilized, large parcel of land located at along a key vehicular thoroughfare within the village of Wellington. The proposed residential uses within Pod B are vertically integrated with commercial uses and are also supported by a comprehensive pedestrian network which provides interconnectivity between each pod and the adjacent ROW.

iii. Provides community benefit(s), open to the public, within the project or off-site on Wellington property that is above any minimum required. Council approval is required for the community benefit(s) as part of the master plan consideration.

Response: Community benefits to be provided include a Main Street styled shopping and dining experience with generous and pedestrian friendly sidewalks, promenades, and recreation areas. The pedestrian oriented public realm includes public plazas, green spaces and gathering places with lush landscaping, ample hardscape, water features, and public art that will be integrated throughout the site. These spaces can be activated for various uses such as community events, programming, and leisure. On a daily basis the public realm will include many passive and interactive components oriented to families, children, and teens.

iv. The project includes at least two facets of sustainable development, that are above any minimum required, such as a renewable energy source (solar/wind power); at least five percent of the required non-residential and residential common area on-site parking is dedicated to electric vehicle (EV) charging stations (Level 2 or Direct-current fast charger); green building design; utilization of recycled materials and/or eco-friendly materials for site and/or vertical construction; water conservation; waste reduction/recycling; etc.

Response: The K-Park MUPD is proposed to be designed and built to minimum LEED Silver or comparable building standards across the entire project. The Project will also be designed to incorporate eco-friendly materials throughout construction.

MASTER PLAN STANDARDS

A. Whether the proposed amendment is consistent with all elements of the comprehensive plan.

Response: The Master Plan for the 71.27-acre K-Park MUPD is consistent with all elements of the Village of Wellington Comprehensive Plan, specifically the Future Land Use & Community Design Element (LU&CD). The project fulfills the vision for large-scale mixed-use developments and aligns with the community's long-term planning principles.

The proposed development is consistent with Goal LU&CD 1, which emphasizes enhancing the character and quality of Wellington's neighborhoods, commercial districts, and corridors. The project achieves this by utilizing a mix of land uses on a site greater than 60 acres, consistent with Objective LU&CD 1.4 and Policy LU&CD 1.4.1 for Large-Scale Mixed-Use (LSMU) designations. It satisfies the required balance of uses by dedicating more than 60% of the project area to non-residential functions, including commercial, and institutional components, while also providing residential density at a level compatible with the maximums density of 5 DU/AC permitted in the Mixed Use (MU) Land Use category. This mixed-use character supports the regional orientation of the site and contributes to a sustainable and walkable development pattern.

Further supporting the Comprehensive Plan's vision, the location of the site along State Road 7 fulfills Objective LU&CD 2.5, which promotes the corridor's evolution into a mixed-use destination with housing, businesses, and employment opportunities. The school, hotel, retail, and dining components encourage job creation and community interaction, consistent with Policies LU&CD 2.5.3 and 2.5.4, while the inclusion of up to an additional 215 multifamily housing units supports Policy LU&CD 2.5.5 by providing residential options for those that work within the MUPD and future potential residents in the Village of Wellington. The project also contributes to Wellington's strategic land use pattern by providing a mix of vertically and horizontally integrated uses, reinforcing the corridor's vitality and reducing development pressure on more suburban and rural areas.

The project has also been designed with integrated vehicular and pedestrian circulation networks in accordance with Policy LU&CD 1.3.2, and will include landscaping, buffering, and open space features that enhance the site cohesion. These features will enhance the transition between uses and preserve the character of adjacent neighborhoods.

The project complies with Policy MB 1.1.3 as the MUPD proposes to maintain the 8' wide sidewalk within the Stribling Way ROW, a comprehensive sidewalk network along the storefront and plaza areas of Pod B, and an 8' minimum lake path surrounding the Pod "O" Common Open Space Lake. The site is also designed to ensure adequate circulation aisles, turning radii and parking spaces for comfortable vehicular circulation, demonstrating compliance with Policy MB 2.1.2.

The project will comply with Policy CI 1.3.3 and 1.4.1 of the Capital Improvements Element as it relates to Proportionate Share cost of expanded facilities and infrastructure, and impact fees required by the proposed development. The Applicant will commit to providing the required proportionate share and impact fees generated by the new development as coordinated with Village staff throughout the development process. Proposed improvements associated with the project will increase the efficiency of public utility infrastructure at the expense of the Applicant in direct support of the CI Element policies.

The project also complies with Policy ED 1.4.2 of the Education Element of the Comprehensive Plan with the provision of the school proposed in Pod A of the MUPD. The proposed school will provide a comprehensive curriculum focused on

academic excellence, global citizenship, and holistic development. Instruction will be delivered in a nurturing, inclusive environment that fosters critical thinking, creativity, and lifelong learning further enhancing the educational opportunities in Wellington.

The project complies with Policy H&N 2.1.3 of the Housing Element of the Comprehensive Plan through the accommodation of mixed-use development and vertically integrated housing and commercial with the Pod B design. The proposed mixed-use pod is also adjacent to State Road 7 which is a major transportation corridor appropriate for the mix of uses.

The project complies with Policy PF 1.1.4 of the Public Facilities Element of the Comprehensive Plan which prescribes that adequate water supplies and required infrastructure are available to serve new development no later than the date of the issuance of the first certificate of occupancy. The proposed MUPD will be designed to provide the necessary water supplies and infrastructure for the Pod A School and Pod B Mixed-Use Village prior to the first Certificate of Occupancy.

The project will also comply with Policy CSR 3.1.1 of the Conservation, Sustainability, & Resiliency Element. The proposed construction for Pod A and Pod B will be designed to achieve LEED silver certification or similar green building ratings on the architecture proposed.

In summary, the proposed master plan for this MUPD is fully consistent with the goals, objectives, and policies of the Village of Wellington's Comprehensive Plan. It advances the community's vision for sustainable, integrated, and economically vibrant development

- A. *Whether the proposed request is not in conflict with any portion of Wellington's LDR and is consistent with the purpose and intent of the LDR.*

Response: The proposed MUPD will be consistent with the purpose and intent of the Land Development Regulations adopted for the Village of Wellington. The proposed MUPD is consistent within the minimum lot dimensional requirements assessed for an MUPD, as well as the maximum building coverage, and maximum 72' building height allowances for properties within a mile of the State Road 7 Corridor, as outlined in Section 6.3.1.G. The MUPD also satisfies the required number of use types for a gross acreage in excess of 30 acres. The K-Park MUPD provides institutional, residential, commercial, office, and common open space uses for the 71.27-acre site.

As part of the Project Standards Manual (PSM) required for Planned Developments in the Village, a number of deviations are requested for each Pod and are further detailed earlier in this narrative and within the PSM.

- a. *Minimize adverse impacts on environmentally sensitive areas*

The site is currently vacant and located within a heavily urbanized, commercial corridor, minimizing any direct encroachment or disruption to environmentally sensitive areas.

b. Increase and promote public transit, bicycle routes, and non-vehicular modes of transportation

A comprehensive pedestrian circulation network connects the residential, educational, commercial, and recreational components internally, while also providing safe access to the Stribling Road and State Road 7 ROW from the site.

c. Increase use of public and private amenities as part of PD design

The MUPD will feature a mix of private amenities for the school and public-facing amenities, inclusive of retail spaces, outdoor plazas, and walkable public areas highlighted in Pod B. These will be integrated into the master plan to encourage daily use by residents, visitors, and employees, thereby supporting community engagement and vibrancy.

d. Provide for efficient use of land, utilities, and infrastructure

By concentrating multiple land uses—including institutional, residential, commercial, office, and common open space within a compact and coordinated development footprint, the MUPD maximizes land efficiency and promotes shared infrastructure. Utilities and stormwater systems are designed comprehensively to serve the full development, reducing redundancy and optimizing long-term maintenance and public service provision.

e. Provide for effective circulation patterns, internal trip capture, integrated land uses, and enhanced open space and recreational areas

The traffic analysis prepared by Simmons & White demonstrates the proposed development meets the requirements of both the Palm Beach County Traffic Performance Standards and the Village of Wellington Traffic Performance Standards with conditions.

f. Ensure adequate storm water management

The MUPD will include a comprehensive stormwater management plan consistent with applicable regulations, incorporating retention/detention facilities and strategies that address both water quality and quantity. See Preliminary Civil Plans and Drainage Statement prepared by Langan included with the submittal package.

g. Encourage distinctive design while achieving compatibility with the surrounding area

The MUPD will be designed as visually distinctive for each pod, yet contextually compatible development while also providing a distinct architectural palette that highlights the unique location at a major intersection within the Village to serve as a focal point along the corridor. Thoughtful massing and breaks and projections in architectural facades will create a human-scaled environment that is both functional and attractive.

C. *Whether and the extent to which the proposed amendment would result in significantly adverse impacts on the natural environment.*

Response: The proposed development will not result in significantly adverse impacts on the natural environment. The site is currently used for low-intensity agricultural purposes and has been cleared of any significant vegetation or tree plantings that would be impacted by the proposed development. The plan also preserves and integrates water management features such as lakes and swales, which support stormwater retention and improve water quality. Environmental impacts will be minimized through careful planning and regulatory compliance.

D. *Provide the overall design concept and show that the design of the proposed request minimizes adverse effects, including visual impact and intensity of the proposed use on adjacent lands*

Response: The overall design minimizes adverse effects, including visual impact and intensity of the proposed use on adjacent lands. The design incorporates extensive perimeter landscaping buffers, and open spaces to minimize visual and operational impacts on adjacent single-family neighborhoods and institutional uses such as Life Church Wellington. Specifically, the MUPD minimizes visual impact to the neighboring residential to the West through the provision of a 10' Type A/F buffer which is adjacent to an existing 20' Landscape Easement for the Oakmont Estates PUD west of the Type A/F buffer. Similarly for the northern buffer, the Applicant provides a 20' Type C buffer and maintains the width of the existing sidewalk within the Stribling Way ROW as 8'. The southern perimeter provides a modified 10' Type A/F buffer with a masonry wall provided for a portion of the length of the buffer. Commercial and hotel uses (Pod B) are placed along State Road 7, a major arterial, where higher intensities are more appropriate and expected. Pedestrian and vehicular connectivity is thoughtfully planned to reduce internal congestion and integrate safely with external road networks.

E. *That the proposed request is consistent with applicable neighborhood plans.*

Response: No neighborhood plans have been identified for the subject site area.

F. *That the proposed request would result in a logical and orderly development pattern.*

Response: The master plan, as proposed, results in a logical and orderly development pattern by converting a large, underutilized agricultural tract into a mixed-use community with a complimentary selection of uses. By clustering commercial uses along a major transportation corridor and placing institutional and residential uses internally with appropriate transitions, the plan avoids land use conflicts and supports efficient access to goods, services, schools, and employment. The internal circulation network, combined with external connectivity to Stribling Way and State Road 7, reflects thoughtful coordination with the broader mobility and land use system. This approach promotes long-term viability, land use balance, and community cohesion.

G. *That the proposed request complies with Wellington building standards.*

Response: The proposed request will comply with Wellington building standards.

CONCLUSION

On behalf of the applicants, Urban Design Studio requests favorable review and consideration of this request to approve the proposed Master Plan. The Project Managers at Urban Design Studio are Ken Tuma, and Lentzy Jean-Louis, who can be reached at 561-366-1100. Please feel free to contact with any questions or for additional information in support of this application.



Business Impact Estimate Form

This Business Impact Estimate Form is provided to document compliance with and exemption from the requirements of Sec. 166.041(4), Fla. Stat. If one or more boxes are checked below under “Applicable Exemptions”, this indicates that the Village of Wellington has determined that Sec. 166.041(4), Fla. Stat., does not apply to the proposed ordinance and that a business impact estimate is not required by law. If no exemption is identified, a business impact estimate required by Sec. 166.041(4), Fla. Stat. will be provided in the “Business Impact Estimate” section below. In addition, even if one or more exemptions are identified, the Village of Wellington may nevertheless choose to provide information concerning the proposed ordinance in the “Business Impact Estimate” section below. This Business Impact Estimate Form may be revised following its initial posting.

Proposed ordinance’s title/reference:

ORDINANCE NO. 2026-06

AN ORDINANCE OF WELLINGTON, FLORIDA’S COUNCIL, APPROVING A REZONING [PETITION NUMBER 2025-0003-REZ] BY AMENDING THE ZONING DESIGNATION FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY FACILITIES (CF) TO MULTIPLE USE PLANNED DEVELOPMENT (MUPD), TOTALING APPROXIMATELY 65.98 ACRES, MORE OR LESS; FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY COMMERCIAL (CC) TO MUPD, TOTALING APPROXIMATELY 5.3 ACRES, MORE OR LESS; FOR A GRAND TOTAL 71.27-ACRES; PROVIDING A CONFLICTS CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING AN EFFECTIVE DATE.

Applicable Exemptions:

- ☐ The proposed ordinance is required for compliance with Federal or State law or regulation;
- ☐ The proposed ordinance relates to the issuance or refinancing of debt;
- ☐ The proposed ordinance relates to the adoption of budgets or budget amendments, including revenue sources necessary to fund the budget;
- ☐ The proposed ordinance is required to implement a contract or an agreement, including, but not limited to, any Federal, State, local, or private grant, or other financial assistance accepted by the municipal government;

- ☐ The proposed ordinance is an emergency ordinance;
- ☐ The ordinance relates to procurement; or
- ☒ The proposed ordinance is enacted to implement the following:
 - ☒ Part II of Chapter 163, Florida Statutes, relating to growth policy, county and municipal planning, and land development regulation, including zoning, development orders, development agreements, and development permits;
 - ☐ Sections 190.005 and 190.046, Florida Statutes, regarding community development districts;
 - ☐ Section 553.73, Florida Statutes, relating to the Florida Building Code; or
 - ☐ Section 633.202, Florida Statutes, relating to the Florida Fire Prevention Code.

Business Impact Estimate:

The Village of Wellington hereby publishes the following information:

1. **A summary of the proposed ordinance (must include a statement of the public purpose, such as serving the public health, safety, morals and welfare):**

2. **An estimate of the direct economic impact of the proposed ordinance on private, for-profit businesses in the municipality, including the following, if any:**

- (a) **An estimate of direct compliance costs that businesses may reasonably incur if the ordinance is enacted:**

- (b) **Identification of any new charge or fee on businesses subject to the proposed ordinance, or for which businesses will be financially responsible:**

- (c) **An estimate of the municipality's regulatory costs, including an estimate of revenues from any new charges or fees that will be imposed on businesses to cover such costs:**


3. **A good faith estimate of the number of businesses likely to be impacted by the ordinance:**

4. Additional information the governing body determines may be useful (if any):

Note: Wellington's provision of information in the Business Impact Estimate section above, notwithstanding an applicable exemption, shall not constitute a waiver of the exemption or an admission that a business impact estimate is required by law for the proposed ordinance. Wellington's failure to check one or more exemptions below shall not constitute a waiver of the omitted exemption or an admission that the omitted exemption does not apply to the proposed ordinance under Sec. 166.041(4), Fla. Stat., Sec. 166.0411, Fla. Stat., or any other relevant provision of law.



Wellington Legal Notice

| | |
|---|--|
| Please choose a category | Public Notices - Wellington |
| Title | ORDINANCE NO. 2026-06 K PARK (REZONING) |
| Publish Date | 01/14/2026 |
| Publish Time | 9:14 AM (EST) |
| Description | AN ORDINANCE OF WELLINGTON, FLORIDA'S COUNCIL, APPROVING A REZONING [PETITION NUMBER 2025-0003-REZ] BY AMENDING THE ZONING DESIGNATION FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY FACILITIES (CF) TO MULTIPLE USE PLANNED DEVELOPMENT (MUPD), TOTALING APPROXIMATELY 65.98 ACRES, MORE OR LESS; FOR A PORTION OF CERTAIN PROPERTY FORMERLY KNOWN AS K PARK FROM COMMUNITY COMMERCIAL (CC) TO MUPD, TOTALING APPROXIMATELY 5.3 ACRES, MORE OR LESS; FOR A GRAND TOTAL 71.27-ACRES; PROVIDING A CONFLICTS CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING AN EFFECTIVE DATE. |
| Attach Files (Optional) | <div> 1.27.26 PZAB (KParK) REZ (3).pdf</div> |
| Submitted by (Email Address) | smilachay@wellingtonfl.gov |
| Notifications | Yes |
| Send Out a Notification to Your Subscribers | Yes |
| Signature |  |